



OAKVILLE

REPORT

Planning and Development Council

Meeting Date: June 27, 2022

FROM: Planning Services Department

DATE: June 14, 2022

SUBJECT: Recommendation Report – Site Plan Applications
SP.1601.028/01 – 560, 570 and 580 Winston Churchill Blvd. and
SP.1601.029/01 – 700 and 750 Winston Churchill Blvd.

LOCATION: 560, 570, 580, 700 and 750 Winston Churchill Blvd. (formerly 772
and 560 Winston Churchill Blvd.)

WARD: Ward 3

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RECOMMENDATION:

1. That the revised Site Plan application, File No.: SP.1601.028/01 to allow the construction of three warehouse buildings be approved, subject to the conditions in Appendix “A” contained within the June 14, 2022, report from the Planning Services Department.
2. That the revised Site Plan application, File No.: SP.1601.029/01 to allow the construction of two warehouse buildings be approved, subject to the conditions in Appendix “B” contained within the June 14, 2022, report from the Planning Services Department.
3. That the Director of Planning Services or designate be authorized to grant final site plan approval to the revised Site Plan application for 560, 570 and 580 Winston Churchill Blvd., File No.: SP.1601.028/01.
4. That the Director of Planning Services or designate be authorized to grant final site plan approval to the revised Site Plan application for 700 and 750 Winston Churchill Blvd., File No.: SP.1601.029/01.
5. That staff be delegated authority to transfer to the Region the easement required for the Region’s preferred servicing strategy on terms satisfactory to the Town Solicitor including the applicant/landowner compensating the Town for the market value of the proposed easement as determined through the Town’s appraisal and reimbursing the Town for the associated costs.

KEY FACTS:

The following are key points for consideration with respect to this report:

- The purpose of this report is to obtain Council's endorsement with respect to two site plan applications (SP.1601.028/01 and SP.1601.029/01) at 560, 570, 580 Winston Churchill Blvd. and 700 and 750 Winston Churchill Blvd.
- The subject properties are located on the west side of Winston Churchill Blvd. and south of the CN railway.
- Site Plan application SP.1601.028/01 for 560, 570 and 580 Winston Churchill Blvd. was submitted in July 2019.
- Site Plan application SP.1601.029/01 for 700 and 750 Winston Churchill Blvd. was submitted in June 2021.
- Pursuant to Section 9 of Site Plan Control By-law 2019-114, notwithstanding the delegation of authority to approve site plans, the Director may present site plans to Council at any time prior to final approval and shall do so if requested by Town Council.
- On February 28, 2022, Council passed a motion requiring the Director of Planning to refer both Site Plan applications to Council for review and consideration.
- On June 9, 2022, the Planning Department received a commitment from the applicants to provide various benefits for the community, which is included within Appendix "I".
- Staff recommend approval of the revised site plan applications subject to the conditions within Appendices "A" and "B".

BACKGROUND:

Ontario Planning Legislation - Nature of Site Plan Authority

Site plans applications may be required under section 41 of the *Planning Act* and provide municipalities with an opportunity to review and approve plans and drawings related to a proposed development. These plans and drawings show the proposed location and design of buildings, parking, landscaping and other facilities.

The general purpose of the site plan process is to manage implementation details related to a development prior to the issuance of building permits or the commencement of site works. These details generally relate to technical matters such as grading, drainage, tree preservation, landscaping, external design of buildings, the location of garbage facilities, and vehicular and pedestrian traffic movement, both within the site and in areas where there are connections to public roads and sidewalks.

Section 41 - Subsection (4) - The site plan approval process involves the review of detailed drawings which illustrate the physical arrangement and layout on the property such as the location of buildings, driveways, parking areas, pedestrian sidewalks, landscaping, fences, light fixtures, waste collection areas, snow storage, stormwater drains, municipal services and similar facilities. Municipal staff and

commenting agencies review the drawings to ensure they comply with relevant development standards, regulations and policies. Unlike zoning by-laws or similar applications, there is no ability to prevent a development through the “refusal” of an application. Instead, revisions are made to the plans and drawings until approval is achieved. If the proposal cannot meet the town’s standards, the application may be appealed to the Ontario Land Tribunal (OLT) to determine the plans' details, drawings and conditions.

It should be noted that changes to matters that are regulated by the zoning by-law (such as use, height, setbacks, the number of permitted parking spaces etc.) are beyond the scope of the changes that can be sought through the site plan process.

The type of conditions that can be imposed on a site plan approval are limited to the matters set out in section 41(7) of the *Planning Act* which includes matters related to:

1. Road widenings – as required by the Official Plan of Town/Region
2. Access to/from Roads – curbs, access ramps, traffic direction signs
3. Loading and Parking Facilities – covered or uncovered, driveways, surfaces
4. Walkways / Pedestrian Access
5. Accessibility for People with Disabilities
6. Lighting
7. Landscaping – including walls, fences, hedges, trees, shrubs or other groundcover, for the landscaping of the lands or the protection of adjoining lands
8. Garbage/Waste – vaults, central storage, collection areas, enclosures
9. Municipal Easements – related to watercourses, ditches, land drainage works, sanitary sewage facilities and other public utilities of the municipality or local board
10. Grading and Drainage – including disposal of storm, surface, and waste water

Since site plan approvals deal with the provision of facilities, rather than the **use** of those facilities, conditions related to operational matters such as restrictions on the hours of operation, construction phasing and prospective tenants are beyond the scope of the site plan authority.

Although many of the comments received from the public ask Council to refuse the application and stop the proposed development that type of action is simply beyond the scope of site plan authority.

Appeal to the Ontario Land Tribunal (OLT)

Section 41 – Subsection (12) of the *Planning Act*, sets out the rights of appeal for site plan applications. Subsection 12 states that only the applicant/landowner can appeal the municipality's lack of a decision after 30 days from the date of submission of the application. The applicant/landowner can also appeal the conditions imposed by the municipality.

The application at 560, 570 and 580 Winston Churchill Blvd. (formerly 560 Winston Churchill Blvd.) was received on July 11, 2019.

The application at 700 and 750 Winston Churchill Blvd. (formerly 772 Winston Churchill Blvd.) was received on June 29, 2021.

To date, neither application has been appealed to the OLT, although both are eligible to file an appeal.

If an appeal is received, the Tribunal would determine the details of the drawings/conditions that will be approved as opposed to refusing a development. Unless the appeal is withdrawn, the OLT would make the final decision on the application instead of the town.

Third parties (such as abutting neighbours) have no right of appeal for a site plan application under the *Planning Act* since the application is not proposing a change in land use, only the technical implementation of what is already permitted in the town's Zoning By-law.

Site Plan Control

The Livable Oakville Plan Section 28.11 states that the entire plan area is subject to site plan control and allows the Town, by by-law to identify specific types of development subject to site plan control or exempt from site plan control. The proposed development is subject to site plan control.

The Town may also require the landowner to fulfill conditions of site plan approval and enter into agreements.

Approval of a Site Plan Application

Under the Town's Site Plan Control By-law 2019-114, the Director of Planning Services is given the authority to approve site plans. However, section 9 of By-law 2019-114 allows the option for the Planning Director to present the site plans to Council at any time before final approval and shall do so if specifically requested by Town Council.

On February 28, 2022, Council passed the following resolution:

“WHEREAS Site Plan Control By-law 2019-114 provides that at the request of Town Council the Director of Planning shall present site plans to Council at any time prior to final approval; and

WHEREAS Site Plan applications for 560 and 772 Winston Churchill Blvd. border residential land uses, must be comprehensively planned and address matters outlined in s.41 (4) and (7) of the Planning Act including the D6 Guidelines related to Land Use Compatibility.

THEREFORE BE IT RESOLVED, THAT the Director of Planning be requested to refer these Site Plans to Council for consideration and review.”

On June 9, 2022, the Planning Department received a commitment from the applicants to provide various benefits for the community, which is included within Appendix “I”. This offer was contingent on Planning staff bringing forward a staff report for Council’s consideration by June 27, 2022.

Effective July 1, 2022, as a result of amendments to the *Planning Act* introduced by Bill 109, decisions related to site plan matters must be made by an officer, employee or agent of the municipality as the “authorized person”, rather than Council.

Proposal

The subject lands are located on the west side of Winston Churchill Blvd, south of the CN rail line and east of Town-owned Aspen Forest Park (Figure 1).



Figure 1: Location

SP.1601.028/01 – 560, 570 and 580 Winston Churchill Blvd. (formerly known as 560 Winston Churchill Blvd.)

The subject site plan application was received in July 2019, to permit the construction of two warehouse buildings with a total of 58,655 m² of floor area. The application was subsequently revised in March 2021 and December 2021.

The current revision (December 2021) is to permit the construction of three warehouse buildings with a total floor area of 59,078.87 m². The proposal can be further described as follows (Figure 2):

- Building A – 16,205.91 m² – height – 11 m
- Building B – 12,790.65 m² – height – 11 m
- Building C – 30,082.31 m² – height – 13.49 m
- 399 parking spaces
- 20 bike parking spaces
- 110 loading spaces
- The loading areas are located in the centre of the site and as such the buildings act as a noise barrier to the residential uses.
- Truck traffic will be restricted to the central driveway.
- Passenger vehicles will have access to the southern driveway.

