

meetmidtown

Midtown OPA

April 22nd 2024

Statutory Public Meeting

Agenda

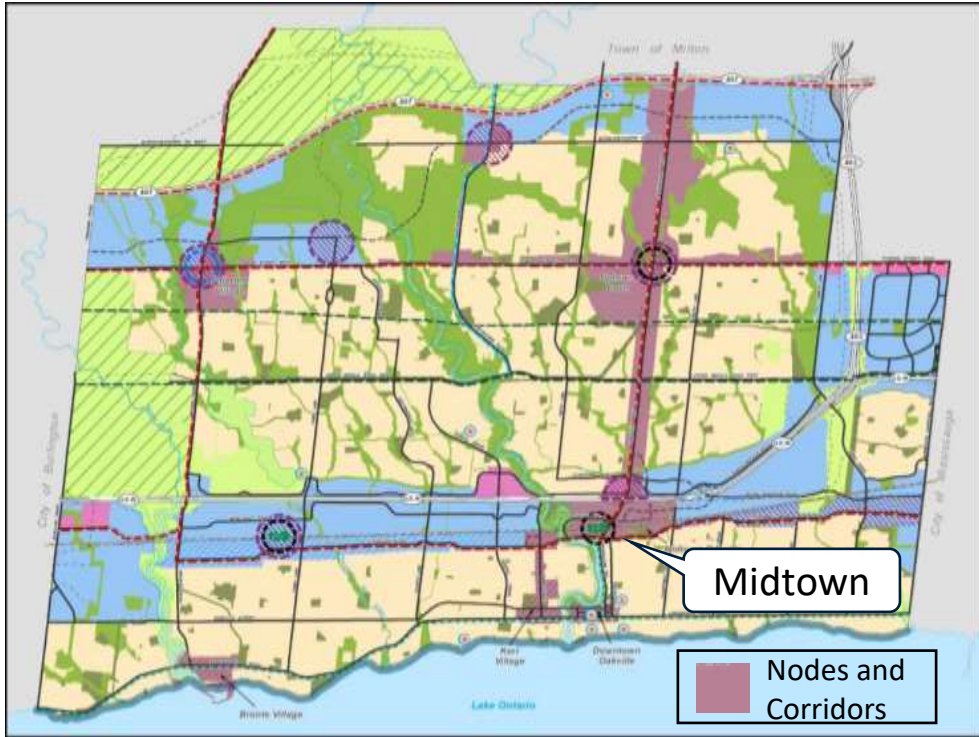
1. Purpose of this Project
2. Project Timeline and Engagement Process
3. Key Changes to the Demonstration Plan
4. OPA Policy Framework
5. Process Forward



**1. Purpose of this
Project**

Purpose

What is Midtown's Role in the Town's Urban Structure?



Oakville Official Plan: Schedule A1 Urban Structure

- The Province sets growth expectations that must be planned for within a specified time horizon, 2051. – **Halton Region - 1.1 million people + 500,000 jobs by 2051**
- Town and Halton Region together estimate where growth will occur in **Oakville, allocating 442,941 people + 212,116 jobs by 2051**, and in Halton Region, allocating 1.39 million people + 624,237 jobs by 2051.
- In Oakville, growth is distributed according to Oakville's Urban Structure which has been in-force since **2017**.
- Midtown Oakville has been identified as an **Urban Growth Centre** and **Major Transit Station Area** – a strategic location for growth and intensification
- **Midtown Oakville is intended to accommodate a large proportion of the Town's future intensification. The existing planning policies for Midtown must be updated to align with current Provincial planning policy direction and legislative changes.**

Purpose

The Town's Responsibility regarding the OPA is to:

- Align the local Official Plan with Provincial policies and directions
- Plan for future job and population growth
- Coordinate the provision of necessary services and infrastructure with anticipated growth
- Responsibly manage land uses, development, and infrastructure for long-term sustainability
- Implement and monitor the Plan, and make necessary adjustments based on trends and changing circumstances and priorities (ie. 5-Year Municipal Comprehensive Review Process)

Today's Goals:

- **Present the OPA framework and key policy directions**
- **Highlight the factors that have shaped the draft OPA**
- **Receive public input on the proposed policy directions, draft OPA policies, and schedules**

Purpose

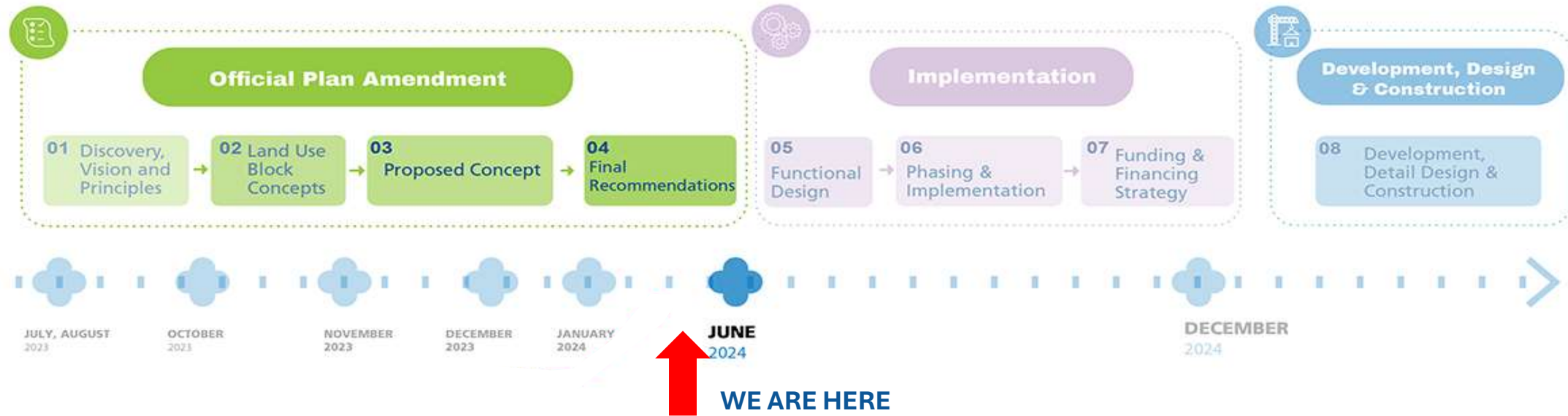
The proposed OPA responds to Midtown's role in policy and plans for growth in Midtown to result in a complete and vibrant community. Proposed polices describe:

- The character and permitted uses throughout Midtown;
- The types of housing, including affordable housing, which will be encouraged in Midtown;
- Midtown's parkland and open space objectives;
- A new fine-grained transportation network;
- A pedestrian-focused public realm and active retail streets; and
- Coordinated infrastructure, transportation network, and public service facilities.

