

## AGENDA

Date: Monday, March 17, 2025  
Time: 6:30 p.m.  
Location: Council Chamber

Town Hall is open to the public and live streaming video is available on <https://www.oakville.ca/town-hall/mayor-council-administration/agendas-meetings/live-stream> or at the town's YouTube channel at <https://www.youtube.com/user/TownofOakvilleTV>. Information regarding written submissions and requests to delegate can be found at <https://www.oakville.ca/town-hall/mayor-council-administration/agendas-meetings/delegations-presentations>.

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### Pages

1. Regrets
2. Declarations of Pecuniary Interest
3. Committee of the Whole
4. Consent Items(s)
  - 4.1 Recommendation Report, 24CDM-24011.1615 – 123 Maurice Drive, Draft Plan of Condominium, 123 Maurice Developments Ltd. 5 - 19

Recommendation:  
That the Director of Planning and Development be authorized to grant draft plan approval to the Draft Plan of Condominium (24CDM-24011/165) submitted by 123 Maurice Developments Ltd., and prepared by Krcmar Surveyors Ltd. dated February 27, 2024.
5. Confidential Consent Item(s)

*Item 5.1 see Confidential Addendum to be distributed.*

  - 5.1 OLTAppeal Argo Trafalgar Corporation
6. Public Hearing Item(s)

- 6.1 Public Meeting Report – Plan of Subdivision and Zoning By-law Amendment, Argo Lions Valley Ltd., 1303 Dundas Street West, File No. 24T-25002/1323 and Z.1323.01** 20 - 92

Recommendation:

1. That comments from the public with respect to the proposed Plan of Subdivision and Zoning By-law Amendment submitted by Argo Lions Valley Ltd. (File No.: 24T-25002/1323 and Z.1323.01), be received.
2. That staff consider such comments as may be provided by Council.

- 6.2 Public Meeting Report – Plan of Subdivision, Official Plan Amendment and Zoning By-law Amendment, Neatt (16 Mile Creek) Inc., 3056 Neyagawa Boulevard, File No. OPA 1321.02, Z.1321.02 and 24T-24006/1321** 93 - 310

Recommendation:

1. That comments from the public with respect to the proposed Plan of Subdivision, Official Plan Amendment and Zoning By-law Amendment submitted by Neatt (16 Mile Creek) Inc., (File No.: 24T-24006/1321, OP.1321.02 and Z.1321.02), be received.
2. That staff consider such comments as may be provided by Council.

- 6.3 Public Meeting and Recommendation Report for Zoning By-law Amendment Application – 65, 71, 77, 83 & 89 Loyalist Trail, Tafia Development Corporation, File No.: Z.1215.04** 311 - 345

Recommendation:

1. That the Zoning By-law Amendment application submitted by Tafia Development Corporation, File No.: Z.1215.04, be refused as proposed;
2. That the notice of Council's decision reflect that Council has fully considered all of the written and oral submissions relating to these matters and that those comments have been appropriately addressed; and,
3. That, in accordance with Section 34(17) of the *Planning Act*, no further notice is determined to be necessary.

**7. Discussion Item(s)**



**7.1 Old Oakville Heritage Conservation District By-law and Plan and Guidelines**

346 - 529

Recommendation:

1. That By-law 2025-054, a by-law to amend By-law 1981-144, a by-law to designate an area of the Town as a Heritage Conservation District under Section 41(1) of the Ontario Heritage Act, to include Part IV properties at 8 Navy Street, 110-114 King Street and a Portion of 144 Front Street, be passed; and
1. That By-law 2025-063, a by-law to adopt the Old Oakville Heritage Conservation District Plan and Guidelines dated March 2025 under subsection 41.1 (2) of the Ontario Heritage Act, be passed.

**8. Confidential Discussion Item(s)**

There are no Confidential Discussion Items listed for this agenda.

**9. Advisory Committee Minutes**

There are no Advisory Committee Minutes listed for this agenda.

**10. Rise and Report to Council**

**11. New Business**

(Emergency, Congratulatory or Condolence)

**12. Consideration and Reading of By-laws**

That the following by-law(s) be passed:

**12.1 By-law 2025-054**

A by-law to amend By-law 1981-144, a by-law to designate an area of the Town as a Heritage Conservation District under Section 41(1) of the Ontario Heritage Act, to include Part IV properties at 8 Navy Street, 110-114 King Street and a Portion of 144 Front Street. **(Item 7.1)**

**12.2 By-law 2025-055**

530 - 531

A by-law to declare that certain land is not subject to part lot control (Block 124, Plan 20M-1272, Lots 26, 27, 28 and Blocks 32, 33, 34, Plan 20M-1281 – Tinor Development (BT) Corp.)

**12.3 By-law 2025-060** 532 - 532

A by-law to repeal By-law 1985-101, a by-law to declare that certain land is not subject to part lot control (Block 3, 4 and 5, Plan 20M=246)

**12.4 By-law 2025-061** 533 - 539

A by-law to amend By-law 2021-136, being a by-law to designate St. John's United Church at 262 Randall Street as a property of cultural heritage value or interest.

**12.5 By-law 2025-063**

A By-law to adopt the Old Oakville Heritage Conservation District Plan and Guidelines dated March 2025 under subsection 41.1 (2) of the Ontario Heritage Act. **(Item 7.1)**

**12.6 By-law 2025-067** 540 - 541

A by-law to declare that certain land is not subject to part lot control (Block 123, Plan 20M-1272, and Blocks 22, 23, 24, 25, 30 and 31, Plan 20M-1281 – Haven-Oak Homes Phase 5 Inc.)

**12.7 By-law 2025-068** 542 - 543

A by-law to declare that certain land is not subject to part lot control Blocks 271, 276 and 281, plan 20M-1288 – Mattamy (Joshua Creek) Limited)

**12.8 By-law 2025-069** 544 - 544

A by-law to confirm the proceedings of a meeting of Council.

Recommendation:

That the by-laws noted above, be passed.

**13. Adjournment**

## REPORT

### Planning and Development Council

Meeting Date: March 17, 2025

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**FROM:** Planning and Development Department

**DATE:** March 4, 2025

**SUBJECT:** Recommendation Report, 24CDM-24011.1615 – 123 Maurice Drive, Draft Plan of Condominium, 123 Maurice Developments Ltd.

**LOCATION:** 123 Maurice Drive

**WARD:** Ward 2

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#### RECOMMENDATION:

That the Director of Planning and Development be authorized to grant draft plan approval to the Draft Plan of Condominium (24CDM-24011/165) submitted by 123 Maurice Developments Ltd., and prepared by Krcmar Surveyors Ltd. dated February 27, 2024.

#### KEY FACTS:

The following are key points for consideration with respect to this report:

- A draft plan of standard condominium application has been submitted by 123 Maurice Developments Ltd., for the lands located at 123 Maurice Drive.
- The four-storey, 55 unit mixed-use apartment includes one commercial unit at-grade, began construction in August 2021 and is now fully constructed and occupied.
- The development received final site plan approval on August 18, 2021.
- Condominium tenure would allow for the shared use of internal private amenities, visitor and commercial parking areas, internal watermain and internal sanitary and storm sewers and any associated maintenance costs would be transferred to the condominium corporation.
- No concerns have been raised by internal departments or external agencies.
- Staff recommends approval of the draft plan of standard condominium, subject to the conditions outlined in Appendix A.

## BACKGROUND:

The purpose of this report is to provide a full staff review of the application and a recommendation on the proposed draft plan of standard condominium application.

The draft plan of condominium application was submitted on October 18, 2024 by 123 Maurice Developments Ltd. The intent of the draft plan of condominium is to establish condominium tenure for the 55-unit mixed use apartment and one commercial unit which will allow for the management and maintenance of shared elements by the future condominium corporation. The details of the management and maintenance clauses are found in the draft declaration, submitted with the application.

## Proposal

The applicant is proposing a standard condominium for the four-storey, 55-unit mixed use apartment building that includes one at-grade commercial unit. Please see excerpts of the Draft Plan of Condominium on Figure 1, 2 and 3 below.

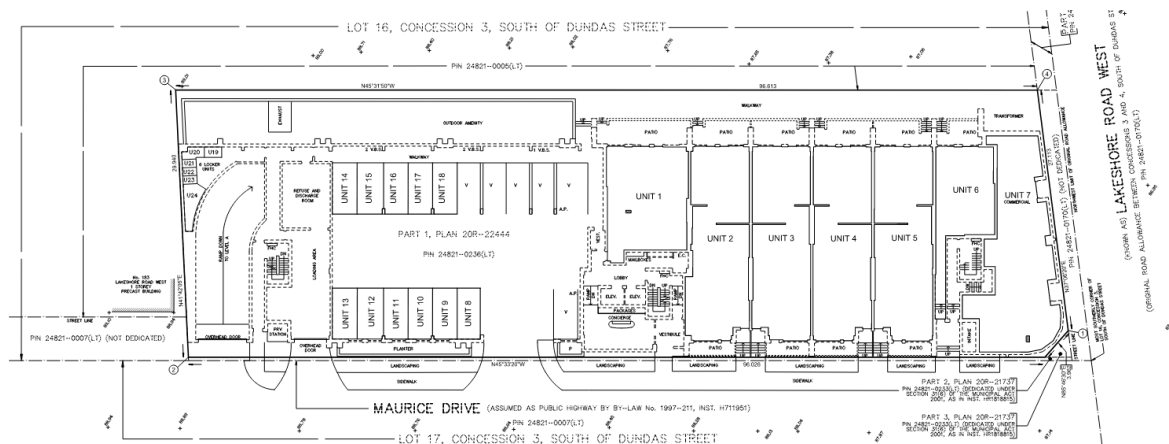


Figure 1 – Draft Plan of Condominium Ground Floor

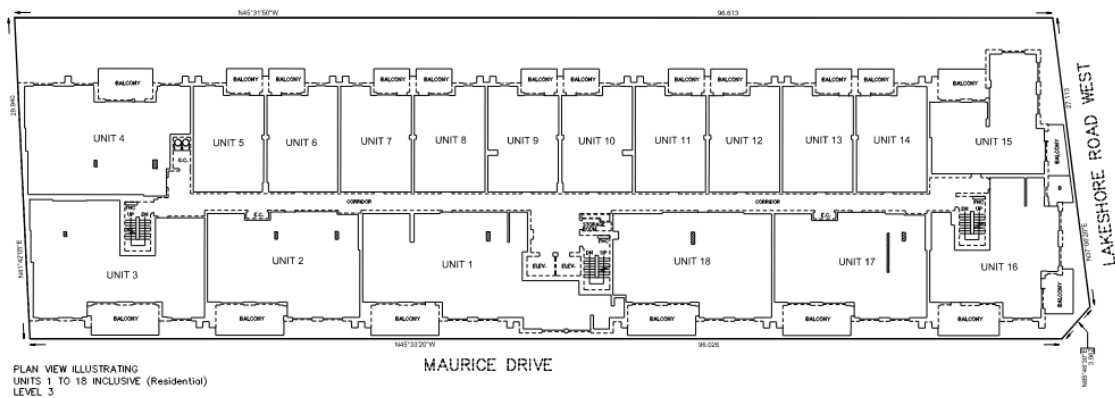
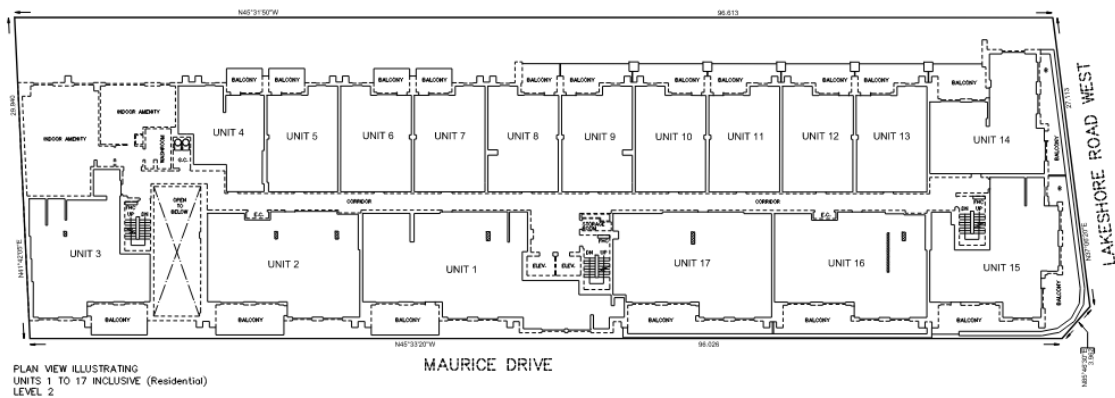