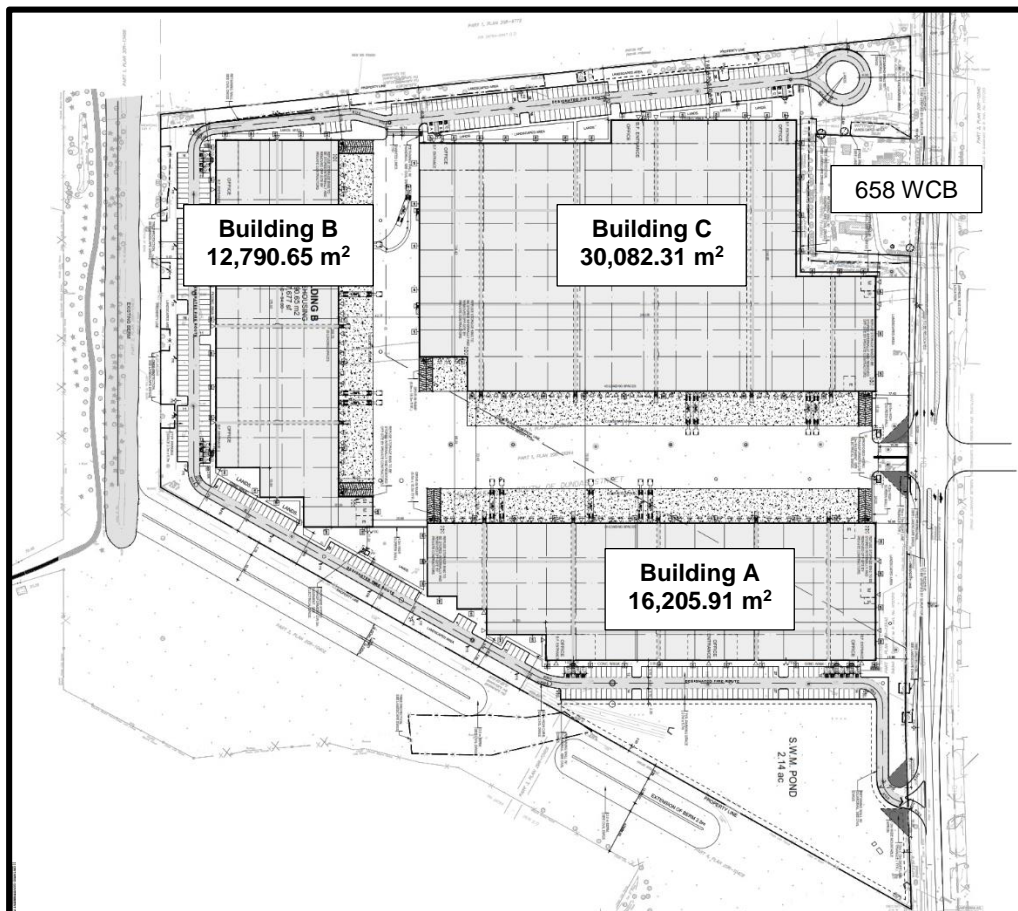


Figure 2: 560, 570, and 580 Winston Churchill Site Plan (Dec 2022)

The following studies, including revised materials and supporting documentation, are also accessible on the town's website <https://www.oakville.ca/business/33916.html>.

- Site Plan

- Elevations and Roof Plan
- Landscape Plans/Details
- Tree Protection Plans
- Canopy Coverage Plan
- Site Grading Plans
- Site Servicing and stormwater management Plans
- Erosion and Sediment Control Plan
- Lighting Plan
- Arborist Report
- Transportation Impact Study
- Stormwater Management and Functional Servicing Report
- Noise Feasibility Study
- Land Use Compatibility Report
- Significant woodlands Assessment

Site Description

The subject property is approximately 12.9 ha in size and is currently vacant.

The surrounding land uses can be described as follows:

North: Clearview watercourse and land subject to site plan application SP.1601.029/01 for two warehouse buildings.

East: Residential use (658 Winston Churchill Blvd.) and employment uses within the City of Mississauga.

South: Aspen Forest Park beyond which are residential uses.

West: Aspen Forest Park including a landscaped berm beyond which are residential uses.

Figure 3 illustrates the location of the proposed buildings with setbacks to Aspen Forest Park and the surrounding residential uses.

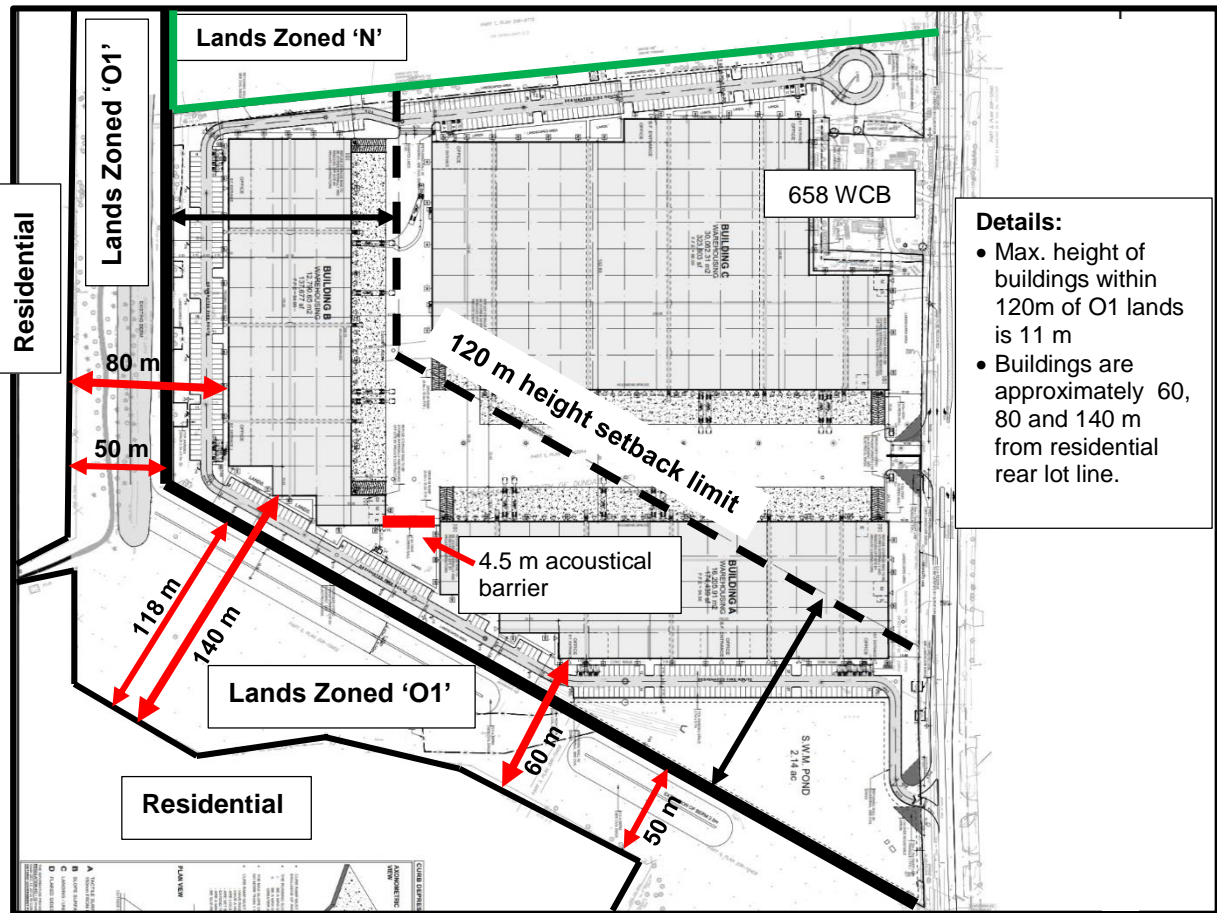


Figure 3: Separation distances between proposed buildings and lands zoned for residential uses

Heritage Resources – 658 Winston Churchill Blvd.:

The subject property (560/570/580 WCB) is located directly adjacent to the property at 658 Winston Churchill Boulevard which is listed on the Oakville Register of Properties of Cultural Heritage or Interest. This property has also been identified as having potential as a cultural heritage landscape.

In order to protect the cultural heritage character of the property at 658 Winston Churchill Boulevard, a landscape buffer is required around the north, west and south boundaries of this lot. The landscape buffer will need to include a sufficient amount of plantings to provide a ‘wall’ of greenery that offers the heritage property visual and physical separation from the subject property. The goal must be to retain the rural setting of the heritage property as much as possible by screening the new development from it. Heritage staff have no concerns with the application as revised.

It is noted that this property has also maintained the employment zoning as the abutting developments.

SP.1601.029/01 – 700 and 750 Winston Churchill Blvd. (formerly known as 772
Winston Churchill Blvd.)

The subject site plan application was received in June 2021, to permit the construction of two warehouse buildings with a total of 61,462 m² of floor area. The application was subsequently revised in January 2022.

The current revision (January 2022) is to permit the construction of two warehouse buildings with a total floor area of 60,108.45 m². The proposal can be further described as follows (Figure 4):

- Building A – 30,905.87 m² – height - 13.19 m
- Building B – 29,202.58 m² – height – 11.00 m
- 340 parking spaces
- 18 bike parking spaces
- 116 loading spaces
- The loading areas are located in the centre of the site and as such the buildings act as a noise barrier to the residential uses.
- The existing channel (zoned N-Natural) provides an additional buffer to the residential uses to the west.

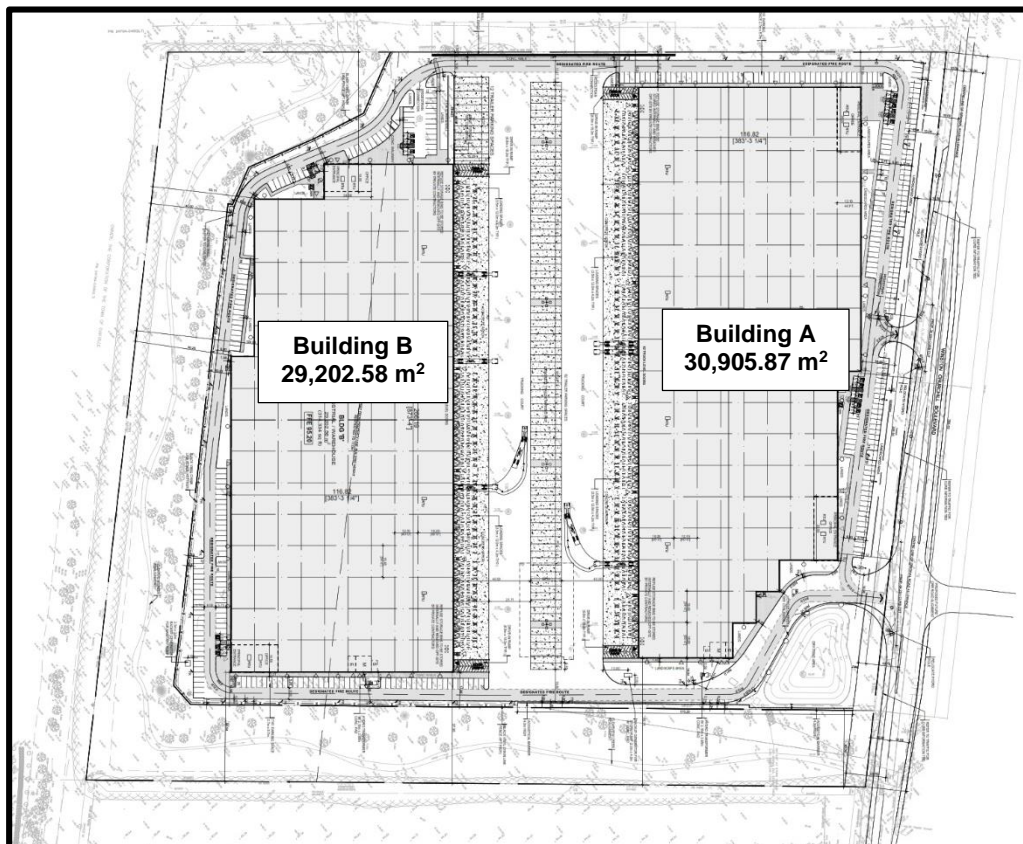


Figure 4: 700 and 750 Winston Churchill Site Plan (Jan 2022)

The following studies, including revised materials and supporting documentation, are also accessible on the town's website <https://www.oakville.ca/business/sp-38512.html>.