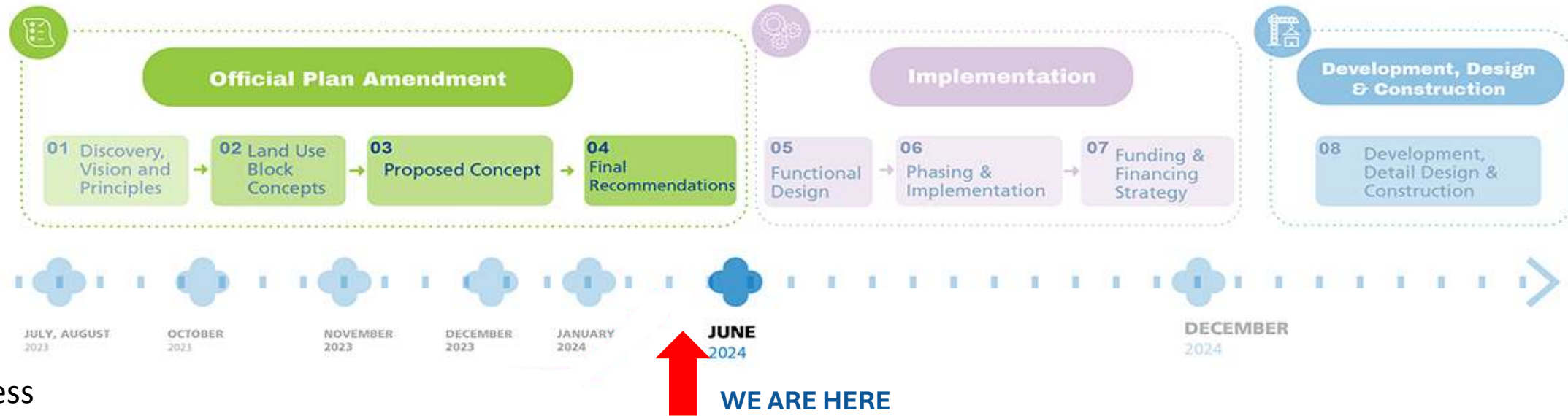


**2. Project Timeline
and Engagement
Process**

Where We Are in the OPA Process

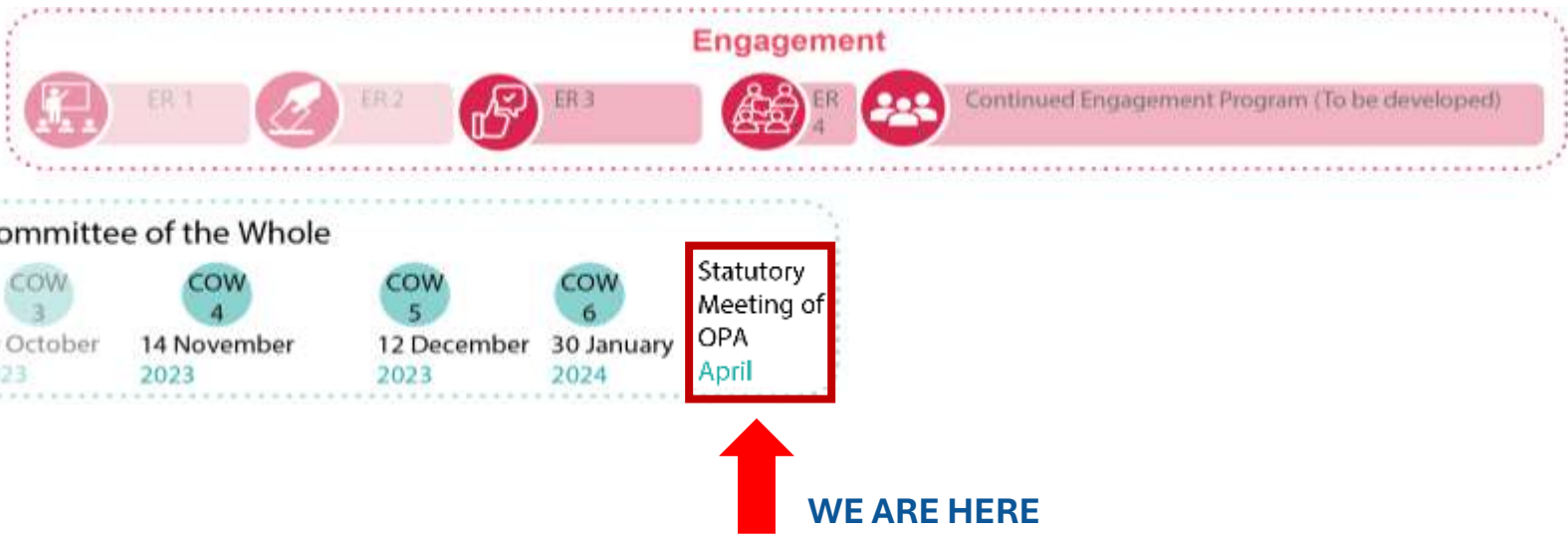


Where We Are in the OPA Process



↑ Process

↓ Engagement



↑ WE ARE HERE

656

Subscribers of project newsletter, open rate 68%



12,000+



Midtown page views (5,000 unique users)

120,000+

Views on 86 Midtown posts reaching 60,000 users



STAKEHOLDERS



3 Technical Advisory Committee meetings

6 Total Number of COW Meetings **4** Resident Association Meetings

1 Special Council Meeting **3** Developers Group Meetings

13+ One on one meeting with different stakeholders



Council



Resident Associations



Government Agencies



Developers

PIC SUMMARY

428 Total attendees across 3 PICs

151

Members of the public attended PIC 1 Meeting

117

Members of the public attended PIC 2 Meeting

160

Members of the public attended PIC 3 Meeting

48%

Participated in public consultation for the first time

286

Views of the live PIC 1 Youtube event



405

Views of the live PIC 3 Youtube event



Our last consultation was focused on the Proposed Concept

- On February 15, 2024 we held a Public Information Centre (PIC) Meeting where we:
 - Discussed Midtown's growth expectations to 2051 and beyond
 - Reviewed the Proposed Concept and its content, and discussed:
 - Project, Process, Policies
 - Height and Density
 - Housing
 - Precincts and Destinations
 - Mobility
 - Open Spaces
 - Gathered public input and answered questions



Diverse Feedback

Feedback received at the PIC highlights the balance required to plan for growth while preserving a sense of the broader Town identity. A range of opinions were expressed across all topics that the OPA addresses

Project, Process, and Policies

- Continued concern over provincial goals of promoting high-density intensification
- Desire for more gentle density to be considered in other areas of Oakville (i.e. multiplexes next to single detached houses in neighbourhoods)
- Interest in understanding Town oversight in implementation
- Questions regarding People Per Unit assumptions

Height and Density

- Support for high-density housing and tall buildings to increase housing supply and create affordability for the next generation; support for density that is reflective of market demand and height that is similar in scale to other Urban Growth Centres
- Concern with high-density growth, and support for low and mid-rise scale development only and lower tower heights for tall buildings.