Figure 2 – Draft Plan of Condominium Floor 2 & 3

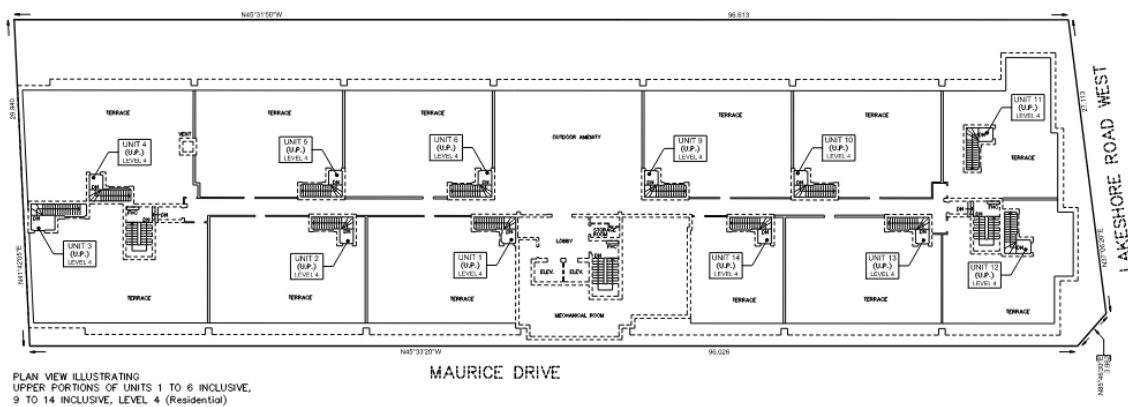
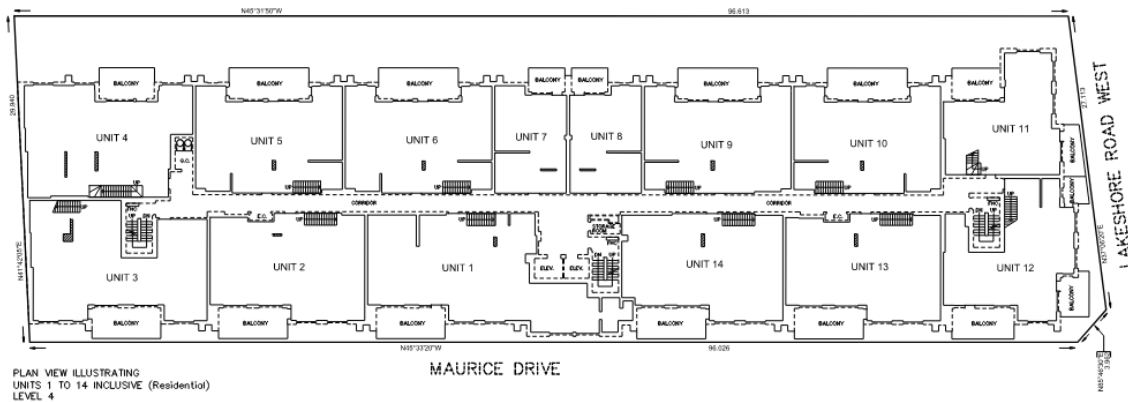


Figure 3 – Draft Plan of Condominium Floor 4 and Rooftop Terrace

### Location & Site Description

The subject property is approximately 0.2919 hectares in size and is located on the east side of Maurice Drive, north of Lakeshore Road West and south of Rebecca Street. The land is legally recognized as Part of Lot 16, Concession 3 Trafalgar, South of Dundas Street, and the apartment construction is now fully constructed and occupied.

### Surrounding Land Uses

The land uses surrounding the subject property are comprised of mixed-use residential, newer townhouse units, existing low rise residential dwellings north of Rebecca Street and service commercial uses.



Figure 4 – Aerial view of 123 Maurice Drive

## **PLANNING POLICY & ANALYSIS:**

### **Halton Region Official Plan (Implemented by the Town)**

Due to recent Provincial legislation, as of July 1, 2024, the Halton Region's role in land use planning and development matters has changed. The Region is no longer responsible for the Regional Official Plan. It is now the responsibility of Halton's four local municipalities. As a result of this change, a Memorandum of Understanding (MOU) between the Halton municipalities and Conservation Authorities was prepared that identified the local municipality as the primary authority on matters of land use planning and development. The MOU also defines the continued scope of interests for the Region and the Conservation Authorities in these matters.

As outlined in the MOU, the Region now only has an interest in supporting our local municipal partners by providing review and comments on a scope of interests that include:

- Water and Wastewater Infrastructure;
- Regional Transportation Systems including stormwater management infrastructure and acoustic mitigation on Regional rights-of-way;
- Waste Collection;
- Affordable and Assisted Housing;
- Responsibilities associated with a specific mandate prescribed by legislation (e.g. source water protection, public health); and
- Other Regional services that have a land component.

The Regional Official Plan provides goals, objectives and policies to direct physical development and change in Halton. The proposed development is located on lands that are designated as 'Urban Area' and is located within the built boundary of the Halton Region Official Plan (ROP). The policies of Urban Area designation support a range of uses and the development of vibrant and healthy mixed-use communities that afford maximum choices for residence, work, and leisure.

The application conforms to the Region of Halton Official Plan (now implemented by the Town of Oakville).

Regional staff offers no objection to the proposed Draft Plan of Condominium approval from a Regional perspective, subject to the conditions outlined in Appendix 'A'.

### ***Livable Oakville Plan***

The subject lands are designated as 'Main Street 1'. As part of the site plan application conformity with Livable Oakville was confirmed. The intent of the subject draft plan of condominium is to establish condominium tenure for the 55-unit mixed-use apartment building. Conformity with Livable Oakville was confirmed through the related site plan application.

On this basis, the proposal conforms to Livable Oakville.

### **Zoning By-law**

The subject property, highlighted in grey is zoned CBD– Central Business District (By-law 2014-014) as shown in Figure 5 below.



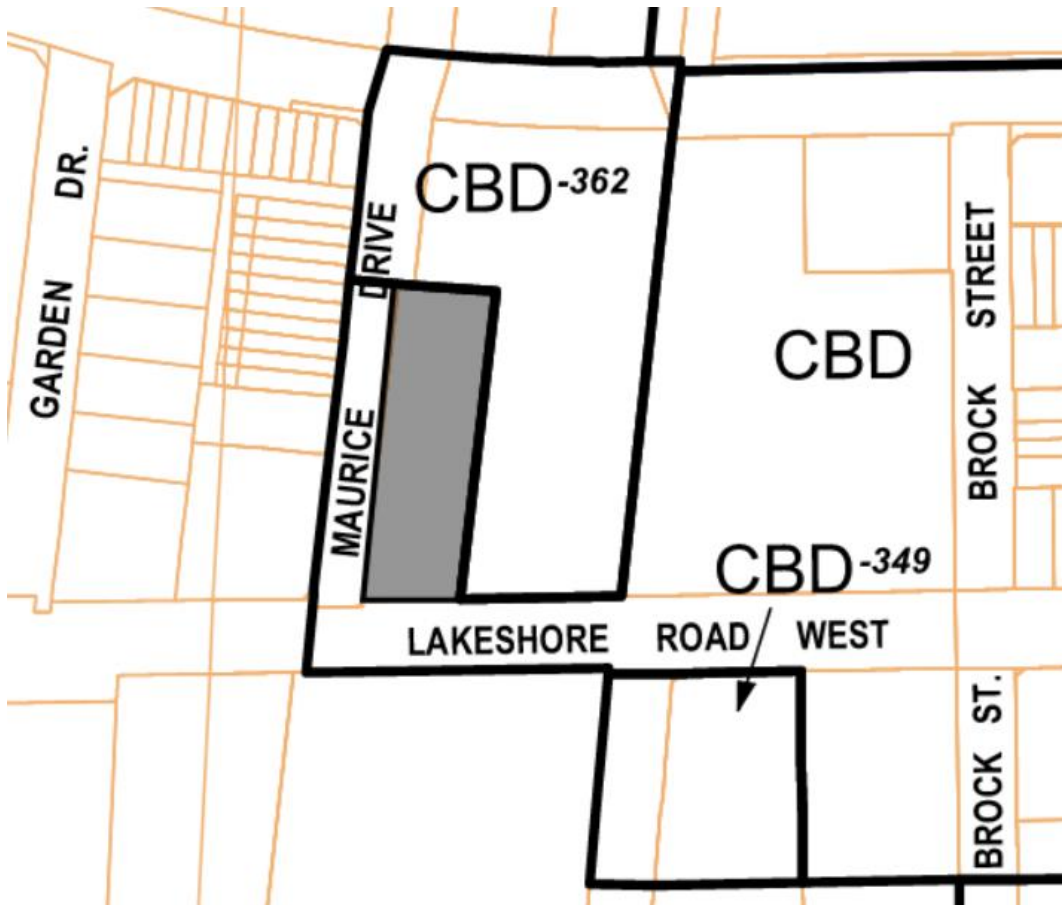


Figure 5 – Oakville Zoning By-law 2014-014 Map Excerpt

Compliance with the Zoning By-law was reviewed as part of the site plan application. As a condition of approval in Appendix A, the applicant will be required to confirm that the “as built” development complies with the Zoning By-law, prior to registration.

#### Site Plan Application

The subject property was subject to site plan control and received final site plan approval on August 18, 2021. Through the site plan control process, among others, the following matters were addressed:

- Site layout;
- Site servicing;
- Canopy coverage;
- Site circulation;
- Stormwater management; and,
- Environmental suitability of the site for residential

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## **TECHNICAL & PUBLIC COMMENTS:**

The draft plan of condominium application was circulated to internal departments and external agencies for comments, and was subject to detailed technical analysis.

Subject to the conditions in Appendix 'A', no further requirements have been identified. There are no outstanding financial, legal or planning issues to be resolved.

Accordingly, the proposed plan of condominium meets the criteria established in Section 51(24) of the *Planning Act* and conforms to the Livable Oakville Plan.

## **CONSIDERATIONS:**

### **(A) PUBLIC**

As this is a standard condominium application and related to tenure, notice is not required with respect to this application.

### **(B) FINANCIAL**

A condition has been included in Appendix A which will ensure that the property taxes are paid and up-to-date.

### **(C) IMPACT ON OTHER DEPARTMENTS & USERS**

None of the circulated departments/agencies raised any issues with respect to the proposal. Any relevant conditions have been included within the draft plan of condominium conditions listed in Appendix A.

### **(D) COUNCIL STRATEGIC PRIORITIES**

This report addresses Council's strategic priority/priorities: Growth Management, Community Belonging, Environmental Sustainability and Accountable Government.

### **(E) CLIMATE CHANGE/ACTION**

The proposal complies with the sustainability objectives of the Climate Change Adaptation Initiative.

## **CONCLUSION:**

The Planning & Development Department undertook a circulation of the application to ensure that all technical and financial matters have been satisfactorily addressed.

Staff is satisfied that this draft plan of condominium application conforms to the overall policy direction of the Livable Oakville Plan and other relevant policy documents, and recommends approval of the application, subject to the conditions in Appendix 'A', as the following requirements have been satisfied:

- The proposed plan of condominium meets the criteria established in Section 51(24) of the *Planning Act*;
- The proposed plan of condominium conforms with the Halton Region Official Plan, conforms to the Livable Oakville Plan, and complies with the Zoning By-law regulations applicable to the subject property; and,
- A full circulation has been undertaken and there are no outstanding financial or planning issues that cannot otherwise be resolved, through the conditions found within Appendix 'A'.

## **APPENDICES:**

### Appendix A – Draft Plan of Condominium Conditions

Prepared by:  
Colin Westerhof, RPP, MCIP  
Planner, Current Planning

Recommended by:  
Paul Barrette, MCIP, RPP  
Manager, Current Planning – West District

Submitted by:  
Gabe Charles, MCIP, RPP  
Director, Planning Services

## Appendix A – Conditions

### TOWN OF OAKVILLE CONDITIONS FOR FINAL APPROVAL FOR THE REGISTRATION OF THE DRAFT PLAN OF STANDARD CONDOMINIUM BY 123 MAURICE DEVELOPMENTS LTD.