- Site Plan
- Elevations and Roof Plan
- Landscape Plans/Details
- Tree Protection Plans
- Canopy Coverage Plan
- Site Grading Plans
- Site Servicing and stormwater management Plans
- Erosion and Sediment Control Plan
- Lighting Plan
- Arborist Report
- Transportation Impact Study
- Stormwater Management and Functional Servicing Report
- Noise Feasibility Study
- Land Use Compatibility Report
- Environmental Site Assessment
- Fisheries Impact Assessment

Site Description

The subject property is approximately 15.5 ha in size and is currently vacant.

The surrounding land uses can be described as follows:

North: CN railway and employment uses.

East: Winston Churchill Blvd. and employment uses within the City of Mississauga.

South: Clearview watercourse and land subject to site plan application
SP.1601.028/01 for three warehouse buildings.

West: Clearview watercourse, Aspen Forest Park including a landscaped berm
beyond which are residential uses.

Figure 5 illustrates the location of the proposed buildings with setbacks to the Clearview watercourse, Aspen Forest Park and the surrounding residential uses.

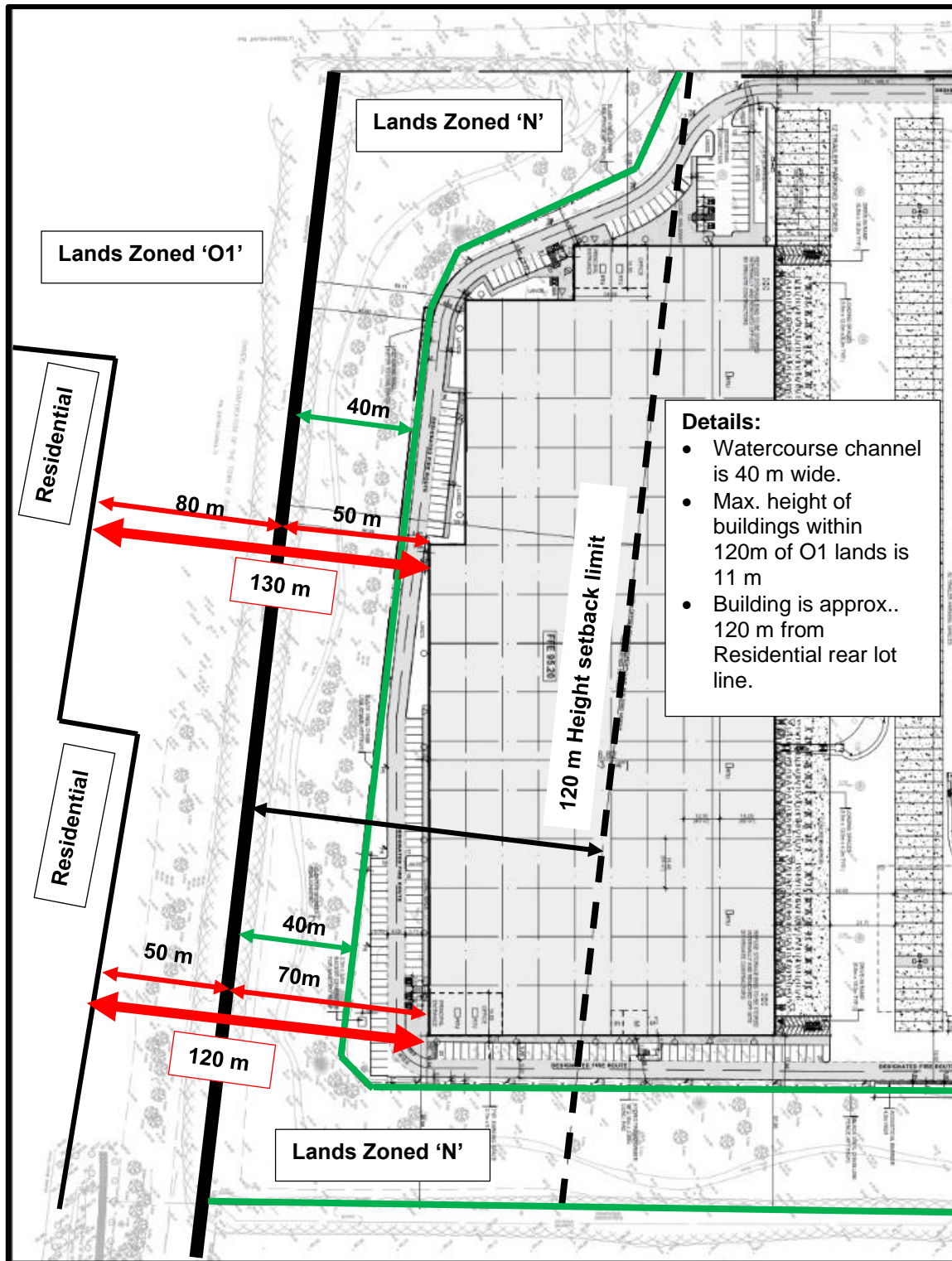


Figure 5: Separation distances between proposed buildings and lands zoned for residential uses

SP.1601.028/01 and SP.1601.029/01 Commitment Letter:

On June 9, 2022, the Planning Department received a joint commitment letter from WeirFoulds LLP and Borden Ladner Gervais the solicitors for both applicants outlining various commitments (Appendix “I”).

In summary, the landowners jointly commit to the following:

- A contribution of \$250,000 to be used to construct a trail within the lands to be conveyed to the Town between the existing Town owned lands and Winston Churchill Blvd., the construction of a splash pad within Aspen Forest Park and additional tree plantings;
- The finish of the buildings to be in a neutral palette;
- Following clauses to be included within the future site plan agreements:
 - “The Owner will undertake to insert a provision in any lease agreement with respect to a reduction in the usage of loading docks after 11:00 pm”
 - “The Owner will undertake not to occupy the buildings on the easterly portions of the Properties, which abut Winston Churchill Blvd. in advance of the commencement of the exterior construction of the buildings located on the westerly portion of the properties closest to the residentially zoned lands.”

These commitments are offered on the basis that a Planning report was brought forward for Council’s consideration at the June 27, 2022, Planning and Development Council Meeting.

PLANNING POLICY & ANALYSIS:

Provincially Significant Employment Zones

The Province has identified employment lands within the Greater Golden Horseshoe to be protected to provide “...*stable, reliable employment across the Region*”. The subject lands have been identified by the Province as part of a Provincially significant employment area (<https://www.ontario.ca/page/provincially-significant-employment-zones>).

Halton Region Official Plan

The subject lands are designated “Urban Area” in the Regional Official Plan (ROP). The Urban Area is “planned to accommodate the distribution of population and employment for the Region and the four Local Municipalities”. The policies of the Urban Area designation support a form of growth that is compact and supportive of transit, the development of vibrant and healthy mixed-use communities which afford maximum choices for residence, work and leisure. Policy 76 notes that the range of permitted uses is to be in accordance with Local Official Plans and Zoning By-laws. All development, however, is subject to the policies of the Regional Plan.

The subject lands are also located within an “Employment Area” (Regional Map 1).

The objectives of employment areas are:

- (1) *“To ensure the availability of sufficient land for employment to accommodate forecasted growth to support Halton’s and its Local Municipalities’ economic competitiveness.*
- (2) *To provide, in conjunction with those employment uses within the residential and mixed use areas of the communities, opportunities for a fully-diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses.*
- (3) *To locate Employment Areas in the vicinity of existing major highway interchanges and rail yards, where appropriate, within the Urban Area.”*

The Region of Halton has no objection to the approval of the applications subject to conditions of approval. Region of Halton comments dated May 25, 2022, are included as Appendices “C” and “D”.

Livable Oakville Plan

The Town’s Official Plan provides a framework for how land in the community should be used/developed. It is the long-term vision of the community. The guiding principles for the Town’s Livable Oakville Plan are to preserve and create a liveable community in order to:

- a) preserve, enhance, and protect the distinct character, cultural heritage, living environment, and sense of community of neighbourhoods;*
- b) direct the majority of growth to identified locations where higher density, transit and pedestrian oriented development can be accommodated; and,*
- c) achieve long term economic security within an environment that offers a diverse range of employment opportunities for residents.*

The subject lands are designated as *Business Employment* on Schedule G – South East Land Use (Figure 6). Lands designated as *Business Employment* are intended to provide a wide range of businesses and industrial uses within enclosed buildings and provide for office uses and light and service industrial operations with minimal impacts on the surrounding areas. Limited outdoor storage and display areas within lands designated *Business Employment* shall be adequately screened.

A portion of 700/750 is designated “Natural Area” which contains Clearview Creek.