Diverse Feedback

Feedback received highlights the balance required to plan for growth needs while preserving a sense of the broader Town identity. A range of opinions were expressed across all topics that the OPA addresses

Housing

- Support for affordable housing, non-market, and a range of housing tenures
- Support for increased housing supply to support future generations
- Concerns over future unit sizes being too small or too large
- Desire for housing to be accompanied by strategies and amenities to create a livable community

Mobility

- Support for a car-free lifestyle and prioritization of improved transit and active transportation
- Concern over lack of parking and support for increased car-oriented infrastructure
- Concern over traffic impacts for current residents
- Concern that sufficient mobility infrastructure will not be in place to support development

Diverse Feedback

Feedback received highlights the balance required to plan for growth needs while preserving a sense of the broader Town identity. A range of opinions were expressed across all topics that the OPA addresses

Precincts and Destinations

- Support for a diverse, unique, and fun destination with a range of large and small retail opportunities
- Concern that too much non-residential and active street frontage is being required
- Support for flexible land use and intimate main streets

Open Spaces

- Support for extensive parks and open space
- Concerns about how parks and open spaces will be paid for or secured by the Town
- Concerns that too much park is being proposed
- Desire for additional public services and additional schools beyond what the School Boards have requested

3. Key Changes

Changes to elements of the OPA framework have arisen through stakeholder consultation and discussions with staff and the consultant team

Key Changes Include:

1. Right-of-Way (ROW) realignments
2. Park land changes
3. Height + Density Refinements

Right-of-Way (ROW) Realignmentments

- Adjustments to the ROW alignments and widths were made in consultation with City Staff and TAC stakeholders
- As a result, some blocks increased in size which increased the developable land in Midtown by 2.5 hectares - from 41.7 ha as shown on Feb 15th to **44.2 ha**

Previous Road Alignments



Current Road Alignments



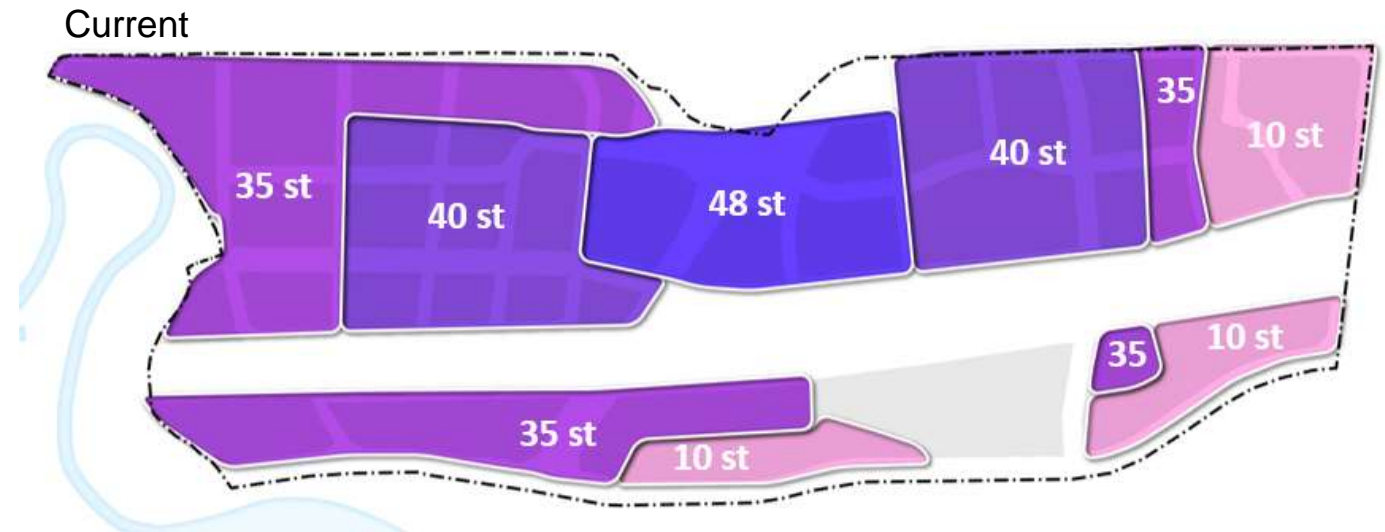
Parkland Adjustments

- Parkland has been **refined**, with some parks being added, removed, or changed
- The **same amount of total Parkland remains**, realized in more park parcels
- The total amount of proposed parkland **remains in line** with the *Parks Plan 2031* (i.e. 12%)



Height Refinements

- Stakeholders have provided **competing views** on the strategy for height and density in Midtown
- The OPA relies on **Maximum heights** along with **minimum densities** and **built form policies** addressing tower separation, podium heights other design elements to control the intensity of development, which can be further detailed in future **urban design guidelines** and Midtown's **implementing by-law**.
- Maximum heights of tall building elements have been refined to **simplify and rationalize** the strategy
- Heights continue to provide for a **height peak** and **transition** to surrounding context.