#### File 24CDM-24011.1615

This approval applies to the Draft Plan of Condominium (File 24CDM-24011.1615) submitted by 123 Maurice Developments Ltd., prepared by Krcmar Surveyors Ltd. dated February 27, 2024.

The final plans are to be reviewed and cleared to the satisfaction of the Town of Oakville.

The Town of Oakville conditions applying to the approval of the final plan for registration of 123 Maurice Developments Ltd., Draft Plan of Condominium (File 24CDM-24011.1615) are as follows:

CONDITIONS		CLEARANCE AGENCY
<b>GENERAL</b>		
1.	That the owner provides confirmation to the satisfaction of the Town's Finance Department that any outstanding development charges and property taxes have been paid prior to plan registration.	<b>OAK(F)</b>
2.	The owner provide a certificate signed by the surveyor and the owner that the plan proposed to be submitted for registration is the same as the latest (most recent) draft approved plan and, if the plans are not the same, that any differences between the proposed registered plan and the latest draft plan are accepted/approved by the Town.	<b>OAK (A)</b>
3.	The Owner shall provide an updated certificate from the Owner's engineer stating that all servicing, grading, drainage, overland flow route, and stormwater management requirements, and base asphalt paving have been completed in accordance with the plans and conditions in the original site plan agreement, or that arrangements to the satisfaction of the Director of Planning and Development have been made for their completion. Additionally, an updated letter should be provided to acknowledge that hydro, gas, lighting and communication services have been installed and are active.	<b>OAK (DE)</b>
4.	The Owner shall provide an as-built drawings illustrating servicing works in the ROW are to be provided prior to registration.	<b>OAK (DE)</b>
5.	That the owner/applicant confirms as-built compliance with the Zoning By-law and that any deficiencies be brought into compliance with the Zoning By-law through the Committee of Adjustment and/or a Zoning By-law amendment prior to plan registration.	<b>OAK(Z)</b>

## LEGAL

6. The Owner shall file with the Director of Planning, a complete copy of the final version of the Declaration and Description to be registered, which includes the following schedules:
- Schedule "A" containing statement from the declarant's solicitor that in this or her opinion, based on the parcel register or abstract index and the plans and drawings recorded in them, the legal description is correct and any easements mentioned in the schedule will exist in law upon the registration of the Declaration and Description; and,
  - Schedule "G" being the certification of the project engineer and/or architect that all buildings have been constructed in accordance with the regulations under the Condominium Act.

OAK (L)

When the Owner files a copy of the Declaration with the Director of Planning, it shall be accompanied with a letter of undertaking, stating that, "This is our undertaking to register the Declaration in the same form and content as was provided to you, subject to any changes the Land Registrar may require. This is also our undertaking to provide you with a registered copy of the Declaration once it is registered. If the Land Registrar requires any amendments to the Declaration we will advise you."

7. Visitors parking spaces will be clearly delineated on the condominium plan to be registered and the Declaration shall contain wording to provide and maintain the visitor parking spaces for the exclusive use of visitors and specifying that visitor parking shall form part of the common elements and neither to be used or sold to unit owners or be considered part of the exclusive use portions of the common elements.

OAK (L)

## REGION OF HALTON

8. The Owner shall provide submission of the Oakville Site Plan Agreement and/or the Condominium Declaration which notes "*That the proposed development, as designed, does not comply with the Regional Development Design Guidelines for Source Separation of Solid Waste and as such is not eligible for Regional Waste Collection.*"

RMH

In this regard, submission of the proposed Condominium Declaration and Oakville Site Plan Agreement is required.

9. That the Declarant provides confirmation that all agreements of purchase and sale shall include a Notice Letter/Acknowledgement informing the prospective purchasers that the site will be serviced by private waste collection in the Purchaser's welcome package, to the satisfaction of the Region of Halton. Where purchase agreements have already been signed, an amended welcome package shall be provided and the change indicated.

RMH

In this regard, submission of the purchase of sale or lease agreement is required.

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|-----|--|------------|
| 10. | If the proposed plan (regardless of private or Regional collection service) requires a Waste collection vehicle to drive onto or over a supported structure, the applicant must submit a letter certified by an Ontario Professional Engineer, indicating that the supported structure can support a fully loaded Waste truck, to the satisfaction of Halton Region. | <b>RMH</b> |
| 11. | The Owner will provide a letter of private collection to the Region indicating that they will provide collection for garbage, recycling and organics.  | <b>RMH</b> |

**HALTON CATHOLIC DISTRICT SCHOOL BOARD**

- |     |  |              |
|-----|--|--------------|
| 12. | We require that the following conditions be placed in the condominium agreement. The conditions are to be fulfilled prior to final approval: | <b>HCDSB</b> |
|-----|--|--------------|
1. The owner agrees to place the following notification in all offers of purchase and sale for all lots/units:
    - a. Prospective purchasers are advised that Catholic school accommodation may not be available for students residing in this area, and that you are notified that students may be accommodated in temporary facilities and/or bused to existing facilities outside the area.
    - b. Prospective purchasers are advised that the HCDSB will designate pick up points for the children to meet the bus on roads presently in existence or other pick up areas convenient to the Board, and that you are notified that school busses will not enter cul-de-sacs and private roads.
  2. In cases where offers of purchase and sale have already been executed, the owner is to send a letter to all purchasers which include the above statements.
  3. The owner will ensure that the HCDSB Notice Letter will be posted on the building's bulletin board.

## **HALTON DISTRICT SCHOOL BOARD**

13. The Halton District School Board has no objection to the development application subject to the following: **HDSB**
1. The owner agrees to place the following notification in all offers of purchase and sale for all lots/units:
    - a. Prospective purchasers are advised that attendance at schools in the area is not guaranteed. Pupils may be accommodated in temporary facilities and/or be directed to schools outside of the area. School attendance areas are subject to change and/or redirections can be put into place to address school accommodation pressures.
    - b. Prospective purchasers are advised that school busses will not enter cul- de- sacs and pick up points will be generally located on through streets convenient to the Halton Student Transportation Services. Additional pick up points will not be located within the subdivision until major construction activity has been completed.
  2. That in cases where offers of purchase and sale have already been executed, the owner sends a letter to all purchasers which include the above statement.
  3. That a copy of the approved sidewalk plan, prepared to the satisfaction of the Town of Oakville be submitted to the Halton District School Board.

## **BELL CANADA**

14. The Owner acknowledges and agrees to convey any easement(s) as deemed necessary by Bell Canada to service this new development. The Owner further agrees and acknowledges to convey such easements at no cost to Bell Canada. **BC**
15. The Owner agrees that should any conflict arise with existing Bell Canada facilities where a current and valid easement exists within the subject area, the Owner shall be responsible for the relocation of any such facilities or easements at their own cost.

## **CANADA POST**

16. That the owner provides written confirmation that all Canada Post matters have been satisfactorily addressed. **CP**

## **ENBRIDGE GAS INC. (UNION GAS)**

17. That the owner provides written confirmation that all Enbridge Gas inc. matters have been satisfactorily addressed. **UG**

## **CLOSING CONDITIONS**

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|-----|--|------------------------------|
| 18. | Prior to signing the final plan the Director of Planning and Development shall be advised that all conditions have been carried out to the satisfaction of the relevant agencies, and that a brief but complete statement detailing how each condition has been satisfied has been provided. | <b>OAK (A)</b>               |
| 19. | Prior to signing the final plan, the Director of Planning and Development shall be advised by Halton Region that all related condition(s) has been carried out to their satisfaction with a brief but complete statement detailing how the condition has been satisfied.                     | <b>OAK(A),<br/>RMH (PPW)</b> |
| 20. | Prior to signing the final plan, the Director of Planning and Development shall be advised by Oakville Hydro that all related condition(s) has been carried out to their satisfaction with a brief but complete statement detailing how the condition has been satisfied.                    | <b>OAK(A)<br/>OH</b>         |
| 21. | Prior to signing the final plan, the Director of Planning and Development shall be advised by Bell Canada that all related condition(s) has been carried out to their satisfaction with a brief but complete statement detailing how the condition has been satisfied.                       | <b>OAK(A)<br/>BC</b>         |
| 22. | Prior to signing the final plan, the Director of Planning and Development shall be advised by Canada Post that all related condition(s) has been carried out to their satisfaction with a brief but complete statement detailing how the condition has been satisfied.                       | <b>OAK(A)<br/>CP</b>         |
| 23. | Prior to signing the final plan, the Director of Planning and Development shall be advised by Enbridge Gas Inc. that all related condition(s) has been carried out to their satisfaction with a brief but complete statement detailing how the condition has been satisfied.                 | <b>OAK (A)<br/>UG</b>        |
|     | All of the above conditions shall be satisfied within 3 years of the granting of draft approval, being <b><i>Month Day, Year.</i></b> (Date of Draft Approval to be inserted as the day after the last date for appeals if no appeals are received).   | <b>OAK (A)</b>               |

**NOTES – The owner is hereby advised:**



1. If the condominium is not registered within 3 years of the date of draft plan approval, then this approval shall be null and void and the plans and drawings must be resubmitted to the Town of Oakville for approval.
2. Fees are required by Halton Region for each extension to draft approval for major revisions to the draft plan or conditions and for registration of the plan.
3. Educational Development Charges are payable in accordance with the applicable Education Development Charge By-law and are required at the issuance of a building permit. Any building permits which are additional to the maximum unit yield which is specified by the Subdivision Agreement are subject to Education Development Charges prior to the issuance of a building permit, at the rate in effect at the date of issuance.
4. Please note the Owner should be made aware that Halton Region will have the following requirements at the time of registration of the condominium:
  - Final draft condominium plans signed and dated by the Owner, Surveyor and initialed by the Town's Planner;
  - Regional Registration fee;
  - Registry Office review form (Appendix D Form (Formerly Schedule J Form)); and,
  - Clearance letter from Applicant/Owner indicating how the Region's *Schedule A* conditions of draft approval have been addressed.

#### **LEGEND – CLEARANCE AGENCIES**

BC	Bell Canada
CP	Canada Post
HDSB	Halton District School Board
HCDSB	Halton Catholic District School Board
OAK (A)	Town of Oakville – Planning Administration
OAK (F)	Town of Oakville - Finance
OAK (L)	Town of Oakville – Legal
OAK (DE)	Town of Oakville – Development Services Department
OAK (Z)	Town of Oakville – Building Services Department, Zoning Section
RMH	Regional Municipality of Halton
UG	Enbridge Gas inc./Union Gas

## REPORT

### Planning and Development Council

Meeting Date: March 17, 2025

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**FROM:** Planning and Development Department

**DATE:** March 4, 2025

**SUBJECT:** Public Meeting Report – Plan of Subdivision and Zoning By-law Amendment, Argo Lions Valley Ltd., 1303 Dundas Street West, File No. 24T-25002/1323 and Z.1323.01

**LOCATION:** 1303 Dundas Street West

**WARD:** Ward 7

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#### RECOMMENDATION

1. That comments from the public with respect to the proposed Plan of Subdivision and Zoning By-law Amendment submitted by Argo Lions Valley Ltd. (File No.: 24T-25002/1323 and Z.1323.01), be received.
2. That staff consider such comments as may be provided by Council.

#### KEY FACTS

The following are key points for consideration with respect to this report:

- **Nature of Application:** The applicant has applied for a zoning by-law amendment and plan of subdivision. The purpose of the applications are to amend Zoning By-law 2009-189 (the “**Zoning By-law**”) and register a plan of subdivision for the subject property to permit the proposed development.
- **Proposal:** The proposal consists of 131 residential dwellings made up of single detached dwellings, dual frontage townhouse dwellings, back-to-back townhouse dwellings and street-oriented townhouse dwellings organized around two future development blocks, a Stormwater Management (SWM) Pond, retention of the on-site and surrounding natural areas as well as new public roads.
- **Public Consultation:** The applicant hosted an in-person Public Information Meeting (“**PIM**”) on December 10, 2024. Councillors Nanda and Xie attended.

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## BACKGROUND

The subject property is currently occupied by radio towers that are required to be removed to accommodate the proposed development.

## APPLICATION SUMMARY

**Applicant/Owner:** Argo Lions Valley Ltd.

**Purpose of Application(s):** The purpose of the zoning by-law amendment and plan of subdivision applications is to rezone the majority of the site from Future Development (FD) to General Urban (GU), Natural Heritage System (NHS) and Stormwater Management Facility (SMF) to allow for 131 residential dwelling units in the form of detached and townhouse dwellings, create two future development blocks along Dundas Street West, create a block for a stormwater management facility, and delineate and dedicate Natural Heritage System blocks to the town.