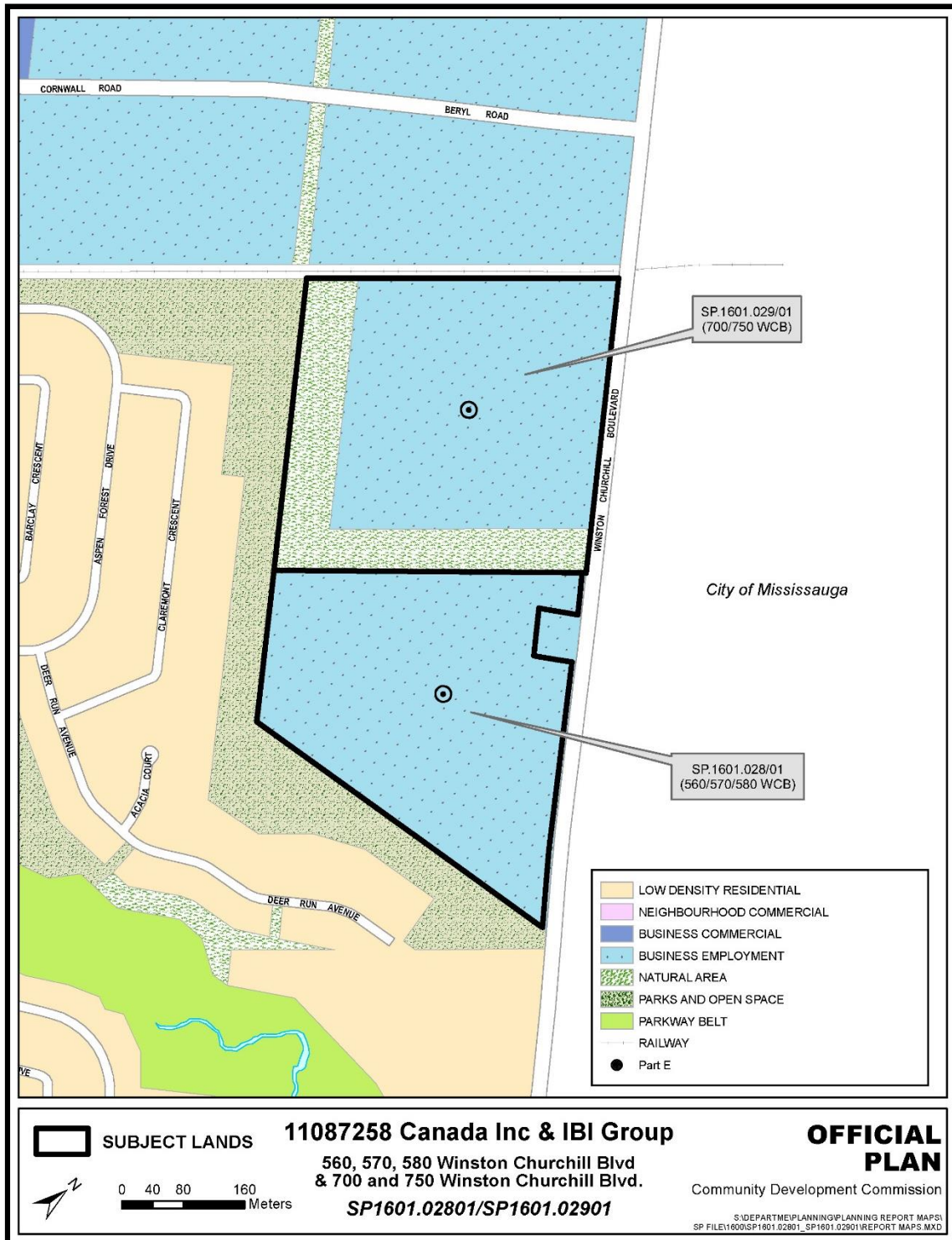


Figure 6: Official Plan - Schedule G - extract

Permitted uses for lands designated as *Business Employment* include:

a) *offices and light industrial uses such as manufacturing, assembling, processing, fabricating, repairing, warehousing and wholesaling. Hotels, public halls, indoor sports facilities, and training facilities and commercial schools may also be permitted.*

d) *New service commercial uses, limited to restaurants, financial institutions and drive-through facilities, may also be permitted on the same lot as uses listed in subsection a), above.*

Accessory uses may be permitted in conjunction with permitted light industrial uses. Accessory uses shall be on the same lot and clearly subordinate, and directly related, to the functions of the permitted use.

Policy 27.2.2 also states: “*On the lands designated Business Employment on the west side of Winston Churchill Boulevard, north of Deer Run Avenue and south of the railway spur line, outside storage may also be permitted provided that it is not adjacent to the open space corridor and is appropriately buffered from the Residential Area to the west.*”

Lands designated as *Business Employment* are intended to provide for a transition between more sensitive land uses (residential) and more ‘heavy’ industrial uses (east side of Winston Churchill Boulevard).

Both properties abut the former Hydro lands which are designated as *Parks and Open Space*. The *Parks and Open Space* lands provide an additional buffer between the residential community and the *Business Employment* lands.

Neither the Town’s Official Plan or Zoning By-law, can regulate the types of goods stored in the warehouse or the end users/tenants.

Both applications for warehouse uses comply with the Town’s Livable Oakville Plan.

Zoning By-law

Zoning By-laws regulate such matters as the use of land, where buildings/structures can be located on the site, lot sizes/dimensions, parking requirements, building heights and setbacks among others. The Town’s Zoning by-law cannot regulate the tenants or operations. The applications before the Town are not to change the zoning on the land, but rather to review the detailed drawings that illustrate the properties’ physical arrangement and layout for a use that is currently permitted.

The subject lands have been zoned for industrial/employment uses for several decades. The Town’s former by-law 1965-136 zoned the lands fronting onto Winston Churchill Blvd as *M1 – Light industrial* (Figure 7) which permitted

warehousing and storage within enclosed buildings and assembly of manufactured products. The Hydro corridor was also zoned as *M1- Light Industrial*.

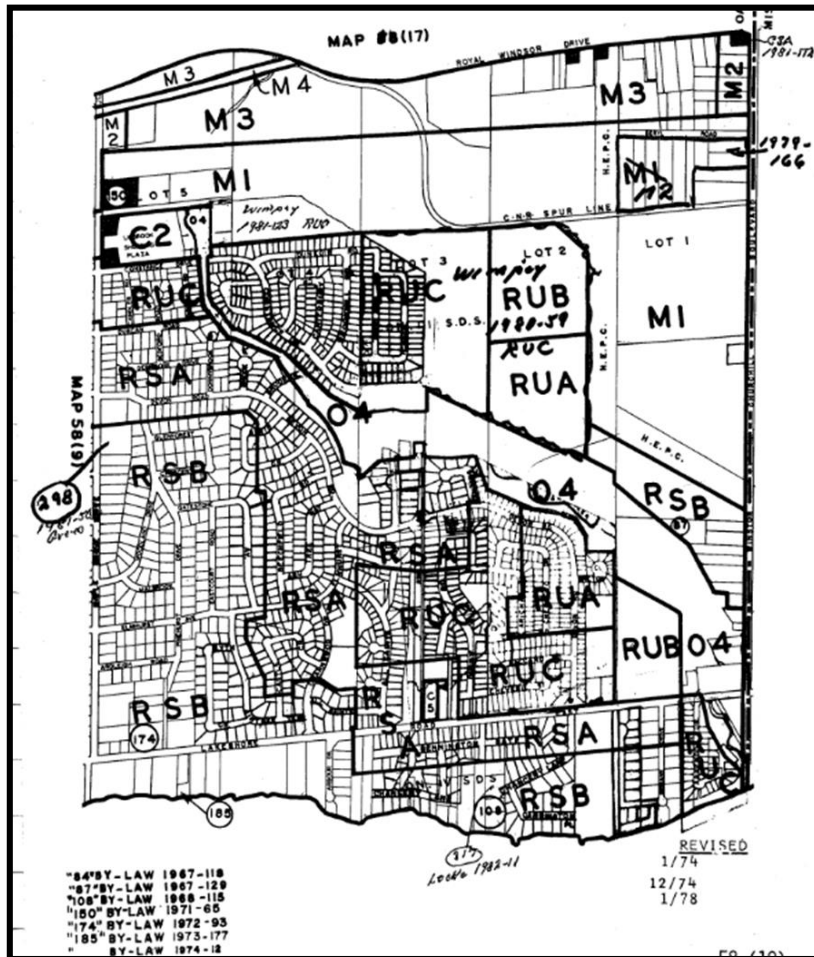


Figure 7: 1965 Zoning By-law Extract – consolidated in 1978

The Town's former Zoning By-law (1984-063) zoned these lands as "E1" and although the by-law did not list "warehousing" as a separately defined permitted use on these lands, "Light Industrial Operations" was permitted and were defined as:

- a) Light manufacturing, fabricating, processing, repair, servicing, packaging and warehousing of products or materials; and
- b) Propane transfer facility for the use of the owner or tenant of the lands"

Based on the above definition, "warehousing" was a permitted use.

Currently, the property at 560, 570 and 580 Winston Churchill Blvd. is zoned E2 - Business Employment subject to special provision 201. The property at 700 and 750 Winston Churchill Blvd. is zoned E2 – Business Employment subject to special

provision 201 and *N – Natural Area*. The *Natural Area* zone reflects the existing realigned channel (Figure 8). This area is intended to remain within the property.

To provide an additional buffer to the residential community to the west and south, the former Hydro corridor was rezoned from *M1 – Light Industrial* to *O1 – Park* and is under the ownership of the Town.

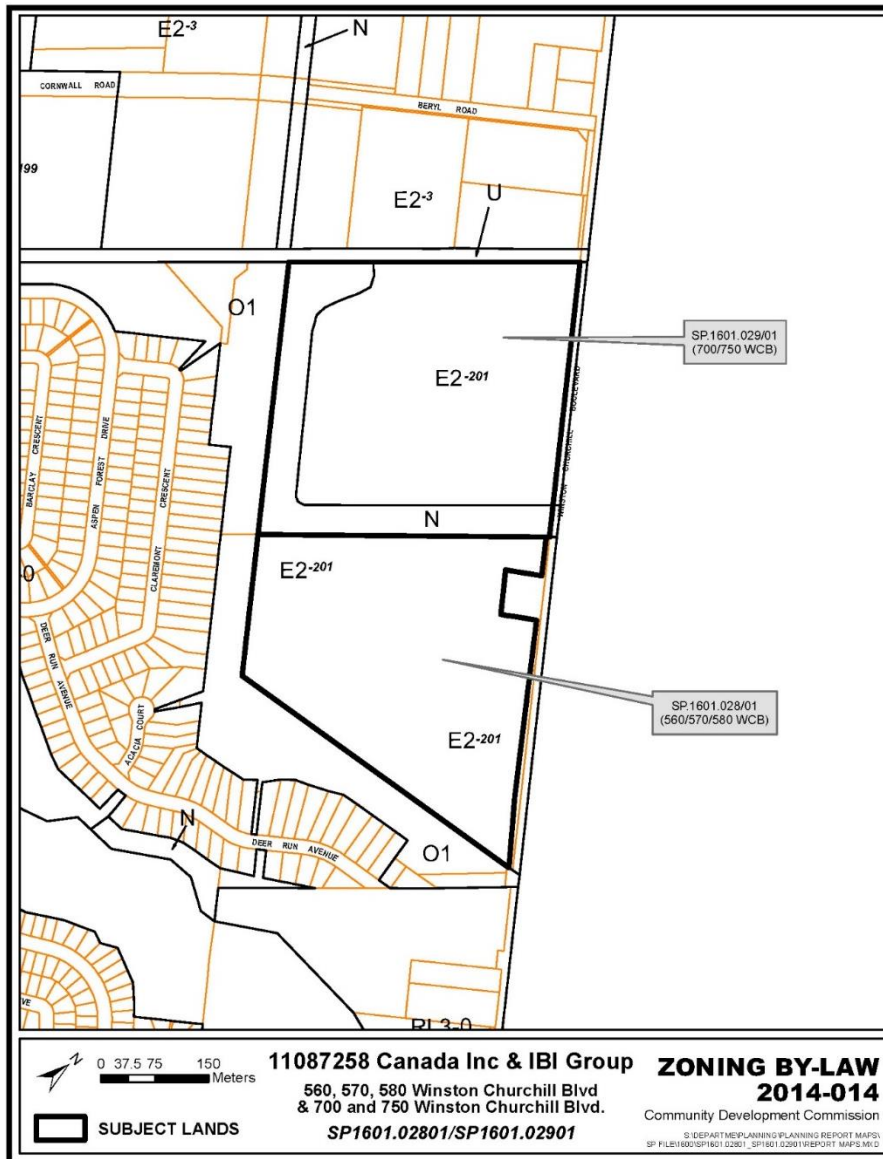


Figure 8: Existing Zoning By-law

The as-of-right permitted uses within the *E2 - Business Employment* zone include:

<ul style="list-style-type: none">• art gallery• commercial school• conservation use• daycare• dry cleaning/laundry establishment• financial institution• food production• manufacturing• outside display and sales area• public works yard• repair shop• retail store - accessory and showroom• sports facility• taxi dispatch• veterinary clinic• training facility• stormwater management facility• warehousing	<ul style="list-style-type: none">• business office• commercial self-storage• contractors establishment• drive-through facility• emergency service facility• foodbank• hotel• medical office• outside storage associated with a permitted use• parking area - heavy vehicle associated with a permitted use• public hall• rental establishment• restaurant• private school• wholesaling
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Vehicle storage is permitted in conjunction with a permitted use.