Summary of Changes

- Realigned and changed **ROWs** resulted in more developable land
- **Parkland** was refined, and continues to equal 12% of the land in Midtown
- **Heights** were refined, and continue to create a height peak in the centre of Midtown, and will be used along with design policies to control development intensity



**4. OPA Policy
Framework**

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OPA Policy Framework

- Goals & Objectives
- Land Use
- Precincts
- Housing
- Main Streets
- Height and Density
- Urban Design
- Parks and Open Space
- Transportation
- Public Service Facilities
- Transit Station, Rail, and Highways
- Infrastructure and Servicing
- Sustainability
- Exceptions
- Implementation
- Other Sections of the OP

Goals and Objectives

- The Goals and Objectives section of the OPA outline the Town's intent
- They describe how the Town aims to achieve a vibrant, complete, people-oriented and transit-supportive community
- The Goals for Midtown are aligned with the area's policy expectations as a UGC and PMTSA



Kendall Square, Boston (Klopfert Martin)

Land Use

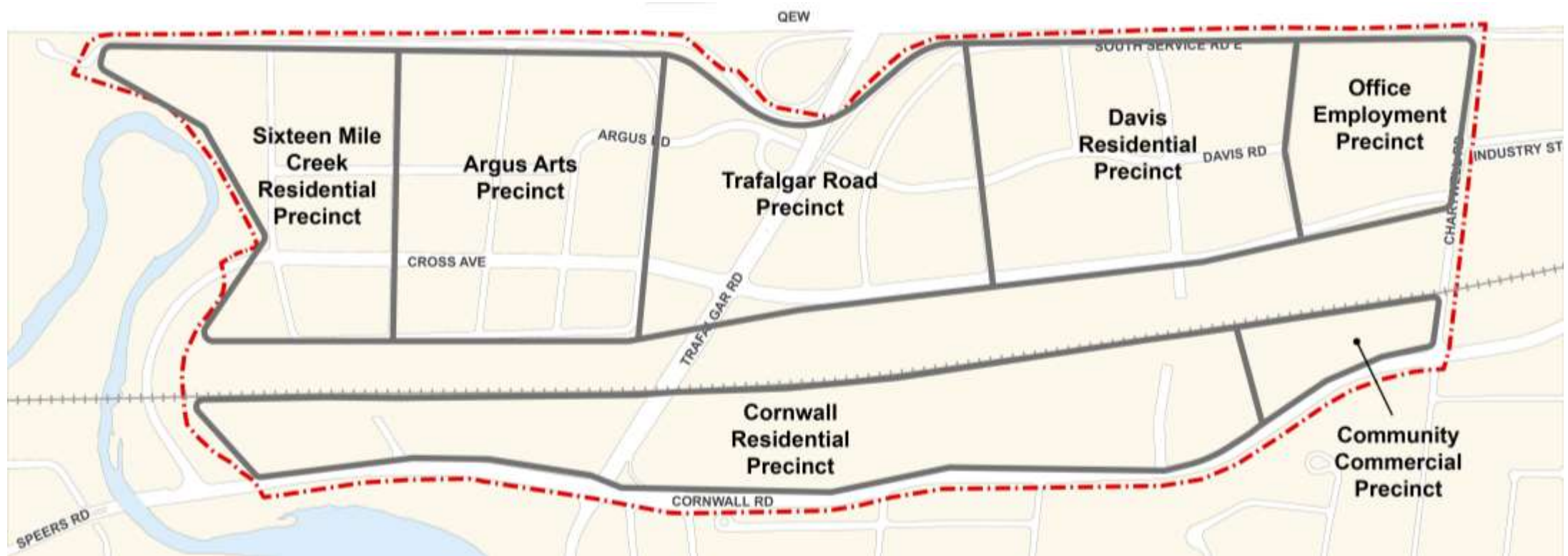
- Land use policies in the OPA guide permitted uses in Midtown.



Schedule L1 Midtown Oakville Land Use

Precincts

- The Precincts establish character areas through which the vision for Midtown can be achieved. Precincts are not land use designations and flexible boundaries are possible.



Schedule L2 Midtown Oakville Precincts

Housing

Housing policies of the OPA provide direction for residential development:

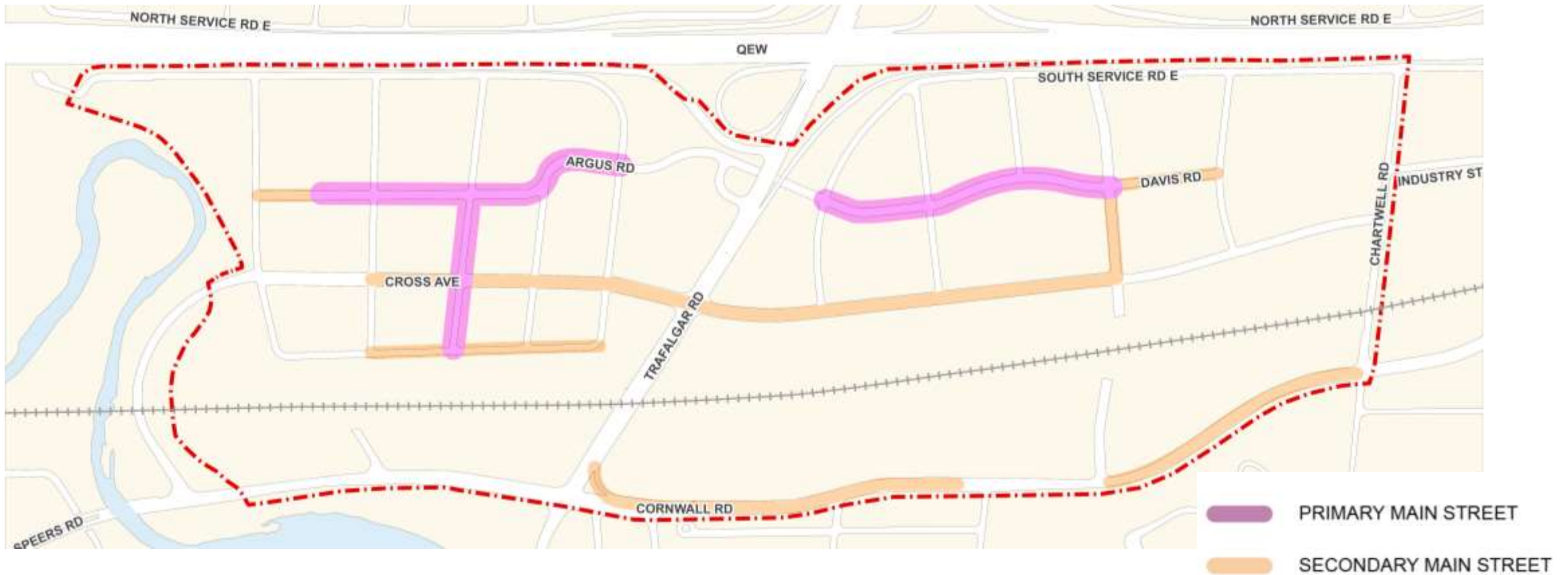
- **Range of Housing:** Development should include a range of building types and unit sizes, including affordable housing and non-market housing.
- **Livable Design:** Development should include balconies, terraces, common areas (indoor/outdoor), operable windows, and storage lockers.
- **Affordable Housing Framework:** An Inclusionary Zoning (IZ) framework may be established for Midtown, and the Town may provide incentives to facilitate the provision of affordable housing or special needs housing.
- **Rental Housing:** The demolition or redevelopment of existing rental housing shall be governed by the Rental Housing Protection By-law.