An aerial photograph, existing Official Plan land use schedules, and an existing zoning excerpt from By-law 2009-189 are included in **Appendix 'A'**.

**Effect of Application(s):** The effect of the Official Plan and Zoning By-law Amendment applications is to:

- Permit a combination of townhouse blocks and detached lots resulting in 131 dwellings.
- Create two future development blocks, a stormwater management block and delineate and dedicate natural heritage system blocks to the Town.

A copy of the applicant's 3D rendering and elevations are included as **Appendix 'B'**.

A copy of the applicant's draft Zoning By-law Amendment is included as **Appendix 'C'**.

## Submitted Plans / Reports:

The proponent has provided technical supporting studies which are currently under review by various public agencies and internal town departments. A full circulation and assessment of the application was undertaken. The following studies and supporting documentation are accessible on the Town's website by visiting [www.oakville.ca](http://www.oakville.ca) and searching Z.1323.01.

**Property Location:** The subject lands are located on the north side of Dundas Street West between Pavilion Path and Lions Valley Park and are municipally known as 1303 Dundas Street West.

**Surrounding Land Uses:**

Surrounding the site are the following:

- North – Sixteen Mile Creek Valley and lands zoned for townhouses to the northwest
- East - Sixteen Mile Creek and Lions Valley Park
- South – south of Dundas Street West is a Cultural Centre with a cemetery and residential neighbourhood consisting mostly of detached dwellings beyond that
- West – residential community consisting of varying built forms

**Key Milestones:**

Pre-Consultation Meeting	October 2, 2024
Public Information Meeting	December 10, 2024
Pre-submission Review	N/A
Application Deemed Complete	January 23, 2025
P & D Council - Public Meeting	March 17, 2025
Date Eligible for Appeal for Non-decision of the zoning by-law amendment	April 25, 2025
Date Eligible for Appeal for Non-decision of the plan of subdivision	May 23, 2025

**PLANNING POLICY CONTEXT**

The subject property is subject to the following policy and regulatory framework:

- Provincial Planning Statement, 2024
- Halton Region Official Plan
- North Oakville East Secondary Plan
- Zoning By-law 2009-189

A full analysis of the Provincial Planning Statement (2024), Halton Regional Official Plan, and North Oakville East Secondary Plan will be included within the future recommendation report.

Official Plan extracts are attached as **Appendix ‘D’** and Zoning By-law extracts are attached as **Appendix ‘E’**.

## **MATTERS UNDER REVIEW**

This application was recently received and is under review by Town departments and agencies. The following are the general issues that will be addressed in a future recommendation report, in addition to any comments from the public, Council and commenting agencies:

- Public & Council Comments/Concerns
- Climate Change/Sustainability Goals
- Consideration of applicable Provincial and Regional policy
- Conformity with North Oakville East Secondary Plan
- Zoning performance standards
- Proposed use and density
- Context and transition to adjacent properties and built form
- Integration/Impact on adjoining and adjacent properties
- Proposed height and setbacks
- Urban design
- Transportation implications (including travel demand management strategies, and parking utilization)
- Pedestrian connections and walkability
- Tree preservation
- Stormwater management
- Natural Heritage System
- Functional servicing

## **CONCLUSION**

Planning staff will continue to review and analyze the subject plan of subdivision and rezoning applications and address all technical matters, if any, along with submitted public comments. No further notice is required; however, written notice of any future public meetings will be provided to those who have made written and/or verbal submissions.

---

## CONSIDERATIONS

### (A) PUBLIC

The applicant held a Public Information Meeting on December 10, 2024, and the Ward 7 Councillors Xie and Nanda attended. The applicant has included a Public Information Meeting Report which includes a summary of the comments received. All other public comments received as of the date of this report are included as “**Appendix ‘F’**”.

Notice of complete application and public meeting were distributed to property owners within 240m of the subject property in accordance with the town’s current notice requirements and *Planning Act*.

### (B) FINANCIAL

None.

### (C) IMPACT ON OTHER DEPARTMENTS & USERS

The application was circulated to internal and external departments and agencies for review. The application remains in technical review.

### (D) COUNCIL STRATEGIC PRIORITIES

This report addresses Council’s strategic priority/priorities:

- Growth Management, Community Belonging, Environmental Sustainability and Accountable Government.

### (E) CLIMATE CHANGE/ACTION

The proposed development will be reviewed to ensure compliance with the Town’s sustainability objectives of the Oakville Plan

## APPENDICES

Appendix “A”: Mapping

Appendix “B”: 3D Rendering and Elevations

Appendix “C”: Applicant’s Draft Zoning By-law Amendment

Appendix “D”: Official Plan Extracts

Appendix “E”: Zoning By-law Extracts

Appendix “F”: Public Written Submissions

Prepared by:

Catherine Buckerfield, M.Pl., MCIP, RPP  
Senior Planner, Current Planning - West

Recommended by:

Paul Barrette, MCIP RPP  
Manager of Current Planning – West

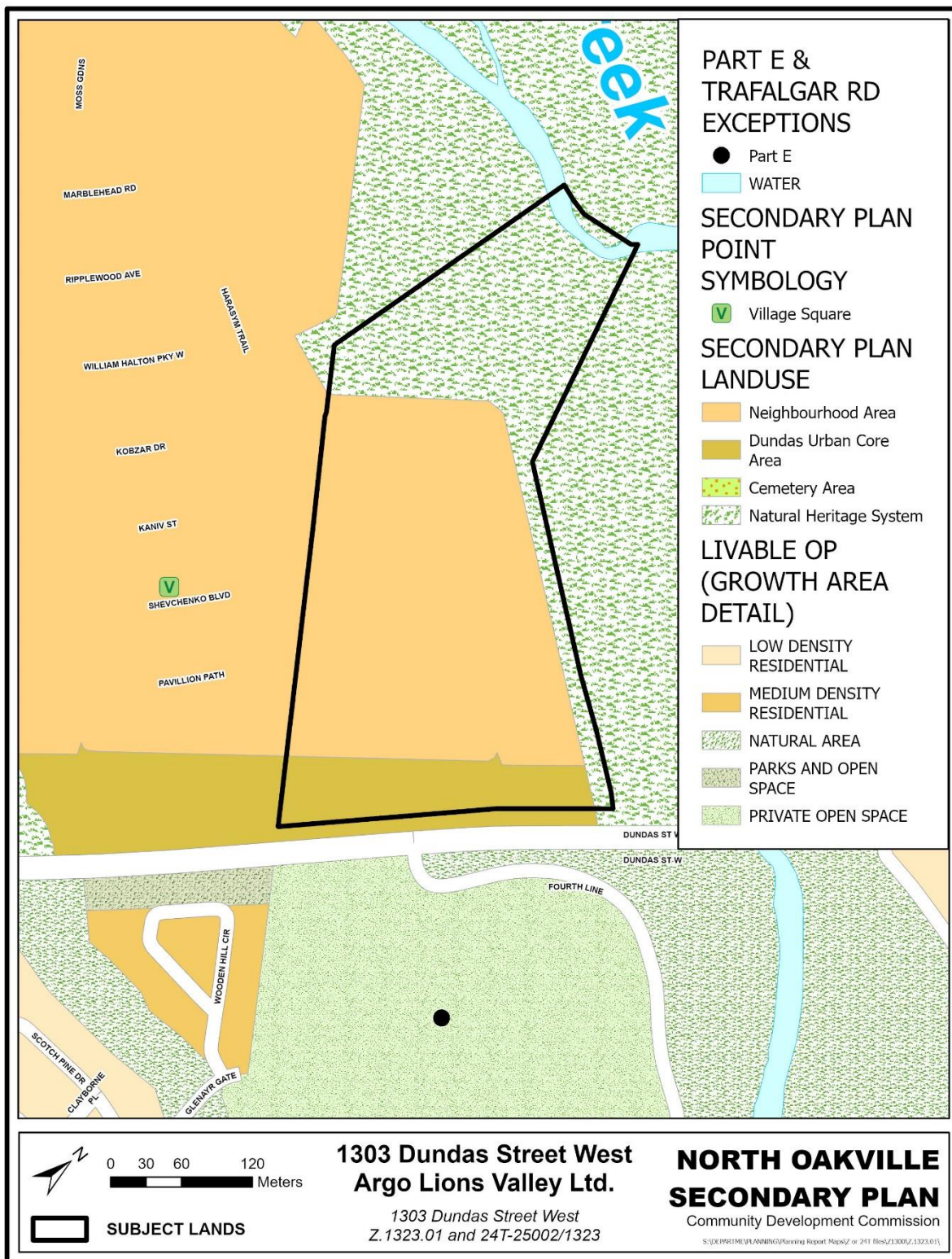
Submitted by:

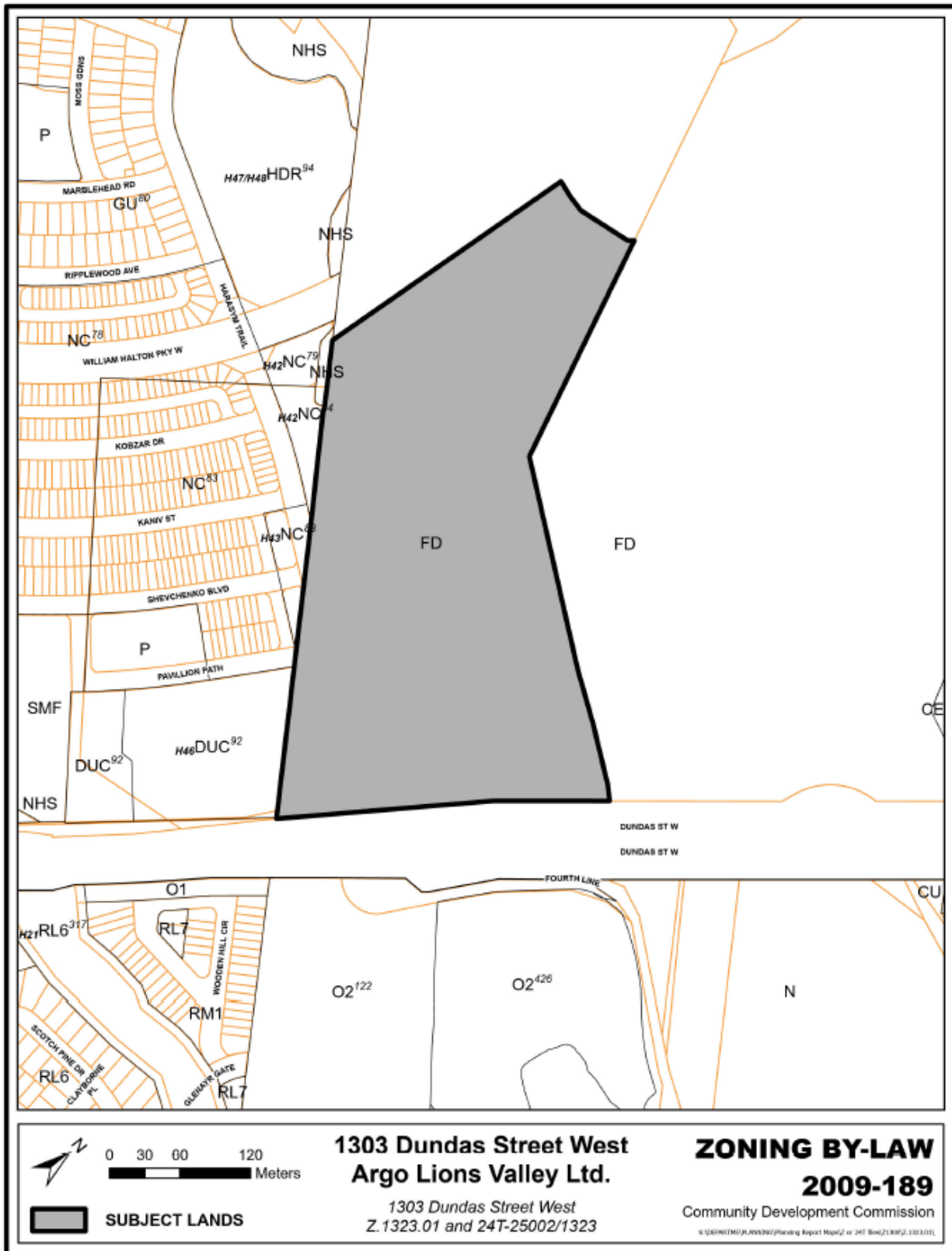
Gabriel A.R. Charles, MCIP RPP  
Director, Planning Services