Within the parent E2 zone category, there are no maximum height or maximum floor area regulations. However, Special Provision 201 (Figure 9) was applied on these lands and included additional regulations that are intended to minimize the impact of as-of-right employment uses on the residential properties in the vicinity.

As mentioned earlier, warehousing and heavy vehicle parking are permitted uses within the E2 zone and given the nature of warehousing operations, it is not unusual for tractor-trailers to be parked on a site. The inclusion of site-specific zoning (Special Provision 201) on these lands was to minimize the impact on the residents by restricting the location of loading areas, restricting the location of outdoor storage and applying a maximum height when a maximum is not included as part of the standard E2 zone regulations.

Special Provision 201 can be summarized as follows:

- a height maximum of 11 m for any buildings within 120 m of the Park zone (O1) whereas the parent E2 zone does not include a maximum height.

- does not permit loading docks to be located between a building and the Park zone (O1) (*Note: the purpose of this regulation allows the building to act as a noise barrier for the loading areas*)
- restricts the location of as-of-right outdoor storage, whereas the parent E2 zone does not provide a restriction

Since Special Provision 201 does not permit the loading docks between the building and the O1 zone the loading areas will be screened/hidden from view from the abutting residential uses.


201	560-772 Winston Churchill Boulevard	Parent Zone: E2
Map 19(10)	(Part of Lot 1, Concession 3 S.D.S.)	(2002-189) (2006-002) (2008-074) PL140317
15.201.1 Zone Provisions		
The following regulations apply:		
a)	Maximum <i>height</i> within 120.0 metres of the Park (O1) Zone	11.0 m
b)	A loading dock shall not be located between a <i>building</i> and any <i>lot line</i> abutting the Park (O1) Zone.	
c)	All lands identified as Block 1 in Figure 15.201.1, below, shall be considered as one <i>lot</i> for the purposes of applying footnote 6 of Table 10.2, related to maximum <i>net floor area</i> for applicable uses.	
15.201.2 Special Provisions for Outside Storage		
The following additional regulations apply for <i>outside storage</i> :		
a)	<i>Outside storage</i> is permitted anywhere on a <i>lot</i> except within 120 metres of the Park (O1) Zone, in a <i>front yard</i> , or any <i>yard</i> abutting Winston Churchill Boulevard.	
b)	Notwithstanding subsection (a) above, <i>outside storage</i> is not permitted within 80.0 metres of the Park (O1) Zone on lands within 150.0 metres south of the northwest limit of lands subject to this Special Provision.	
c)	<i>Outside storage</i> exceeding 1.8 metres in height is permitted only in a rear yard.	
d)	<i>Outside storage</i> shall not exceed the <i>height</i> of the <i>building</i> .	
15.201.3 Special Site Figures		
Figure 15.201.1		
Special Provision 201		
		

Figure 9: Special Provision 201

Both applications have been reviewed against the in-effect zoning by-law and comply with the E2 regulations and Special Provision 201, specifically with respect to height, location of loading doors, and setbacks.

Part 4 - General Provision - Section 4.14 *Municipal Services Required* states:

- a) *“On lands subject to this By-law south of Dundas Street, no building may be erected or enlarged unless the land is serviced by municipal water and sewage systems.”*

Servicing of these lands is discussed later within this report. However, final site plan approval will not be issued until the applicant designs and constructs the servicing to the lands. A condition of approval requires the owners to *“Provide confirmation that the final Approved Site Plan complies with By-law 2014-014, as amended, specifically with respect to Section 4.14 a).”*

ABUTTING MUNICIPALITY/REGION OF PEEL/CONSERVATION AUTHORITY:

Region of Peel

The site plan applications were circulated to the Region of Peel for comments. The Region of Peel has no objections to the approval of these applications, subject to conditions of approval. Region of Peel comments dated May 2, 2022, are included as Appendices “E” and “F”. Region of Peel conditions of approval are included within Appendices “A” and “B”.

City of Mississauga

The site plan applications were also circulated to the City of Mississauga for comments. The following email correspondence was received.

SP.1601.028/01:

“The City of Mississauga has no concerns with this application but our Transportation & Works department note to consult the Region of Peel as Winston Churchill Boulevard (Lakeshore Road West to Dundas Street) falls under the jurisdiction of the Region of Peel. They also noted that the applicant should also mitigate any flooding issues as dictated by CVC.”

SP.1601.029/01:

“We understand this is a SP application within the current zoning permissions. Accordingly, the City of Mississauga will not be providing comment.”

Credit Valley Conservation Authority (CVC)

The site plan applications were circulated to CVC for comments. CVC has no objections to the approval of these applications, subject to conditions of approval. CVC conditions of approval are included within Appendices “A” and “B”.

TECHNICAL & PUBLIC COMMENTS:

Through the review of the applications, the residents have raised various concerns, some of which can be reviewed under section 41 of the *Planning Act* and other concerns that are outside of the scope of a site plan application (i.e. operational issues). Below is a summary of the concerns raised to date.

Traffic Concerns

As part of any site plan application submission, the Town may request an applicant to prepare a Transportation Impact Study. A Transportation Impact study evaluates the effects a proposed development is expected to have on an existing road system and is based on established transportation planning and traffic engineering principles.

These principles include but are not limited to:

- study assumptions such as horizon years and analysis periods, traffic data collections;
- existing conditions such as a site and area description, study area description, road network, traffic volumes, transit service, bicycle and pedestrian facilities, traffic intersection operations;
- future background traffic conditions including planned road network improvements, existing traffic, background traffic growth, background development traffic, future background intersection operations;
- details regarding the proposed development including a site concept plan, site access;
- future total conditions with development including vehicular trip generation, trip distribution, future total traffic intersection operations, queuing analysis, etc.;
- site traffic impacts;
- preliminary design review; and
- conclusions and recommendations

The scope or terms of reference for a Traffic Impact Study is usually discussed with the Town's transportation engineers and Regional transportation engineers if the site abuts a Regional road. The terms of reference for a typical traffic study are outlined in the Halton Region TIA Guidelines. These studies are reflective of what is forecasted based on the methodology used in North America for the proposed developments' trip generation. The methodology is a combination of different land uses with trip generation rates based on surveys collected by the Institute of Transportation Engineers (ITE). The total future traffic volumes are determined by adding the site-generated traffic to future background traffic, which is the sum of existing background traffic factored up for growth and traffic generated by other

known area developments and in this case, this would also include known developments in Mississauga, east of Winston Churchill Boulevard.

The future traffic level of service is determined by calculating volume to capacity ratios for all movements at key intersections. Capacity is a theoretical measure of the amount of volume an intersection can process in a given time frame based on the future lane configurations and signal timing. The future volume is compared to the capacity to calculate a volume to capacity ratio. Typically any ratio < 0.90 is acceptable, anything 0.90 and greater is considered critical and requires mitigation measures for improvement. These improvements may include but are not limited to recommendations to adjust signal timing plans, upgrading traffic control devices, upgrading the road configurations and widening roadways or adjustments to the location of site access points. Traffic concerns identified through site plan do not provide an opportunity for preventing the development through a refusal of the application.

The Town requested the submission of Transportation Impact Studies (TIS) as part of the two site plan applications under consideration. The transportation reports were circulated to the Town's Transportation engineers, the Transportation engineers of the Region's of Halton and Peel, the City of Mississauga and the Ministry of Transportation for review and comment. In addition, the TIS was reviewed by the Town's peer reviewer (Paradigm).

The following reports were submitted in support of the applications:

SP.1601.028/01:

- Transportation Impact Study dated November 2021, and prepared by C.F. Crozier & Associates.

SP.1601.029/01:

- Transportation Impact Study dated September 2021, and prepared by IBI Group.

Peer Review Conclusions and Recommendations:

SP.1601.028/01:

The peer reviewer concludes that the study follows a typical technical approach and does well to address the key concern of traffic operations on the road network surrounding the subject site. In addition the following conclusions were identified:

Traffic Operations – *“A number of vehicle movements and study intersections have been identified to have poor measures of effectiveness (MOEs) during the AM*

and/or PM peak hour over various study scenarios. The cause of the critical conditions is largely attributable to background conditions, not site trips. Recommendations for traffic signal timing optimization and monitoring of operations have been recommended in the study.”

Signal Timing – “Some signal timings could be adjusted to reflect study conditions. Updates or clarification on reduced pedestrian walk times and lost time adjustment factors could help confirm study details, but may not impact study results to a significant extent.”

Transportation Demand Management – “A review of TDM aspects indicates single-occupant vehicle trips generated by the site could be decreased through a number of initiatives. Further efforts could be investigated to reduce single-occupant vehicle trips generated by the site. Efforts could include the installation or protection of right-of-way for future sidewalk, MUP [multi-use path] and/or bus stop needs. Strong connections (i.e. sidewalks) from the right-of-way to the site may be beneficial as well.”

Site Access – “There are no significant issues with the site accesses as proposed. A review of sight distances and other potential safety issues at the access could be provided in the study.”

Site Plan and Functional Design – “The site plan and functional design contain minor discrepancies related to transportation operations. It is likely that these minor issues will not impact future detailed design efforts to a significant extent.”

Parking – “The proposed parking supply is adequate to serve site operations.”

Site Circulation – “No vehicle movement conflicts were identified through the swept path analysis.”