Radio City Condos. Source: Strata.ca

Main Streets

- **Main Streets as identified in the OPA shall be attractive, pedestrian-oriented, cultural spines. Main Street policies include requirements for active non-residential uses at-grade**
 - (Minimum 70% in Primary Main Streets and minimum 40% in Secondary Main Streets)



Schedule L6 Midtown Oakville Main Streets

Urban Design

Height and density in Midtown will be managed through a combination of height and built form policies:

- **Built Form:** Development will demonstrate design excellence, including policies to guide the following:
 - A **minimum separation distance** of 15 m for all buildings, and 30 m for high-rise towers.
 - Incorporating **slender floorplates** and provide adequate setbacks for tall buildings, to promote pedestrian comfort and minimize impacts on the public realm.
 - **Varying building heights** of multiple towers within a block;
 - **Street-framing podium buildings** and **pedestrian-scaled development**
 - **Comfortable microclimate** conditions for pedestrians;
 - Incorporating where possible rooftop **terraces, gardens and landscaped areas** for private amenity areas;
- Many building types are permitted and will reflect market and community demand.



Urban Design

- **Area Design Plans:** Required for development applications, these plans will show a comprehensive development scheme for an entire block.
- **Public Realm:** Development shall contribute to a high-quality public realm. Policies focus on pedestrian comfort and pedestrian-oriented spaces.
- **Block Design:** The configuration of blocks shall create a well-connected street network, further promoting pedestrian comfort and opportunities for active transportation and transit use.
- Further direction will be included in the urban design guidelines: the **Designing Midtown Oakville** document and the **Livable by Design Manual**.
- **A Community Planning Permit by-law** or a **zoning by-law**, can be used to implement urban design policies