## APPENDIX A: Mapping











## APPENDIX B: Renderings and Elevations



























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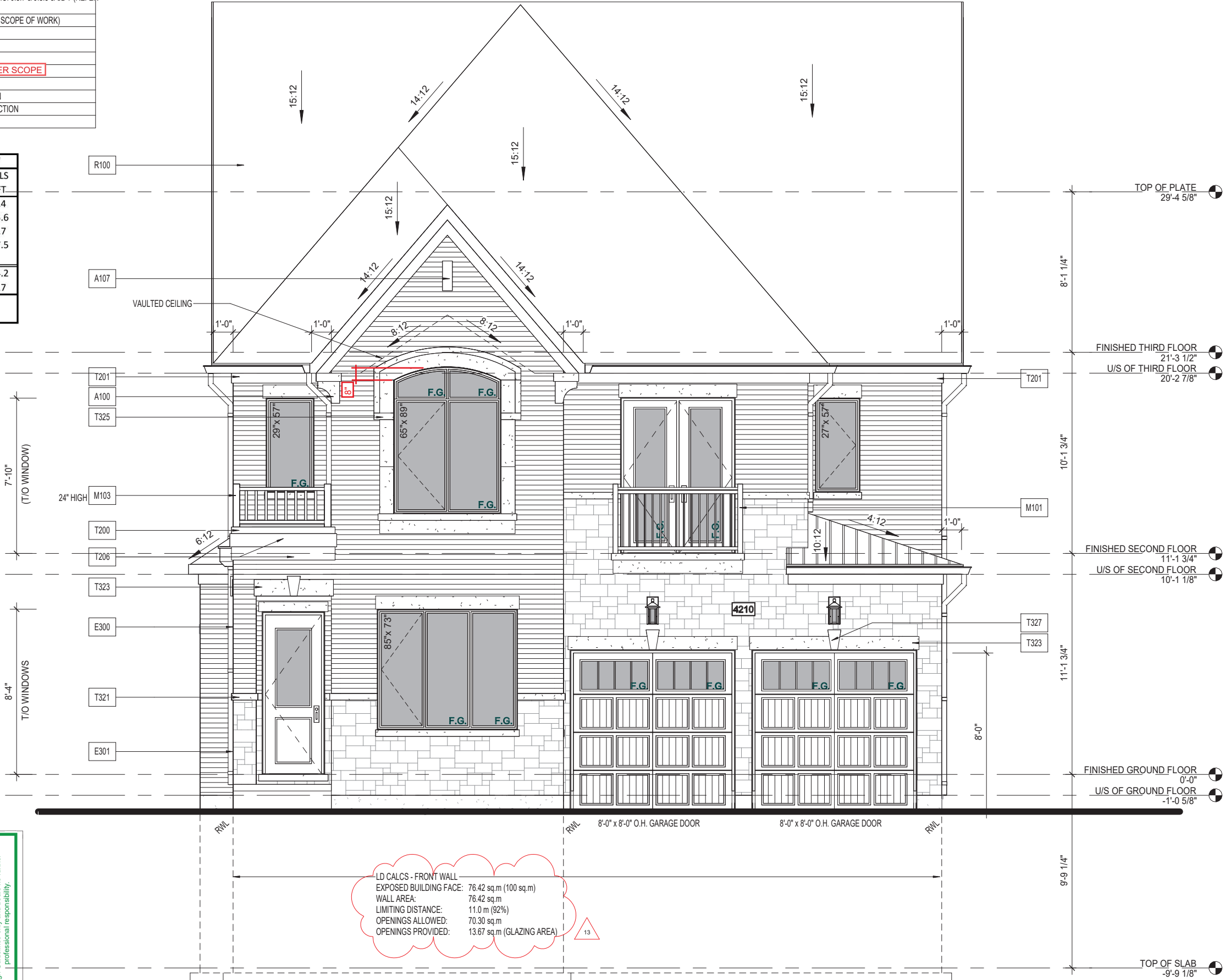
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**JOHN G. WILLIAMS LTD., ARCHITECT**  
ARCHITECTURAL CONTROL REVIEW AND APPROVAL  
APPROVED BY:   
DATE: Dec 05, 2023  
This stamp certifies compliance with the applicable Design Guidelines only and bears no further professional responsibility.

NOTE:  
\*ALL ROOF OVERHANG DIMENSIONS ARE FROM EXTERIOR OF FINISHES.

Material Legend	
A100	DECORATIVE BRACKET (SEE DETAIL)
A107	DECORATIVE GABLE DETAIL (SEE DETAIL)
E300	FACE BRICK (TYP.)
E301	STONE VENEER (TYP.)
M101	ALUMINUM RAILING INSTALLED TO COMPLY W/ O.B.C. 9.8.7 & 9.8.8 & SB-7 (REFER TO SCOPE OF WORK)
M103	DECORATIVE WROUGHT IRON RAILING (REFER TO SCOPE OF WORK)
R100	ASPHALT SHINGLES (TYP)
T200	ALUM. CLAD TRIM BOARD
T201	ALUM. CLAD FRIEZE BOARD
T206	ALUM. CLAD TRIM SURROUND <b>OR PVC PER SCOPE</b>
T321	PRECAST CONCRETE SILL W/ 1/2" PROJECTION
T323	SMOOTH PRECAST OR STONE W/ 1/2" PROJECTION
T325	SELF SUPPORTING PRECAST ARCH W/ 1/2" PROJECTION
T327	PRECAST CONCRETE KEYSTONE

ELEVATION STYLE	ELEVATION 'A'	
AREA OF GLAZING OR WALLS	GLAZ. SQ.FT	WALLS SQ.FT
FRONT ELEVATION	165.9	986.4
LEFT SIDE ELEVATION	77.5	1303.6
REAR ELEVATION	278.5	976.7
RIGHT SIDE ELEVATION	14.7	1337.5
TOTAL SQ.FT (FT²)	536.5	4604.2
TOTAL SQ.M (m²)	49.8	427.7
RATIO OF GLAZING TO WALLS	11.65%	



LD CALCS - FRONT WALL  
EXPOSED BUILDING FACE: 76.42 sq.m (100 sq.m)  
WALL AREA: 76.42 sq.m  
LIMITING DISTANCE: 11.0 m (92%)  
OPENINGS ALLOWED: 70.30 sq.m  
OPENINGS PROVIDED: 13.67 sq.m (GLAZING AREA)

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Q4 ARCHITECTS

ARCHITECTS

ASSOCIATION of ARCHITECTS

FRANCES MARTIN DI GIUSEPPE  
LICENCE 7216

Sheet Title

**A - FRONT ELEVATION**

Project Title

**CAVAN - TASD 42' MODULE**

Location

**ONTARIO**

Client

**CAVAN**

Project No.

20028

Scale

3/16" = 1'-0"

Drawn By

JF

Checked By

P.M./K.B.

13

REVISED AS PER CITY COMMENTS

08/24/23

JF

12

ISSUED FOR PERMIT

05/13/23

JF

11

ISSUED FOR ENGINEER FINAL REVIEW

05/03/23

JF

10

ISSUED FOR ENGINEER REVIEW

04/17/23

JF

9

ISSUED FOR CONSULTANT'S REVIEW

04/04/23

JF

8

FINAL DD ISSUED FOR CLIENT

10/25/22

PC

7

FINAL DD PACKAGE ISSUED FOR CLIENT REVIEW

06/30/22

NR

6

REVISED FINAL SD PACKAGE

09/20/21

JE

5

FINAL SCHEMATIC DESIGN ISSUED

05/28/21

KB

4

FULL SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW

05/21/21

KB

3

CON ELEVATIONS ISSUED FOR CLIENT REVIEW

05/14/21

KB

2

SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW

05/07/21

KB

1

PRELIM SCHEMATIC DESIGN

04/06/21

KB

13

ISSUED FOR REVIEW

12

ISSUED FOR REVIEW

11

ISSUED FOR REVIEW

10

ISSUED FOR REVIEW

9

ISSUED FOR REVIEW

8

ISSUED FOR REVIEW

7

ISSUED FOR REVIEW

6

ISSUED FOR REVIEW

5

ISSUED FOR REVIEW

4

ISSUED FOR REVIEW

3

ISSUED FOR REVIEW

2

ISSUED FOR REVIEW

1

ISSUED FOR REVIEW

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LICENCE 7216

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TASD4210

Project No.

20028

Scale

3/16" = 1'-0"

Drawn By

JF

Checked By

P.M./K.B.

A2.01



Material Legend	
E103	SMOOTH FIBER-CEMENT PANEL(REFER TO SCOPE OF WORK)
E204	WOOD - LIKE CLADDING WALL FINISH
E301	STONE VENEER (TYP.)
P103	PORCH PIER W/ TRIMMED PANELS (SEE DETAIL)
R100	ASPHALT SHINGLES (TYP)
R101	METAL VALLEY FLASHING
R103	PREFIN. ALUM. FASCIA, VENTED SOFFIT, GUTTER & DOWNSPOUTS
R111	METAL FLASHING (EXTEND 6" BENEATH CLADDING)
T109	METAL TRIM SYSTEM FOR FIBER-CEMENT PANELS - EASYTRIM OR EQUAL (TYP.)
T200	ALUM. CLAD TRIM BOARD
T208	ALUM.CLAD PORCH BEAM <b>OR PVC PER SCOPE</b>
T321	PRECAST CONCRETE SILL W/ 1/2" PROJECTION
T323	SMOOTH PRECAST OR STONE W/ 1/2" PROJECTION

**NOTE:**

- ALL WINDOW HEADERS ARE TO BE REMOVED FROM CONTEMPORARY STYLE

**NOTE:**

- ALL ROOF OVERHANG DIMENSIONS ARE FROM EXTERIOR OF FINISHES.
- WINDOWS AND FINISHES THAT APPEAR ALIGNED ARE ALIGNED
- REFER TO STANDARD ELEVATIONS FOR MATERIAL NOTATIONS



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**JOHN G. WILLIAMS LTD., ARCHITECT**  
ARCHITECTURAL CONTROL REVIEW  
AND APPROVAL

APPROVED BY:   
DATE: SEP 21 2023

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## B - FRONT ELEVATION

Sheet Title

Issued / Revision Chart

**TASD5011**

Project No. **20028**

Scale **3/16" = 1'-0"**

Drawn By **K.B./N.S.**

Checked By **P.M.**

Project Title  
**CAVAN - 50' MODULE**

Location  
**OAKVILLE, ONTARIO**

Client  
**CAVAN COMMUNITIES**

NO	ISSUED FOR PERMIT	DATE	BY
10	ISSUED FOR PERMIT	09/05/23	NR
9	ISSUED FOR FINAL ENG. REVIEW	07/21/23	JE
8	ISSUED FOR ENG. REVIEW	06/27/23	NS
7	PRELIM DD ISSUED FOR CLIENT	09/16/22	NR
6	REVIEW CON & PRELIM STRUCTURE ISSUED FOR CLIENT	10/15/21	JE
5	FINAL SD PACKAGE ISSUED FOR CLIENT	09/24/21	JE
4	PLAN & ELEVATION REDISIGN	09/17/21	KB
3	SCHEMATIC DESIGN ISSUED FOR CLIENT	09/10/21	GP
2	PRELIM SCHEMATIC DESIGN	09/03/21	GP
1	PRELIM SCHEMATIC INTERVAL	04/05/21	KB



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## B - FRONT ELEVATION (CONTEMPORARY)