“Updates to the identified issues can be made to better reflect existing and projected conditions. However, such updates would likely not impact the conclusions of the report to a significant extent. The following recommendations are suggested to ensure that project stakeholders can accommodate development plans:

- Monitor traffic signals for future signal timing optimization needs;*
- Explore opportunities to improve TDM efforts;*
- Provide an analysis of site access sightlines;*
- Clarify site trip assignment details;*
- Consider design aspects in future detailed design; and*
- Update other study details as needed.”*

SP.1601.029/01:

The peer reviewer concludes that the study follows a typical technical approach and does well to address the key concern of traffic operations on the road network surrounding the subject site. In addition the following conclusions were identified:

Traffic Operations – “The eastbound through/right turn movement at the intersection of Winston Churchill Boulevard and Royal Windsor Drive has been identified to have poor measures of effectiveness (MOEs) during the AM peak hour over various study scenarios. The cause of the critical conditions is largely attributable to background conditions, not site trips. The report contains traffic signal timing optimization analysis that mitigates the poor conditions.”

Signal Timing – “Some signal timings could be adjusted to reflect study conditions. It is not anticipated that updates to the signal timings will impact results to a significant extent.”

Transportation Demand Management – “A TDM section is not included in the study. A TDM section may wish to be included to reduce single-occupant vehicle trips generated by the site. Efforts could include the installation or protection of right-of-way along the site frontage for future sidewalk, MUP [multi-use path] and/or bus stop needs. Strong connections (i.e. sidewalks) from the right-of-way to the site may be beneficial as well.”

Site Access – “There are no significant issues with the site accesses as proposed.”

Site Plan – “The site plan contain minor discrepancies related to transportation operations. It is likely that these minor issues will not impact future detailed design efforts to a significant extent.”

Parking – “The proposed parking supply is adequate to serve site operations.”

Site Circulation – “The site circulation analysis indicates that there are no vehicle conflicts for the observed vehicle types and turns. The analysis does not include all vehicle types and potential turning conflict locations on site.”

“Updates to the identified issues can be made to better reflect existing and projected conditions. However, such updates would likely not impact the conclusions of the report to a significant extent. The following recommendations are suggested to ensure that project stakeholders can accommodate development plans:

- Monitor traffic signals for future signal timing optimization needs;*
- Explore opportunities to improve TDM efforts;*
- Clarify the setback distance for the site access sightlines;*
- Clarify site trip assignment details;*

-
- *Provide further site circulation details;*
 - *Consider design aspects in future detailed design; and*
 - *Update other study details as needed.”*

The Paradigm peer review reports can be found on the Town’s website.

Additional Clarification Required:

An issue has been raised concerning the noise generated by the truck traffic on the site if all the buildings on the two properties are operating at maximum capacity. In discussions with the Town’s peer reviewer they have indicated that it is very unlikely that the 226 loading bays on the site would be in use simultaneously (eg. trucks backing into spaces, coupling or uncoupling). However, Transportation and Engineering staff have asked the Town’s transportation peer reviewer to confirm the following:

1. Whether the number of truck trips entering and exiting the site as described within the Transportation Impact Studies seems reasonable given the proposed use (warehousing) and considering the 226 loading bays across both sites.
2. Confirmation that the Transportation Impact Studies accurately take into consideration the number of trucks within the peak hours and on a daily basis given the proposed 226 loading bays over both properties.
3. Confirmation that the Transportation Impact Studies analysis has accounted for the truck volumes correctly.

It is expected that the Town’s Transportation peer reviewer can provide this analysis before the June 27th Planning and Development Council Meeting.

Road Capacity

Winston Churchill Boulevard Classification: The Town’s Official Plan - Schedule C – Transportation Plan identifies Winston Churchill Boulevard as a “Major Arterial” road. The purpose of Major Arterial roads is to accommodate high volumes of traffic moving between communities travelling to activity centres and traffic on route to the Provincial Highway system. Major Arterial roads are 4-6 lanes in width (35m-50m) and are designed to accommodate 40,000 to 60,000 vehicles per day. The Region of Peel undertakes traffic counts on Winston Churchill Boulevard and provided the following traffic counts:

2019 – 5369 vehicles/day
2020 – 4092 vehicles/day
2021 – 4759 vehicles/day

Winston Churchill Boulevard is a 4-lane cross-section in the vicinity of the site, and it is not expected to reach 60,000 vehicles per day on this road. If volumes increase because of this use or any other use, it is expected that Winston Churchill Boulevard can handle the volumes safely.

Lastly, the existing road has a right-of-way width of 41.5 m (20.75 m from the centreline of the road) and both the Region of Halton and the Region of Peel have advised that a road widening will be required. Conditions of approval have been included requiring a road widening along the frontage of the sites.

Land Use Compatibility/ Noise Concerns

The Region of Halton requested both landowners to prepare a Land Use Compatibility report (LUC). The LUC reports provide an assessment on noise, vibration, odour and air pollution effects to provide better clarity on the impacts of the proposed warehouse development on the surrounding sensitive land uses and to demonstrate if the proposal is in accordance with the D-6 guidelines, including potential mitigation measures. Typically the submission of a LUC study is required when an application for an Official Plan Amendment and/or Zoning By-law Amendment is proposed as these applications would be introducing a use in an area that is not already permitted. In the case of the two site plans under review warehousing is permitted within the Official Plan and the Zoning By-law.

Noise studies are prepared in accordance with the Ministry of Environment document referenced as “*Environmental Noise Guidelines Stationary and Transportation Sources – Approval and Planning, Publication NPC-300*”. The Ministry guidelines set out acceptable sound levels for various times of day (7am-11pm and 11pm-7am) and recognize that backup beepers are safety devices and as such do not provide any specific guidelines for these sources of noise. The Ministry guidelines also do not require that noise is inaudible but that the noise levels fall within the acceptable limits.

The following reports were submitted in support of the applications:

SP.1601.028/01:

- Land Use Compatibility, Environmental Air Quality, Noise and Vibration dated July 16, 2019, and prepared by Novus Environmental
- Addendum to Compatibility & Mitigation Study Air Quality, Dust, Odour”, dated November 2021, and prepared by SLR Consulting Limited
- Noise Feasibility Study dated September 2021, and prepared by HGC Consulting

SP.1601.029/01:

- Land Use Compatibility Assessment (Air), dated December 2021, and prepared by Ortech.
- Preliminary Environmental Noise Report and Land Use Compatibility Report, dated December 2021, and prepared by Jade Acoustics.

The above Land Use Compatibility Studies (LUC)/Noise Studies were reviewed by the Region of Halton's peer reviewer (Dillion Consulting).

Peer Review Conclusions and Recommendations:

The peer reviewer provides the following comments with respect to air quality:

“Both studies characterize the potential for air quality impacts from the respective proposed facility as minor and insignificant at nearby sensitive receptors. Additionally, the significant sources at both facilities are vehicle emissions and combustion equipment for comfort heat. Dillon recommends that the addition of minor facilities with vehicular and combustion emissions is unlikely to significantly change the composition (i.e. which chemical species are emitted) or quantity of air emissions to the Airshed. Dillon recommends that these proposed facilities are not significant when considered in the context of the Clarkson Airshed Study.

Dillon was also asked to comment on the potential for cumulative impacts as a result of both facilities being operational. The two studies did not present results in a way which is easily compared: the Addendum Report for 560 Winston Churchill uses the significance of the emission sources to justify compatibility while the Air Quality Report for 772 Winston Churchill uses dispersion modelling to quantify the impacts to justify compatibility. This difference in the methods used makes it difficult to comment on the cumulative nature of the two facilities; however, Dillon recommends that when considering the nature of the proposed facilities and the expected emissions, the potential for cumulative impacts is low.”

The peer reviewer provides the following comments with respect to noise:

“Through reviewing the Noise Feasibility Study completed by HGC Engineering (560 Winston Churchill Boulevard) and the Noise Report completed by Jade Acoustics Inc. (772 Winston Churchill Boulevard), the surrounding sensitive receptors with the greatest potential to experience cumulative noise impacts were identified to be residential houses located at 658 Winston Churchill Boulevard and 645 Winston Churchill Boulevard.

Based on the predicted worst-case noise impacts presented in both noise assessments, there is the likelihood that both 658 Winston Churchill Boulevard and 645 Winston Churchill Boulevard would experience cumulative noise impacts. However, the worst-case cumulative impacts would likely only be a marginal exceedance of the noise criteria, less than 3 dB, which is typically imperceptible.

To fully understand the potential quantitative cumulative noise impacts from both industrial uses on the surrounding sensitive receptors, a stationary noise assessment should be completed by a Qualified Acoustic Consultant encompassing the operations of both 560 Winston Churchill Boulevard and 772 Winston Churchill Boulevard proposed developments.”

The Dillon peer review reports can be found on the Town’s website.

Water and Wastewater Servicing

Currently, there is not a watermain or a sanitary sewer located adjacent to either property.

As a submission requirement, a Functional Servicing Report (FSR) was prepared by A. M. Candaras Associates Inc. and dated August 31, 2020. This FSR was a comprehensive study that addressed the servicing for both properties, including 658 Winston Churchill Blvd.

The Region of Halton has reviewed the FSR to establish a preferred servicing scenario for the subject lands.

The Region’s preferred servicing of the subject lands would be as follows:

- Water servicing would be by constructing a minimum 300mm dia. watermain on Winston Churchill Boulevard from Deer Run Avenue northward to Beryl Road.
- Wastewater servicing would be constructing a gravity sanitary sewer to be located within the Town of Oakville’s open space block/park, walkway block with a connection to the existing sewer on Acacia Court. The existing sewer on Acacia Court has to be upgraded to a larger size to accommodate the flows from these developments.
- Accommodating the sanitary servicing for the future development of 658 Winston Churchill Boulevard (the small parcel surrounded by 560 WCB) through the approvals for these files.

All costs associated with the construction of these services would be borne by the developers.

The Region has advised that the developer would be responsible to design, construct, fund and coordinate the extension of the watermain under the CN railway line and along Winston Churchill Blvd.

The Region of Halton has provided servicing conditions to reflect the above servicing strategy and on the assumption Town Council will approve the granting of an easement to the Region through the Town parkland to support the preferred servicing strategy.

Regional comments for both applications are located within Appendices “C” and “D”. A comprehensive set of conditions for each application are located within Appendices “A” and “B”.

Conditions of approval have been included dealing with construction impacts and are summarized below:

- the submission of a work plan to address the timing of the construction operations, details on construction methods, sewer excavation, paving operations, landscaping restorations, traffic control, resident disruptions, sewer lateral "switchovers" and methods for replacing a "live" sanitary sewer
- a pre-condition survey of any existing buildings, existing landscape features, fences, structures, etc. that are adjacent to the proposed sanitary sewer and that could be affected by its construction
- a construction vibration study with recommendations related to reducing/minimizing vibration impacts from construction activities
- a landscape plan to address restoration of disturbed areas (including the Town's park and walkway)
- the requirement to restore the Acacia Court right-of-way where it has been impacted by development, including fully repaving the road (as opposed to patching); repairing curbs and boulevards, driveway repairs and landscaping of boulevards .