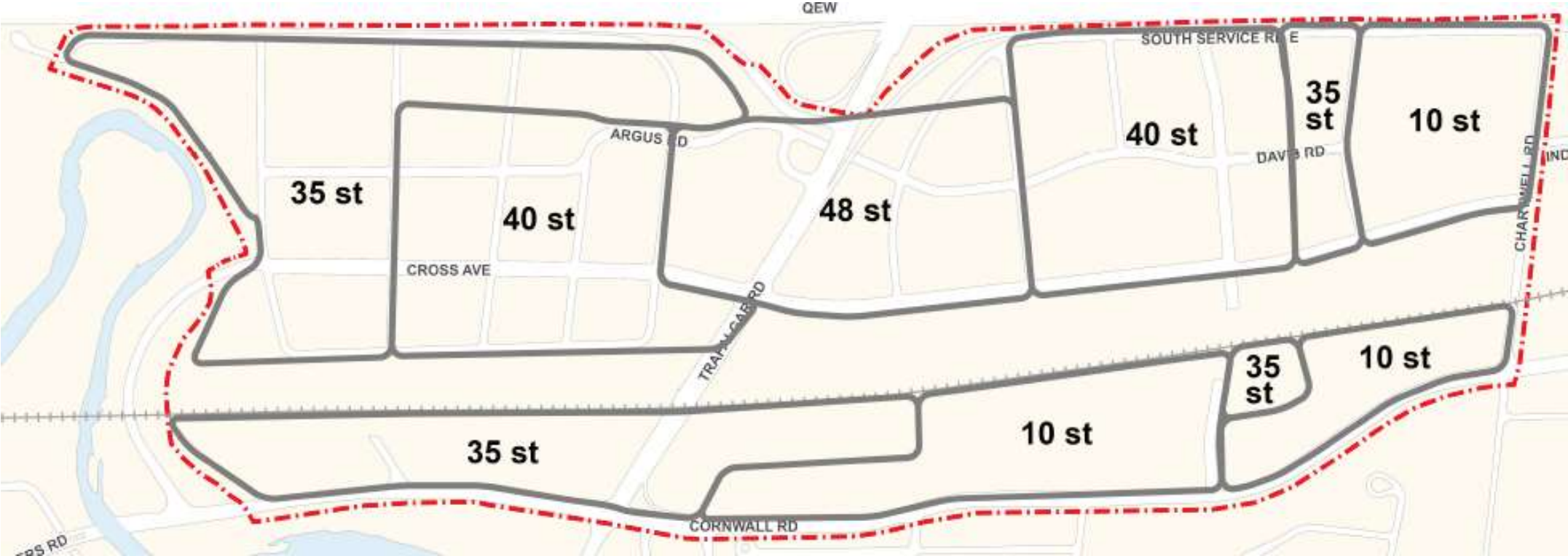
Regent Park, Toronto



Designing Midtown Oakville

Height and Density

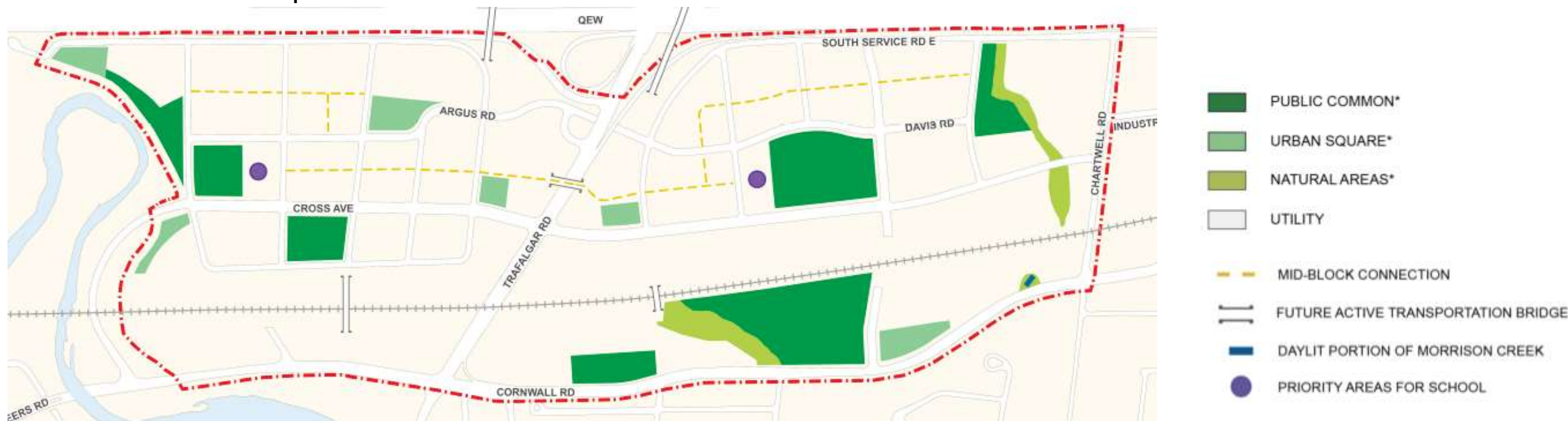
- Height and density in Midtown will be managed through a combination of height and built form policies.
- **Minimum Density** of 1.0 Floor Space Index (FSI) shall apply to all of Midtown, except for sites less than 2,500 m² in size
- A **Community Planning Permit By-law (CPPS Community Planning Permit System)** may be implemented in the future which may require specified community benefits or cash-in lieu in order to achieve certain heights.



Schedule L7 Midtown Oakville Maximum Height

Parks and Open Space

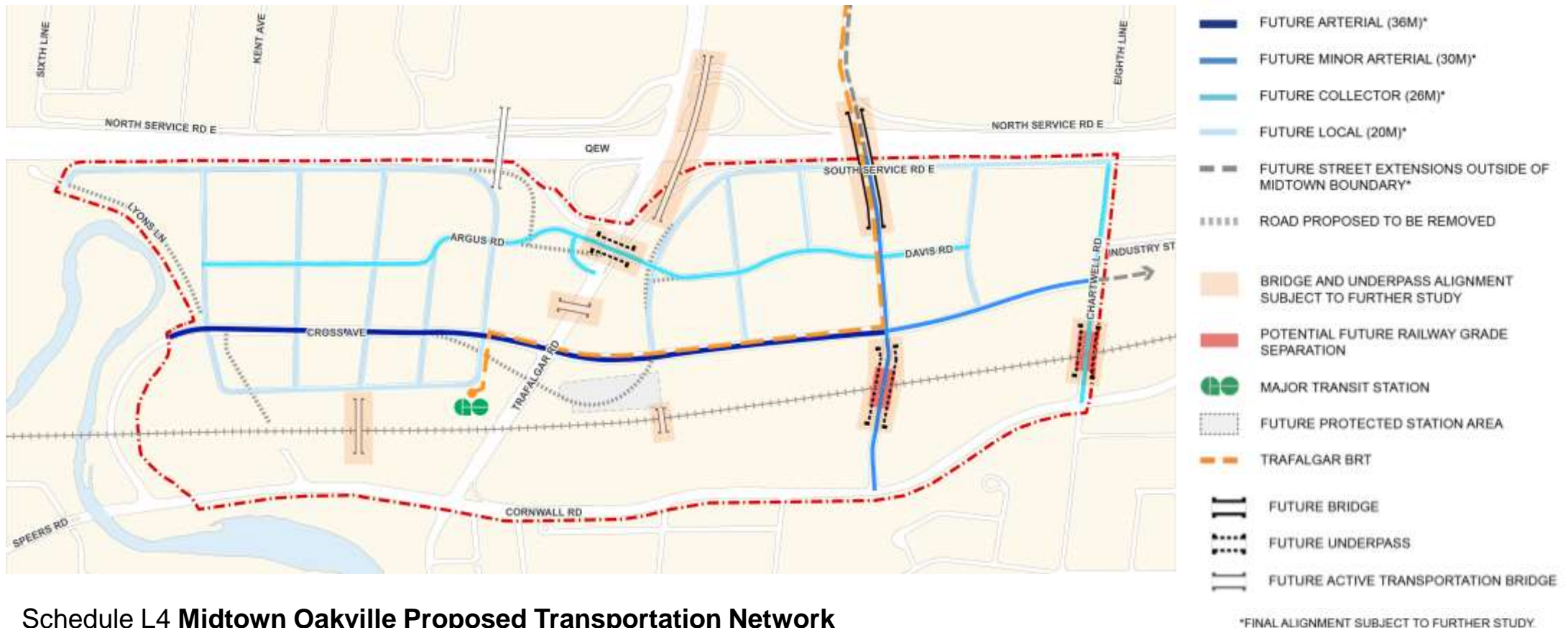
- **Parks and Open Space** reflect the Town's Parks Plan, and shall be **further complemented** by active transportation connections and privately-owned publicly accessible spaces (POPS)
- Overall parkland should be planned to achieve approximately **12 per cent of the gross area** of Midtown. This does **not** include POPS.
- The Midtown **Public Realm Master Plan** will add to the directions of the OPA, and define the role, function and character of parks.



Schedule L3 Midtown Oakville Open Space

Transportation Network

- Transportation policies prioritize year-round walking, cycling and transit, and create a fine-grained street network and improved transit to support mobility options throughout Midtown.



Schedule L4 Midtown Oakville Proposed Transportation Network

Active Transportation Network

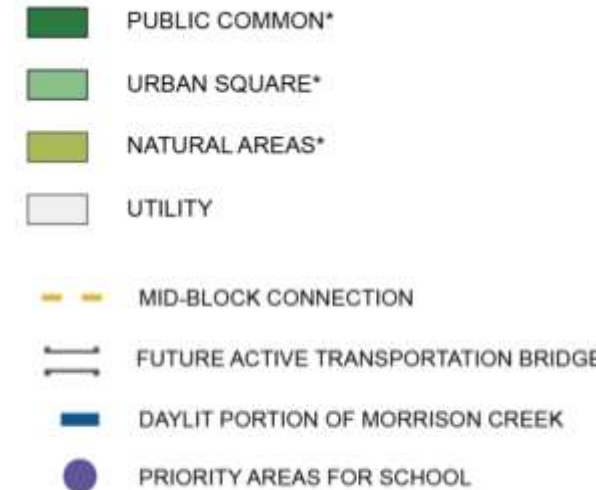
- Midtown will include active transportation routes and in addition to multi-modal mid-block connections to prioritize active transportation and pedestrian comfort throughout Midtown.