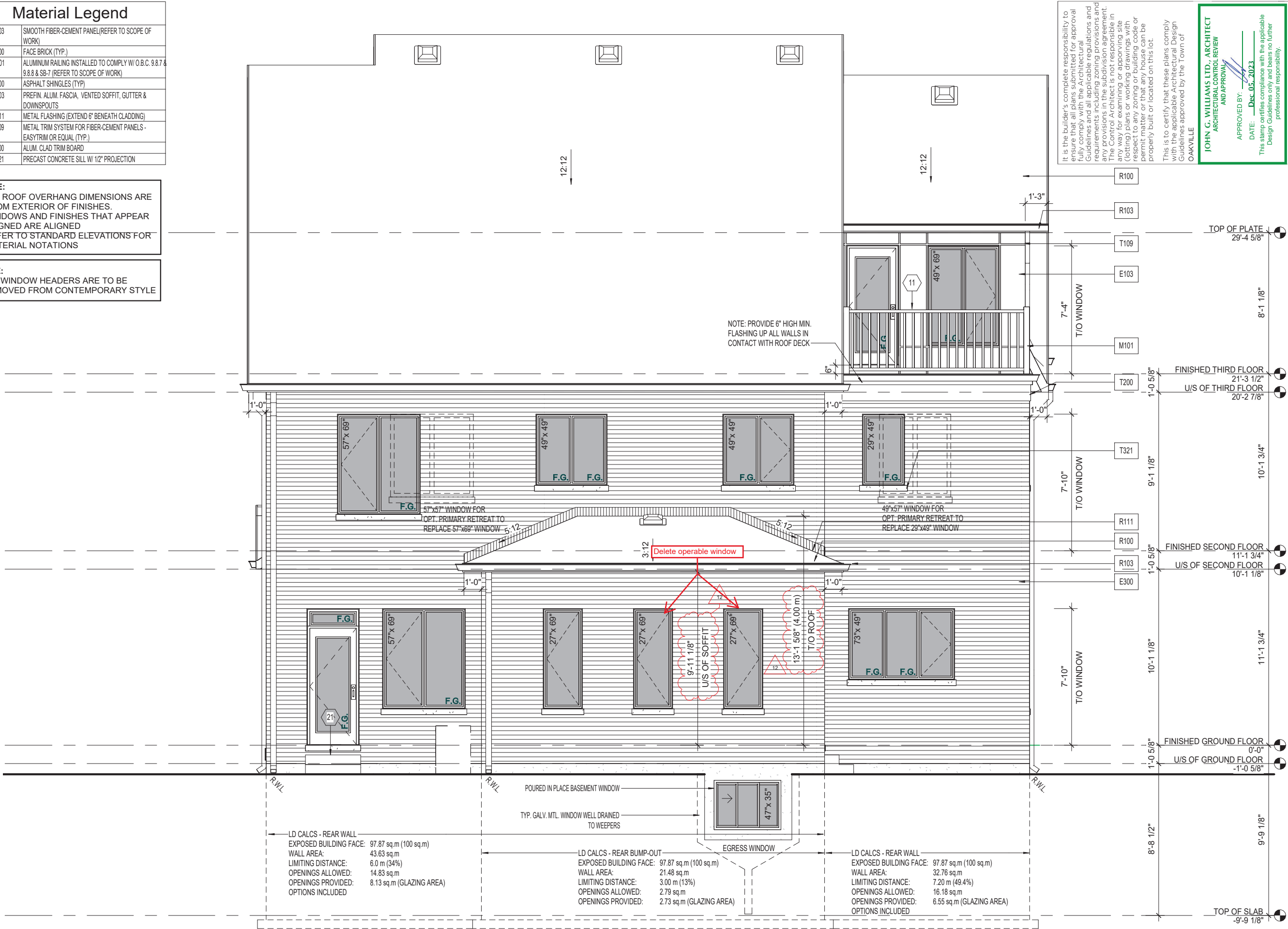
Material Legend	
E103	SMOOTH FIBER-CEMENT PANEL(REFER TO SCOPE OF WORK)
E300	FACE BRICK (TYP.)
M101	ALUMINUM RAILING INSTALLED TO COMPLY W/ O.B.C. 9.8.7 & 9.8.8 & SB-7 (REFER TO SCOPE OF WORK)
R100	ASPHALT SHINGLES (TYP)
R103	PREFIN. ALUM. FASCIA, VENTED SOFFIT, GUTTER & DOWNSPOUTS
R111	METAL FLASHING (EXTEND 6" BENEATH CLADDING)
T109	METAL TRIM SYSTEM FOR FIBER-CEMENT PANELS - EASYTRIM OR EQUAL (TYP.)
T200	ALUM. CLAD TRIM BOARD
T321	PRECAST CONCRETE SILL W/ 1/2" PROJECTION

**NOTE:**

- ALL ROOF OVERHANG DIMENSIONS ARE FROM EXTERIOR OF FINISHES.
- WINDOWS AND FINISHES THAT APPEAR ALIGNED ARE ALIGNED
- REFER TO STANDARD ELEVATIONS FOR MATERIAL NOTATIONS

**NOTE:**

- ALL WINDOW HEADERS ARE TO BE REMOVED FROM CONTEMPORARY STYLE



B - REAR ELEVATION (CONTEMPORARY)

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APPROVED BY:

DATE: Dec 05, 2023

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JOHN G. WILLIAMS LTD., ARCHITECT  
ARCHITECTURAL CONTROL REVIEW  
AND APPROVAL

B - REAR ELEVATION

Sheet Title

Issue / Revision Chart

TASD5011	
Project No.	20028
Scale	3/16" = 1'-0"
Drawn By	K.B./N.S.
Checked By	P.M.

Project Title	CAVAN - 50' MODULE
Location	OAKVILLE, ONTARIO
Client	CAVAN COMMUNITIES

10	REVISED AS PER CITY COMMENTS	10/17/23	KB
9	ISSUED FOR PERMIT	09/05/23	MR
8	ISSUED FOR FINAL ENG. REVIEW	07/21/23	JE
7	ISSUED FOR ENG. REVIEW	06/27/23	NS
6	PRELIM. DD ISSUED FOR CLIENT REVIEW	09/16/22	NR
5	REVIEW ON A PRELIM STRUCTURE ISSUED FOR CLIENT REVIEW	10/15/21	JE
4	FINAL SD PACKAGE ISSUED FOR PLAN & ELEVATION REDSIGN	09/24/21	JE
3	ISSUED FOR CLIENT REVIEW	09/17/21	KB
2	SCHEMATIC DESIGN ISSUED FOR PRELIM. SCHEMATIC DESIGN	09/10/21	GP
1	PRELIM. SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW	09/03/21	GP
		04/05/21	KB

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of  
ARCHITECTS

*Frances Martin Di Giuseppe*

FRANCES MARTIN DI GIUSEPPE  
LICENCE  
7216

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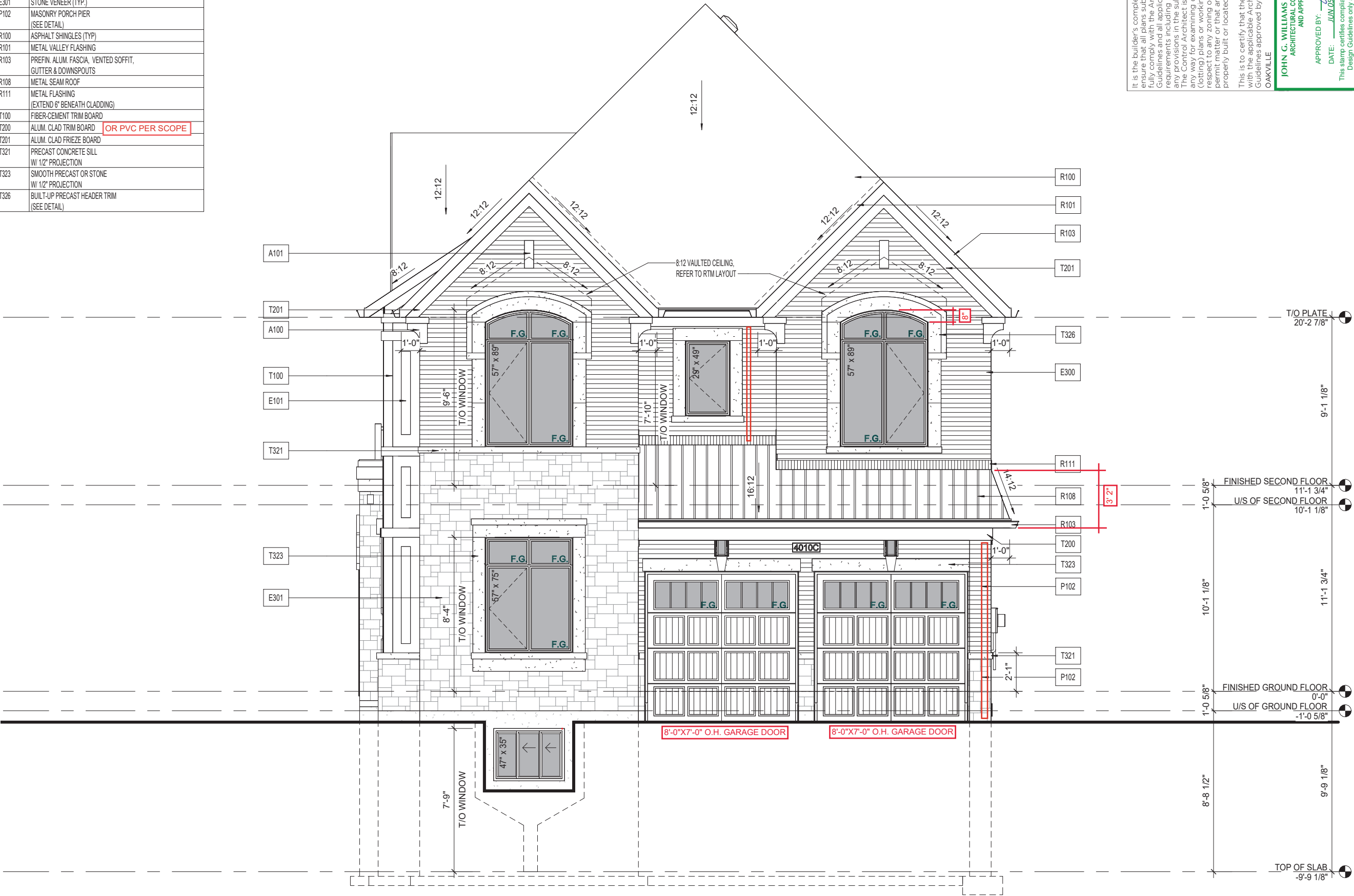
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B2.03



Material Legend	
A100	DECORATIVE BRACKET (SEE DETAIL)
A101	DECORATIVE FEATURE (SEE DETAIL)
E101	FIBER-CEMENT BOARD & BATTEN - SMOOTH PANEL W/ 1"x2" BATTENS @ 8" O.C.
E300	FACE BRICK (TYP.)
E301	STONE VENEER (TYP.)
P102	MASONRY PORCH PIER (SEE DETAIL)
R100	ASPHALT SHINGLES (TYP)
R101	METAL VALLEY FLASHING
R103	PREFIN. ALUM. FASCIA, VENTED SOFFIT, GUTTER & DOWNSPOUTS
R108	METAL SEAM ROOF
R111	METAL FLASHING (EXTEND 6" BENEATH CLADDING)
T100	FIBER-CEMENT TRIM BOARD
T200	ALUM. CLAD TRIM BOARD OR PVC PER SCOPE
T201	ALUM. CLAD FRIEZE BOARD
T321	PRECAST CONCRETE SILL W/ 1/2" PROJECTION
T323	SMOOTH PRECAST OR STONE W/ 1/2" PROJECTION
T326	BUILT-UP PRECAST HEADER TRIM (SEE DETAIL)

**NOTE:**  
• ALL ROOF OVERHANG DIMENSIONS ARE FROM EXTERIOR OF FINISHES.



A - FRONT ELEVATION

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APPROVED BY:   
DATE: JUN 05, 2023

JOHN G. WILLIAMS LTD., ARCHITECT  
ARCHITECTURAL CONTROL REVIEW  
AND APPROVAL

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Sheet Title

# A - FRONT ELEVATION (ENGLISH MANOR)

Project Title

## CAVAN - TASD 40' CORNER MODULE

Location

ONTARIO

Client

CAVAN

Project No.

20028

Scale

3/16" = 1'-0"

Drawn By

P.C./G.B.

Checked By

P.M./K.B.