Aspen Forest Park Easement

As discussed above, the Region of Halton's preferred servicing scenario is for the construction of a 250mm gravity sanitary sewer extending through the Town's lands (Aspen Forest Park), connecting to an existing sewer on Acacia Court and Deer Run Avenue (Figure 10). The area of this easement will be approximately 0.843 acres.

The easement will restrict what can be built on top of it, but the infrastructure will be below ground, so the surface can still be used as open space.

The Town has initiated an appraisal to determine the market value of the easement through the Town's park. If Council approves the granting of this easement, the

applicant/landowner will be required to pay all costs required to transfer the easement to the Region and pay the Town the market value of the easement as determined by the Town's appraisal.

Draft R-Plan

Context Plan

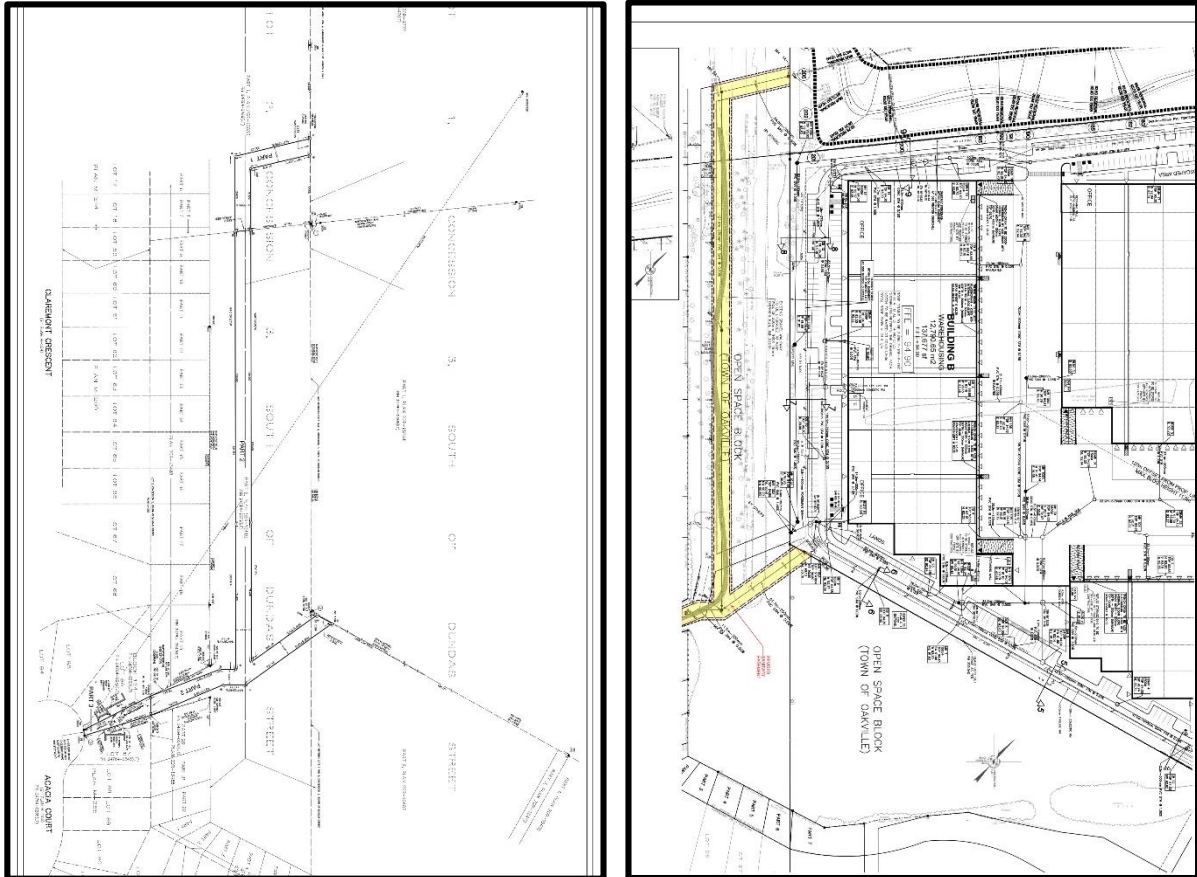


Figure 10: Proposed easement location

Flooding Concerns

The Stormwater Management (SWM) criteria for both of these developments were based on the previously approved Subwatershed Study (SWS) for Clearview Creek. Both applications were reviewed by the Credit Valley Conservation Authority (CVC) and the Town's Stormwater Engineer. The CVC has indicated that they have no objection to the approval of the applications subject to conditions which have been included with Appendices "A" and "B".

Berm/Landscaping/Fencing/Lighting Concerns

As mentioned earlier Section 41, subsection 7 of the *Planning Act* outlines the types of items the municipality can review as part of the site plan process. The Town's Urban Design and Parks staff have reviewed the on-site landscaping requirements, as well as the berm location, fencing and landscaping.

An existing berm is located west of 700/750 Winston Churchill Blvd. and is proposed to be extended along the southern property limit of 560/570 and 580 Winston Churchill Blvd (Figure 11).

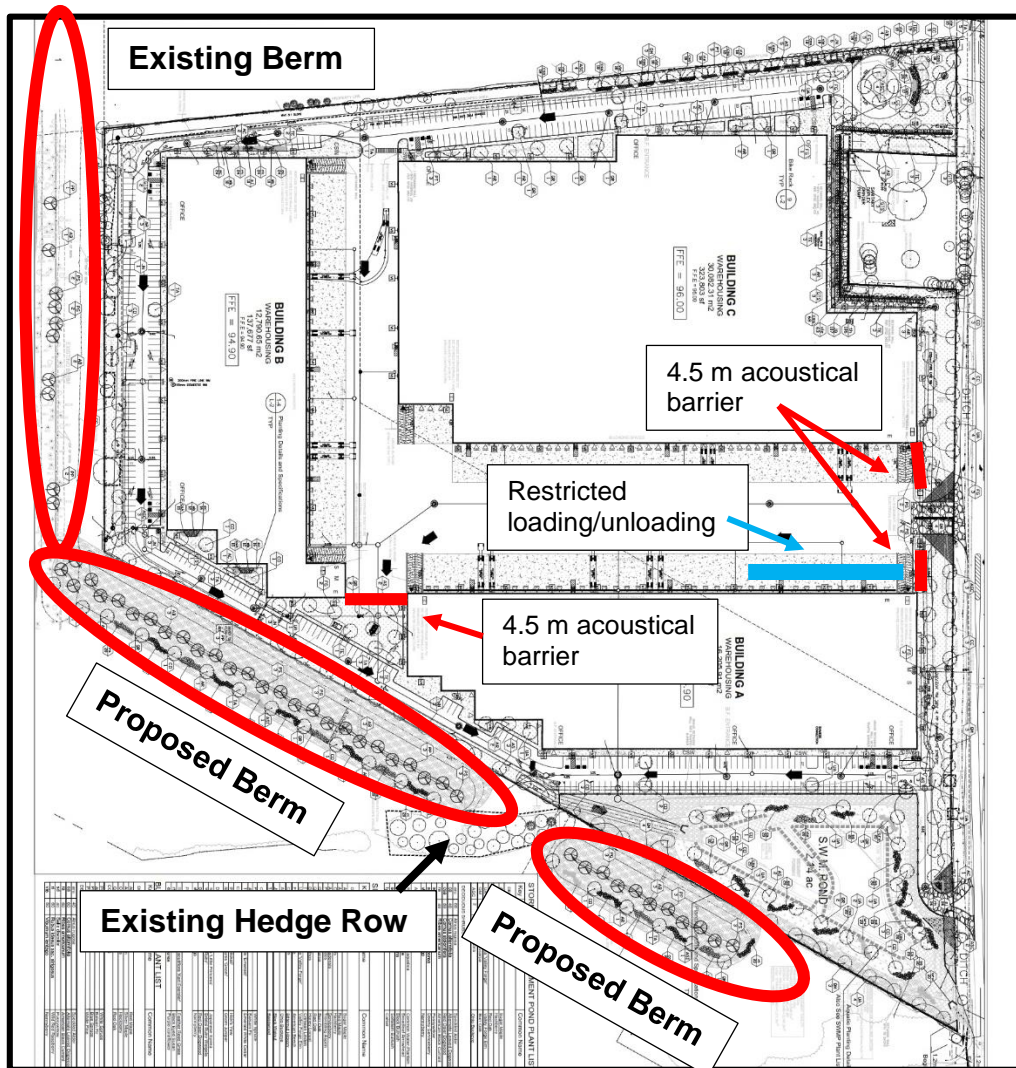


Figure 11: Landscape Plan 560/570/580 Winston Churchill Blvd.

The new berm is proposed to be 3 m in height plus tree plantings and 19 m wide at the base. The berm would have a minimum 3:1 slope which would allow for the berm to be landscaped. A condition of approval is included that requires the submission of a drawing package that will provide additional details with respect to the proposed grades for the new berm including the proposed planting and general construction methods, materials, haul routes and final plantings.

Landscaping – 560/570/580 Winston Churchill Blvd.:

As part of the site plan application the applicant was required to provide a landscape plan (Figure 11). Landscaping is proposed on both sides of the drive aisle around the site including along the common property lot line with the lands Zoned “O1”, around the proposed stormwater management pond and between the buildings and Winston Churchill Blvd. The proposed landscape plan can be further described as follows:

- Stormwater Management Pond
 - Deciduous trees – 37
 - Deciduous shrubs – 320
 - Aquatics Plants – 640
- On Site Planting
 - Deciduous trees – 210
 - Coniferous trees - 21
 - Deciduous shrubs – 556
 - Coniferous shrubs - 213
 - Perennials and Grasses – 243
- Berm Plant list (off-site)
 - Deciduous trees – 14
 - Coniferous trees - 50
 - Deciduous shrubs – 320

The proposed canopy coverage plan/calculation is 20% for the site.

Landscaping – 700/750 Winston Churchill Blvd.:

As part of the site plan application the applicant was required to provide a landscape plan (Figure 12). Landscaping is proposed on both sides of the drive aisle around the site including along the common property lot line with the lands Zoned “N”, around the proposed dry pond and between the buildings and Winston Churchill Blvd. The proposed landscape plan can be further described as follows:

- Trees – 160
- Deciduous shrubs – 649
- Coniferous shrubs - 362
- Perennials and grasses – 127

The proposed canopy coverage plan/calculation is 20.3% for the site which includes the proposed planting and the existing planting with the Clearview channel owned by the applicant.