Schedule L5 Midtown Oakville Active Transportation

Public Service Facilities

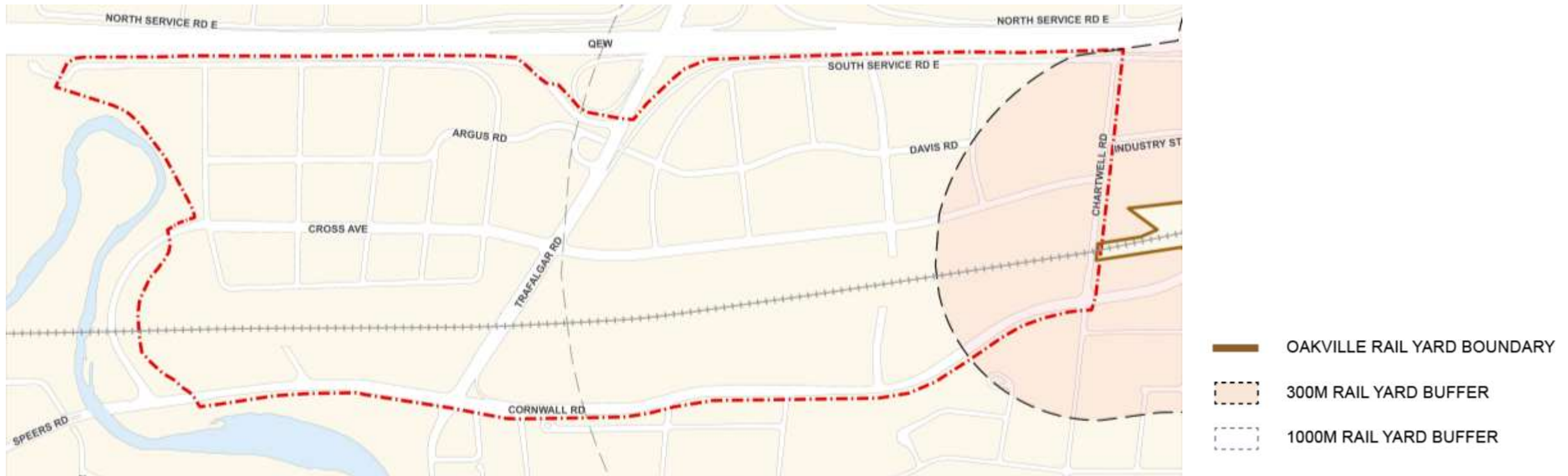
- The **Public Service Facilities** policies describes that social infrastructure (e.g. schools, social assistance, recreation, police and fire protection, health and educational programs, and cultural services) **will be coordinated with development and provided in Midtown**
- At least **four schools** may be located in Midtown, and will be encouraged to utilize strategies to optimize land use such as co-locating adjacent to parks (**2 priority school areas** shown below)



Schedule L3 Midtown Oakville Open Spaces

Transit Station, Rail, and Highways

- These policies provide guidance for development located in proximity to the transit station, rail, and highways.
- New or expanded residential uses are not permitted within 300m of the rail yard.



Schedule L9 Midtown Oakville Rail Facilities and Influence Areas

Infrastructure and Servicing

- **The Infrastructure and Servicing policies include guidance related to:**
 - **Stormwater management:** Development should include green infrastructure and low-impact development (LID) to address climate resiliency and improve water quality and biodiversity
 - **Hazardous Lands:** Development near hazardous lands (i.e. Sixteen Mile Creek) may be required to provide additional technical studies and consult with the Conservation Authority
 - **Wastewater and servicing:** Development must demonstrate there is sufficient existing and proposed water, sanitary sewer, and other utility infrastructure capacity to support needs

The Benefits of Green Infrastructure



WITHOUT



WITH



Good GI is integrated and multifunctional

Source: Meristem Design

Sustainability

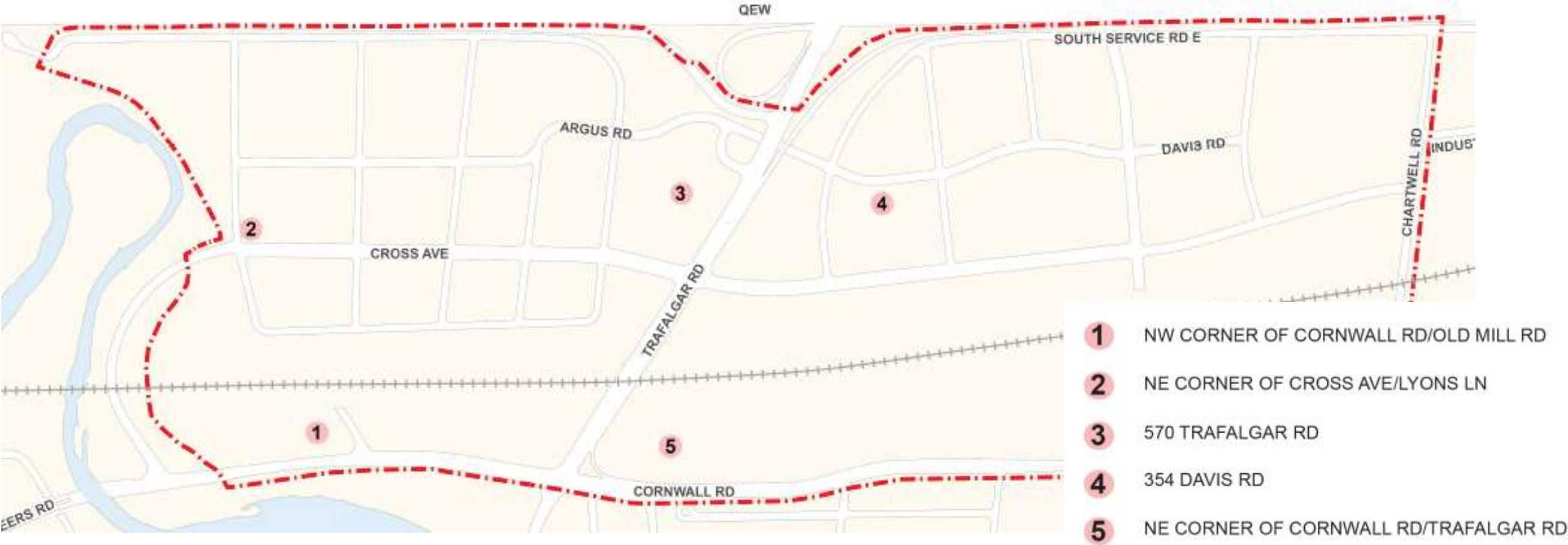
- The sustainability policies guide development to incorporate sustainable design practices, including **sustainable building practices** and **district energy policies**.
- Applications for development in the Midtown Oakville should submit a **Sustainable Development Report** describing how the development meet the sustainability policies of the OPA.