# TASD4010C

# A2.01

Issued / Revision Chart

14	ISSUED FOR PERMIT	05/31/23	JE
13	ISSUED FOR FINAL ENG. REVIEW	05/16/23	JE
12	ISSUED FOR ENGINEER REVIEW	04/12/23	GB
10	FINAL DD ISSUED TO CLIENT	10/25/22	JF
9	REVIEW DD ISSUED FOR CLIENT	07/11/22	RR
8	REVISED FINAL SD PACKAGE ISSUED FOR CLIENT REVIEW	09/20/21	PC
7	FINAL REVIEW DD DESIGN ISSUED FOR CLIENT REVIEW	08/04/21	KB
6	SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW	07/30/21	KB
5	SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW	06/14/21	KB
4	SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW	06/07/21	KB
3	SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW	06/02/21	KB
2	FINAL REVIEW DD DESIGN ISSUED FOR CLIENT REVIEW	05/21/21	KB
1	SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW	05/07/21	KB

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**ONTARIO ASSOCIATION of ARCHITECTS**

*Frances Martin Di Giuseppe*

**FRANCES MARTIN DI GIUSEPPE**

**LICENCE 7216**

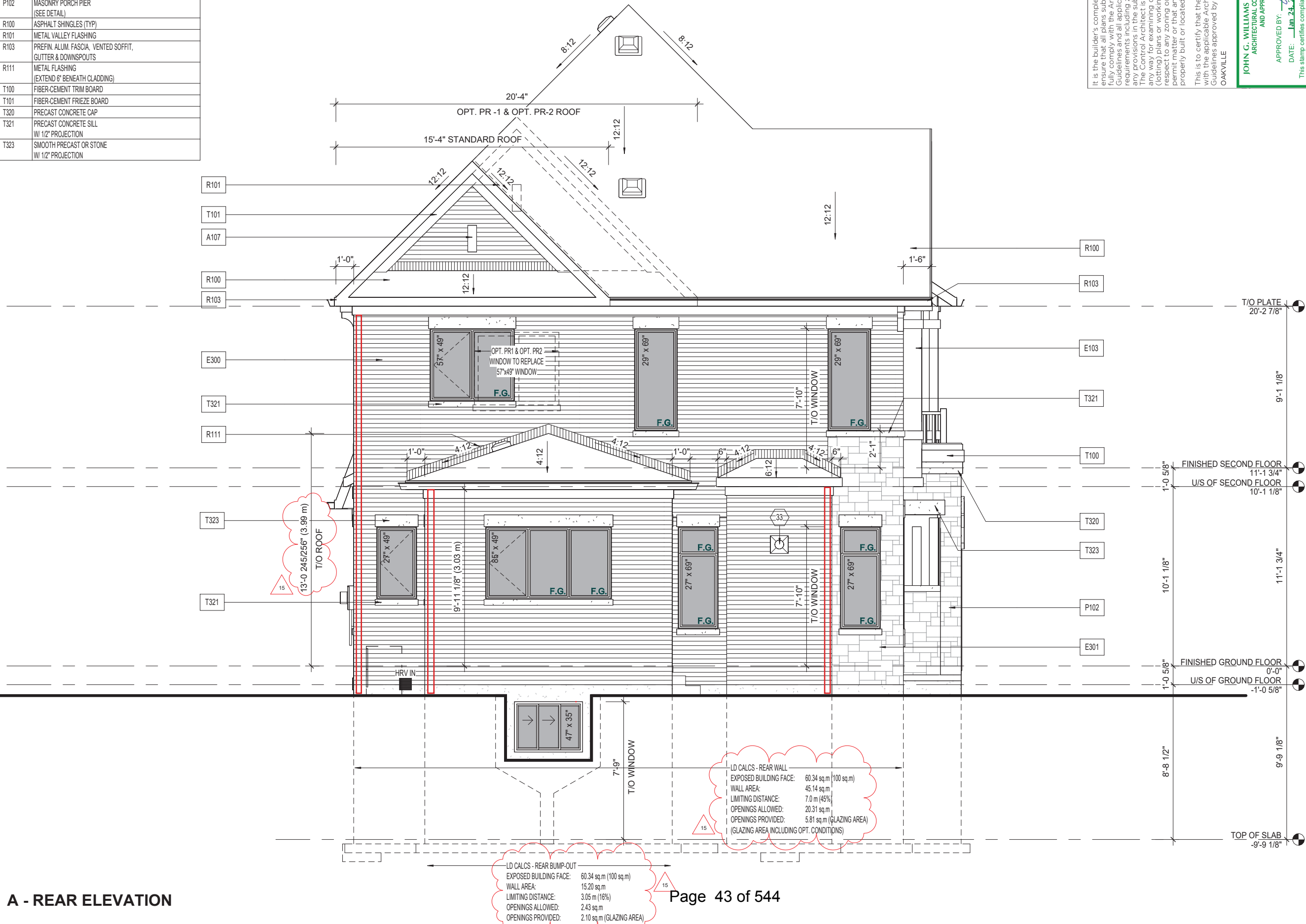




Material Legend	
A107	DECORATIVE GABLE DETAIL (SEE DETAIL)
E103	SMOOTH FIBER-CEMENT PANEL (REFER TO SCOPE OF WORK)
E300	FACE BRICK (TYP.)
E301	STONE VENEER (TYP.)
P102	MASONRY PORCH PIER (SEE DETAIL)
R100	ASPHALT SHINGLES (TYP)
R101	METAL VALLEY FLASHING
R103	PREFIN. ALUM. FASCIA, VENTED SOFFIT, GUTTER & DOWNSPOUTS
R111	METAL FLASHING (EXTEND 6" BENEATH CLADDING)
T100	FIBER-CEMENT TRIM BOARD
T101	FIBER-CEMENT FRIEZE BOARD
T320	PRECAST CONCRETE CAP
T321	PRECAST CONCRETE SILL W/ 1/2" PROJECTION
T323	SMOOTH PRECAST OR STONE W/ 1/2" PROJECTION

**NOTE:**

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JOHN G. WILLIAMS LTD., ARCHITECT  
ARCHITECTURAL CONTRACT REVIEW  
AND APPROVAL

APPROVED BY: \_\_\_\_\_  
DATE: Jan 24, 2024

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professional responsibility.

Sheet Title  
**A - REAR ELEVATION  
(ENGLISH MANOR)**

**CAIVAN - TASD 40' CORNER MODULE**

Location  
**ONTARIO**

15	ISSUED FOR PERMIT	01/07/23	KB
14	ISSUED FOR PERMIT	05/01/23	JE
13	ISSUED FOR FINAL ENG. REVIEW	05/08/23	JE
12	ISSUED FOR ENGINEER REVIEW	04/04/23	GB
11	FINAL DD. ISSUED TO CLIENT	10/25/22	JR
9	PRELIM. DD. ISSUED FOR CLIENT REVIEW	07/11/22	JR
8	ISSUED FOR FINAL SD PACKAGE	09/20/21	PC
7	FINAL SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW	08/04/21	KB
6	SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW	07/30/21	KB
5	SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW	06/14/21	KB
4	SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW	06/07/21	KB
3	SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW	06/02/21	KB
2	SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW	05/27/21	KB
1	SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW	05/07/21	KB



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TASD4010C

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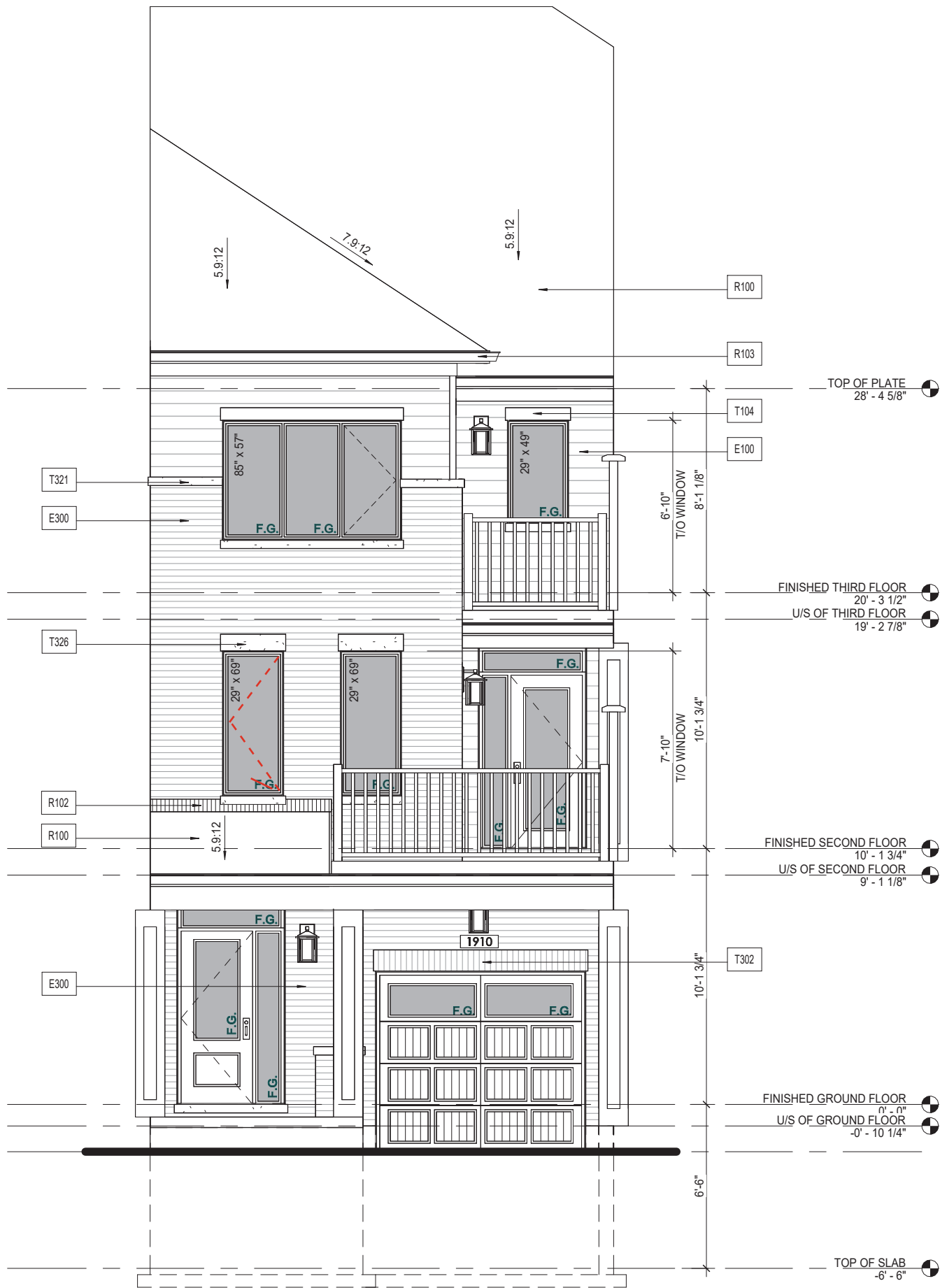
Project No. 20028

Scale  $3/16" = 1'-0"$

Drawn By	P.C./G.B.
Checked By	P.M./K.B.

## A2.03

**NOTE:**  
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## A - FRONT ELEVATION

# PROFESSIONAL STAMP

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4	FINAL SD ISSUED FOR CLIENT REVIEW	2024-05-24	DS
2	SD ISSUED FOR CLIENT REVIEW	2024-05-14	MR
1	PRELIM. SD ISSUED FOR CLIENT REVIEW	2024-05-03	DS

Page 44 of 544

Page 44 of 544

Sheet Title	
<b>FRONT ELEVATION</b>	
Project Title	
<b>CAIVAN - TCBB - BACK TO BACK TOWNS</b>	
Location	
<b>ONTARIO</b>	
Client	
<b>CAIVAN COMMUNITES</b>	