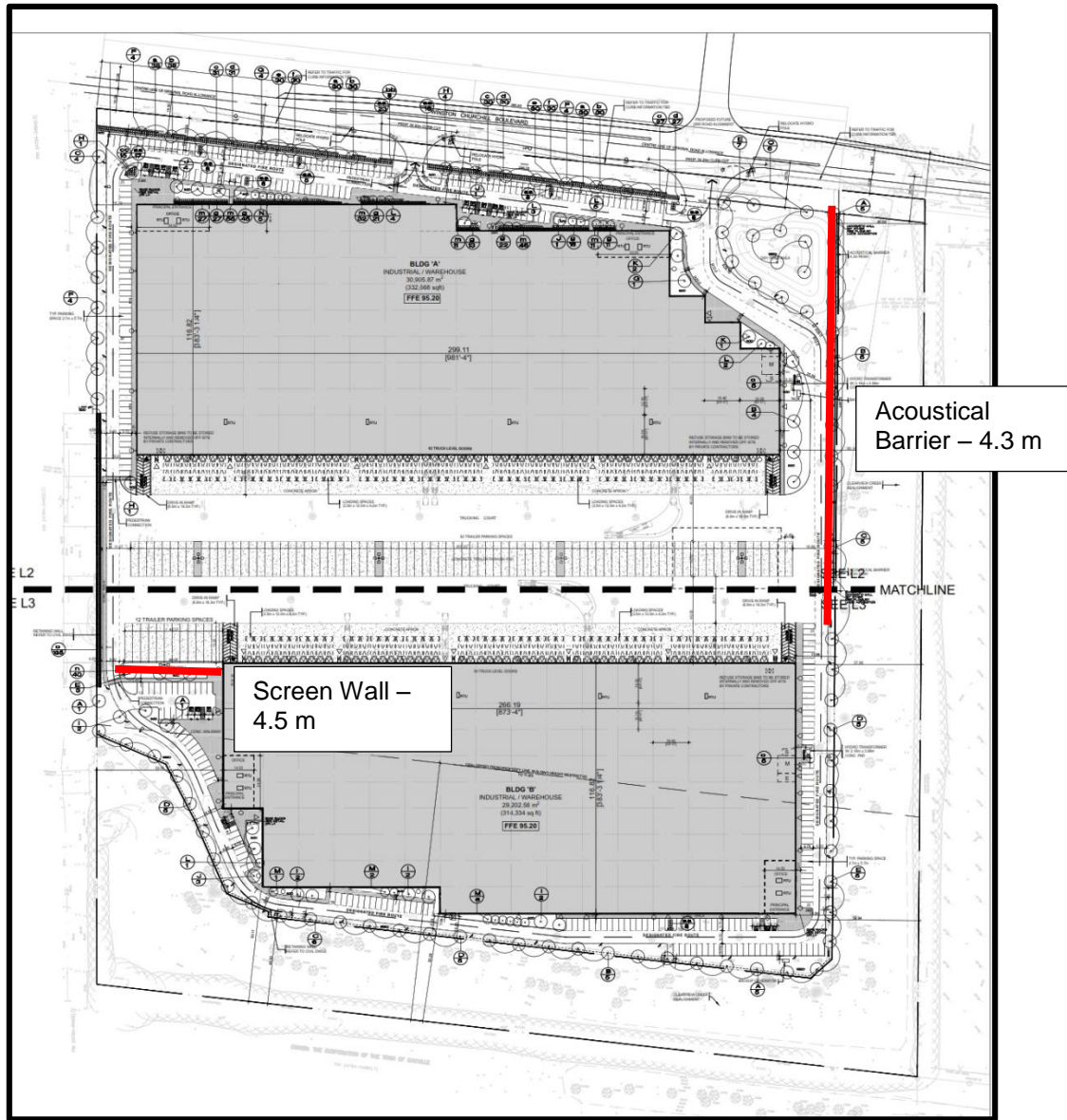


Figure 12: Landscape Plan 700/750 Winston Churchill Blvd.

Fencing:

Fencing was reviewed as part of the site plan applications and a condition of approval has been included that requires the site plan and landscape plans be revised to include a 1.2m black vinyl chain link fence. The Noise walls and screen walls are included within Figures 11 and 12.

Site Lighting:

The applicants were also required to submit a photometric plan prepared by a professional electrical engineer. The purpose of this plan is to demonstrate the location of light fixtures and the illumination levels. The Town requires all exterior

lighting fixtures are full cut-off in order to direct illumination downward and dark sky compliant to reduce light pollution, sky glare, and light trespass onto neighbouring properties. The plans will need to demonstrate illumination levels of 0.0 lux measured at all property lines and to provide readings that extend 6 m beyond the property lines of the subject site. The plans received to date comply with these requirements.

General Operational Issues

The current E2 zoning permits warehousing operation and heavy vehicle parking. Given the nature of warehousing operations, it is not unusual for tractor trailers to be parked on a site. The site-specific zoning was placed on these lands to minimize impact on the residents by restricting the location of loading areas and by placing the buildings between the residential uses and the loading areas, the loading area will not be visible from the residential uses in the vicinity. In discussions with the Town's peer reviewer they have indicated that it is very unlikely that the 226 loading bays on the site would be in use simultaneously (eg. trucks backing into spaces, coupling or uncoupling).

Since site plan approvals deal with the provision of facilities, rather than the use of those facilities, conditions related to operational matters such as restrictions on the hours of operation, construction phasing and prospective tenants are beyond the scope of the site plan authority.

Enforcement Issues

The Municipal Enforcement Services Department has officers that enforce the Town's by-laws, rules, laws, codes or regulations. If there are concerns, residents can lodge a complaint with Municipal Enforcement Services Department and they will investigate and determine if there is an infraction.

Relevant nuisance by-laws:

Noise - The Town has a Noise By-law (2008-098 as amended). The Noise by-law is administered and enforced by the Municipal Enforcement Section and is based on provincial standards.

Idling – Idling is an operational issue and falls outside of the scope of Section 41 of the *Planning Act*, however, the Town has an Anti-Idling By-law (2002-153 as amended by 2017-011 and 2019-050). The Anti-Idling by-law is enforced by the Municipal Enforcement Services.

Halton Regional Police may be contacted to request enforcement for speeding or *Highway Traffic Act* violations.

Some resident concerns have been raised regarding coyote displacement with the construction of the proposed warehouse buildings. The Municipal Enforcement

Services Department has coyote information and an education/response procedure including a response strategy available online. Residents are encouraged to report coyote sightings and encounters through our coyote reporting form as this assists staff with our coyote response and educational programs including installation of signs.

General Comments

The subject lands are unique in Oakville, south of Dundas Street, as they are the largest individual employment blocks currently undeveloped. Other large employment areas in Oakville (i.e. Winston Park) have smaller individual employment blocks but cumulatively would have more employment floor area and more loading bays. Unlike the subject lands, the Winston Park employment area does not provide a cap on height or has specific regulations about the location of loading areas. The site-specific regulations for the subject lands relating to the height and location of loading areas are intended to mitigate impacts of the employment uses to the residential properties in the vicinity.

Public Input

Through the processing of these applications the Planning Department has received some letters of concern (Appendix G).

Issues raised include:

- Scale of the development (number of buildings, height of buildings, number of loading bays/vehicle parking spaces)
- Impact of the proposed development on the residential neighbourhood
- Definition of minimal impact
- Traffic, including road capacity/cumulative impact
- Truck routes
- Noise including noise from vehicles entering and exiting the site, back up beepers and rooftop mechanicals, coupling and uncoupling of tractor trailers and the need for berms or noise walls.
- Landscaping including the need to increase the height of the proposed berm/additional landscaping
- Clarification around zoning and when 'warehousing' uses were permitted on the lands
- Need for fencing/sound barrier
- Light spill onto residential areas
- Air Quality and Emissions
- D6 guidelines
- Timing of transportation and LUC/Noise peer reviews

- Nuisances created by the construction to the servicing through the Town's park to Acacia Court
- Operational issues such as control over which types of companies and lease the buildings, hours of operation, idling vehicles, garbage/recycling
- Impacts to wildlife

The manner in which above issues have been addressed is described in the body of the report. To the extent that these issues are related to matters of site plan authority, changes to the plans and drawings have been achieved, and conditions imposed.

Commitment Letter

The solicitors for the applicant have provided a commitment letter attached as Appendix "I" for Council's consideration. These commitments can be captured within the future Site Plan agreement.

CONSIDERATIONS:

(A) PUBLIC

A joint developer-initiated Public Information Meeting/video conference was held on September 27, 2021, where 60 members of the public and the Ward Councillors attended. Minutes from the video conference are attached within Appendix "H".

Notice for the meeting regarding these site plan applications was provided through a mailing of all properties abutting the subject lands, residents who submitted comments, and attended the developer-initiated Public Information meeting, the Resident Associations, agencies including the abutting Region/municipality.

(B) FINANCIAL

Development Charges would apply to this development. Parkland dedication is applicable. The compensation for the Region easement will be deposited in the Parks administrative external revenue account and all costs involved in the transfer of the easement will be covered by the owner.

(C) IMPACT ON OTHER DEPARTMENTS & USERS

The application was circulated to internal and external departments and agencies for comment. The Parks and Open Space, Engineering and Transportation, Planning and Legal departments were consulted in the preparation of this report

Conditions of approval have been included as requested by the commenting agencies and departments.

(D) CORPORATE STRATEGIC GOALS

This report addresses the corporate strategic goal(s) to:

- *be the most livable town in Canada*

(E) CLIMATE CHANGE/ACTION

Managing potential impacts of development is important in achieving environmental sustainability.

CONCLUSION:

Staff acknowledge that while not all issues have been resolved yet, conditions of approval have been imposed that will require the completion of the various studies to the Town and Region's satisfaction, and incorporated into the sites' designs and plans prior to final approval.

In addition, the site plan agreement will include the following clause:

"The Owner agrees to implement the recommendations outlined in all studies forming part of the Approved Site Plan, including transportation impact study, stormwater management and functional servicing report, land use compatibility study, noise feasibility study, significant woodlands assessment or arborist's report. Further the Owner agrees to modify the site plan as required to reflect the final approved studies."

Based on the above staff recommends approval of Site Plan application, File No.: SP.1601.028/01 subject to the conditions in Appendix "A" and Site Plan application, File No.: SP.1601.029/01 subject to the conditions in Appendix "B"

APPENDICES:

- Appendix "A" – Conditions of approval – SP.1601.028/01
- Appendix "B" – Conditions of approval – SP.1601.029/01
- Appendix "C" – Halton Region comments - dated May 25, 2022 – SP.1601.028/01
- Appendix "D" – Halton Region comments - dated May 25, 2022 – SP.1601.029/01
- Appendix "E" – Peel Region comments - dated May 2, 2022 – SP.1601.028/01
- Appendix "F" – Peel Region comments - dated May 2, 2022 – SP.1601.029/01
- Appendix "G" – Written Submissions
- Appendix "H" – Development Public Information Meeting Minutes
- Appendix "I" – Commitment Letter

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