Native Child and Family Services of Toronto Roof Garden
Source: Scott Torrance Landscape Architect

Exceptions

- The exception section outlines site-specific policies of approved applications



Schedule L8 Midtown Oakville Exceptions

Implementation

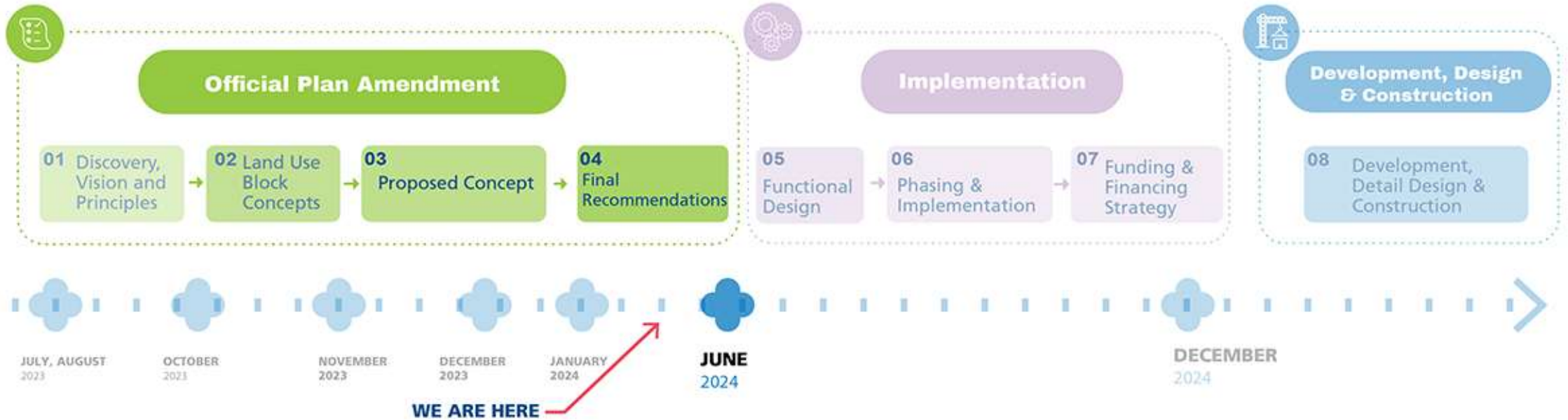
- **Implementation policies include policy direction on:**
 - **Town-led strategies, master plans, and implementation documents**
 - **Phasing / Transition:** Policies note infrastructure and social services must be coordinated with growth
 - **Area Design Plans:** Policies require applications to ensure blocks are comprehensively planned
 - **Monitoring:** Policies require the Town to track development in Midtown and ensure appropriate infrastructure and transportation improvements are occurring
 - **Landowner's Agreement / Cost Sharing:** Requires local landowners to enter into an agreement to ensure a fair distribution of the cost of delivering **parkland, infrastructure, parking, and servicing**

Other sections of the Official Plan

- The OPA will additionally amend other sections of the OP:
 - **Section 28.15 – Development Permits:**
 - Enabling policies have been added to allow the Town to initiate and adopt a Community Planning By-law (aka Development Permit System)
 - On March 18, 2024 Council directed staff to investigate and, if deemed appropriate, implement a Community Planning By-law for Midtown
 - **Section 29 – Glossary:**
 - Addition of a definition for the following building forms:
 - Low-rise: up to and including 6 storeys
 - Mid -rise: 7 to 12 storeys
 - High-rise: 13 storeys and greater

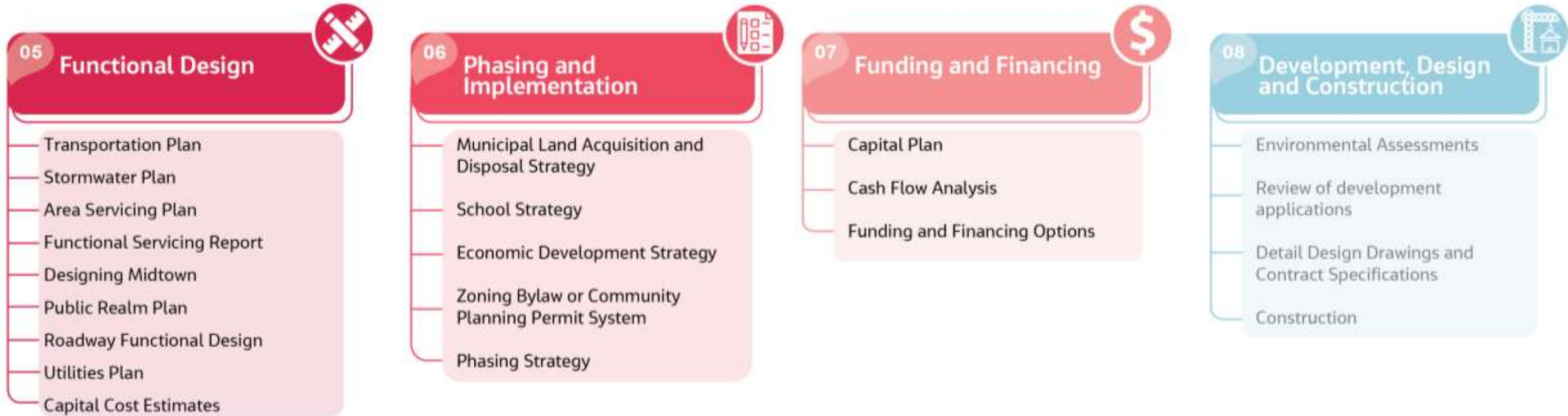
5. Process Forward

OPA Next Steps



- Additional refinements based on feedback from all parties (stakeholders and public), feedback can be shared with townclerk@oakville.ca
- June 24th P&D - Final recommendations to Council and recommended OPA

Implementation Program



Continued Engagement Program (To be developed)

- Information will be shared on technical studies as they are completed.
- We'll be sharing details in a few weeks on the next public engagement (PIC#4) on public realm, transportation and stormwater tentatively being planned for June.

December
2024