**TCBB1910**

---

Project No. **23026**

---

Scale **3/16" = 1'-0"**

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Drawn By **DS**

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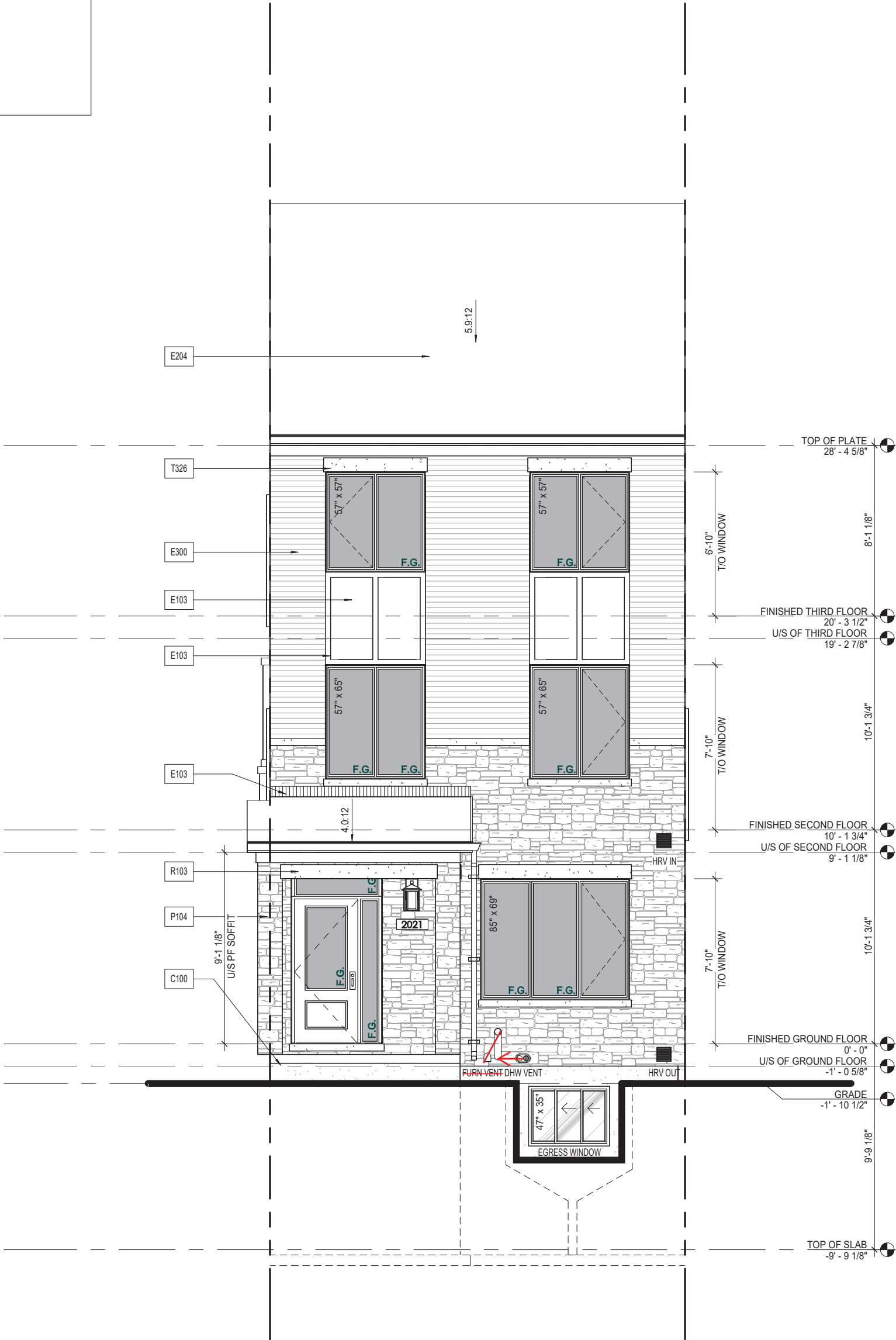
Checked By **PM & KB**

## A-4.1

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This is to certify that these plans comply with the applicable Architectural Design Guidelines approved by the City of  
BRAMPTON

Material Legend	
C100	POURED CONC. FOUNDATION WALLS AND FOOTING (TYP.)
E103	SMOOTH FIBER-CEMENT PANEL(REFER TO SCOPE OF WORK)
E204	WOOD - LIKE CLADDING WALL FINISH
E300	FACE BRICK (TYP.)
P104	SQUARE VINYL OR FIBERGLASS COLUMN
R103	PREFIN. ALUM. FASCIA, VENTED SOFFIT, GUTTER & DOWNSPOUTS
T326	BUILT-UP PRECAST HEADER TRIM (SEE DETAIL)



D - FRONT ELEVATION



**Q4 ARCHITECTS INC.**  
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Suite 602, Toronto ON.  
M2P 2B7  
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F. 416.322.7294  
E. info@q4architects.com

PROFESSIONAL  
STAMP

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Issued / Revision Chart

7	DD PACKAGE ISSUED FOR CLIENT REVIEW	2024.09.13	DS
6	DD PACKAGE ISSUED FOR CLIENT REVIEW	2024.08.23	GB
5	SD PACKAGE ISSUED FOR CLIENT APPROVAL	2024.05.09	MR
4	SD PACKAGE ISSUED FOR CLIENT APPROVAL	2024.05.02	MR
3	SD PACKAGE ISSUED FOR CLIENT APPROVAL	2024.04.16	MR
2	SD PACKAGE "A ELEVATION" ISSUED FOR CLIENT APPROVAL	2024.04.12	MR
1	PRELIM SD ISSUED FOR CLIENT REVIEW	2024.04.01	GB

Sheet Title

D - FRONT ELEVATION

Project Title

CAIVAN - TADF - DUAL FRONT TOWNS

Location

ONTARIO

Client

CAIVAN COMMUNITIES

TADF2021

Project No. 22015-2

Scale 3/16" = 1'-0"

Drawn By G.B./M.R.

Checked By K.B./P.M.

D2.01

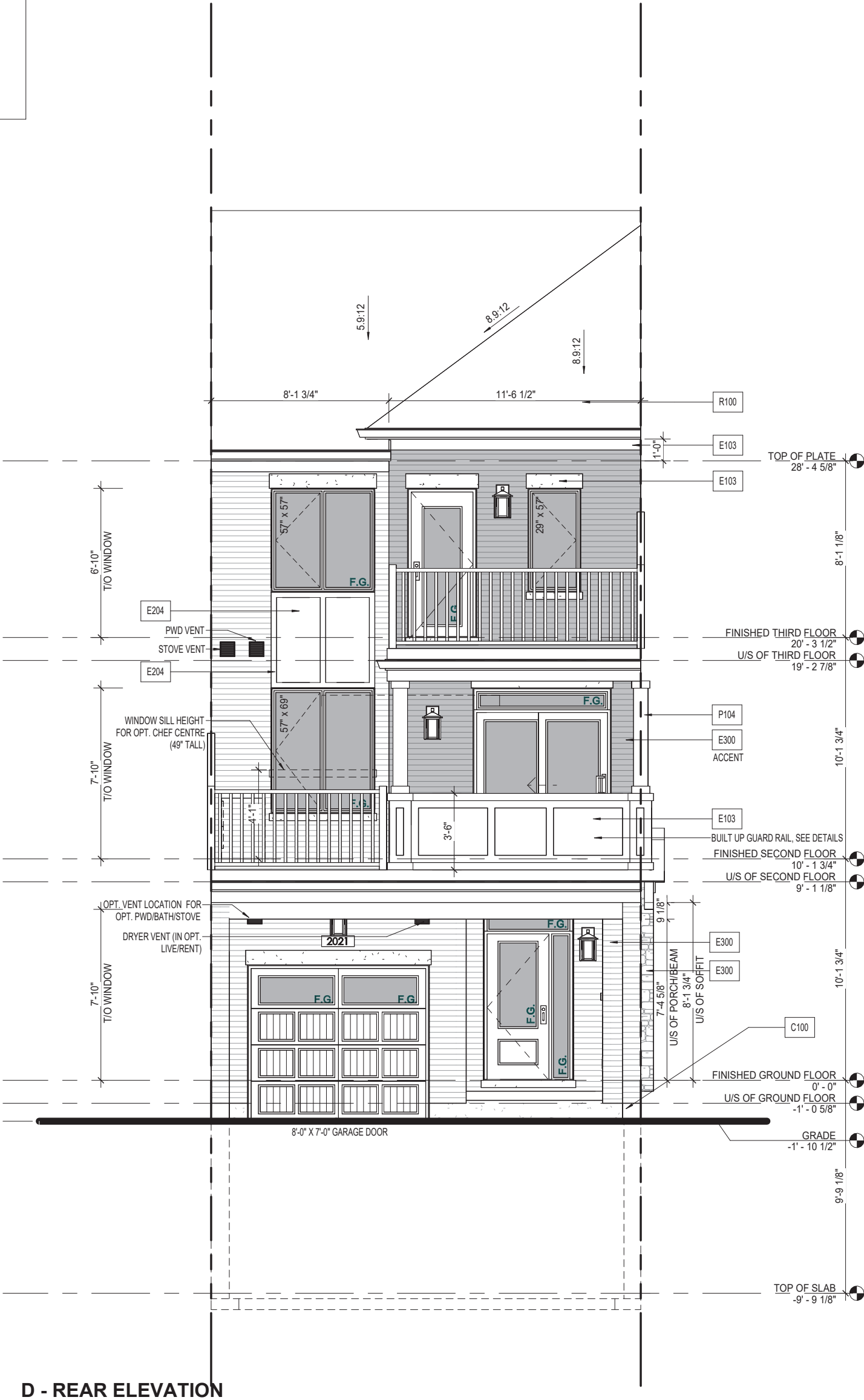
PM

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Material Legend

C100	POURED CONC. FOUNDATION WALLS AND FOOTING (TYP.)
E103	SMOOTH FIBER-CEMENT PANEL(REFER TO SCOPE OF WORK)
E204	WOOD - LIKE CLADDING WALL FINISH
E300	FACE BRICK (TYP.)
P104	SQUARE VINYL OR FIBERGLASS COLUMN
R100	ASPHALT SHINGLES (TYP)



D - REAR ELEVATION

Issued / Revision Chart

Sheet Title

D - REAR ELEVATION

Project Title

CAIVAN - TADF - DUAL FRONT TOWNS

Location

ONTARIO

Client

CAIVAN COMMUNITIES

TADF2021

Project No. 22015-2

Scale 3/16" = 1'-0"

Drawn By G.B./M.R.

Checked By K.B./P.M.

**Q4A**  
ARCHITECTS

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STAMP

7	DD PACKAGE ISSUED FOR CLIENT REVIEW	2024.09.13	DS
6	DD PACKAGE ISSUED FOR CLIENT REVIEW	2024.08.23	GB
5	SD PACKAGE ISSUED FOR CLIENT APPROVAL	2024.05.09	MR
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3	SD PACKAGE ISSUED FOR CLIENT APPROVAL	2024.04.16	MR
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1	PRELIM SD ISSUED FOR CLIENT REVIEW	2024.04.01	GB

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Material Legend	
E103	SMOOTH FIBER-CEMENT PANEL (REFER TO SCOPE OF WORK)
E300	FACE BRICK (TYP.)
E301	STONE VENEER (TYP.)
P101	SQUARE RECESSED PANEL COLUMN (SEE DETAIL)
R100	ASPHALT SHINGLES (TYP)
R101	METAL VALLEY FLASHING
R103	PREFIN. ALUM. FASCIA, VENTED SOFFIT, GUTTER & DOWNSPOUTS
R108	METAL SEAM ROOF
R111	METAL FLASHING (EXTEND 6" BENEATH CLADDING)
T101	FIBER-CEMENT FRIEZE BOARD
T208	ALUM.CLAD PORCH BEAM <b>OR PVC PER SCOPE</b>
T321	PRECAST CONCRETE SILL W/ 1/2" PROJECTION
T323	SMOOTH PRECAST OR STONE W/ 1/2" PROJECTION

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This is to certify that these plans comply with the applicable Architectural Design Guidelines approved by the Town of OAKVILLE

JOHN G. WILLIAMS LTD., ARCHITECT  
ARCHITECTURAL CONTROL REVIEW  
AND APPROVAL

APPROVED BY:

DATE: **Jan 09, 2024**

This stamp certifies compliance with the applicable Design Guidelines only and bears no further professional responsibility.



FRONT ELEVATION - ENGLISH MANOR (A)



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Issued / Revision Chart

18	PORCH ROOF REV. AS PER CLIENT COMMENTS	12/05/23	KB
16	REVISED PER ENGINEER REVIEW	10/12/22	PC
15	REVISED PER STAIR MANUF. COMMENTS	08/11/22	PC
14	ISSUED FOR PERMIT	04/21/22	JE
13	ISSUED FOR PERMIT WITH 9" FOUNDATIONS	04/08/22	PC
12	FINAL COORDINATION ISSUED FOR ENG. REVIEW	03/14/22	PC
11	ISSUED FOR CLIENT & ENGINEER REVIEW	02/18/22	PC
10	ADDITIONAL GROUND FLOOR POWDER ROOM OPTION	01/13/22	PC
9	FULL DD PACKAGE ISSUED FOR CLIENT REVIEW	12/10/21	PC
8	REVISED 9' SECOND FLOOR CEILING HEIGHTS AS PER CLIENT REQUEST	11/12/21	JE
7	REVISED ELEVATIONS & PLANS ISSUED FOR CLIENT REVIEW	05/13/21	LS
6	SCHEMATIC DESIGN w/ CONTENTS	04/23/21	KB

Sheet Title

FRONT ELEVATION

Project Title

CAIVAN - TATH24 MODULE

Location

Client

CAIVAN COMMUNITIES

TATH2401

Project No. 20028

Scale 3/16" = 1'-0"

Drawn By PC

Checked By CT/KB

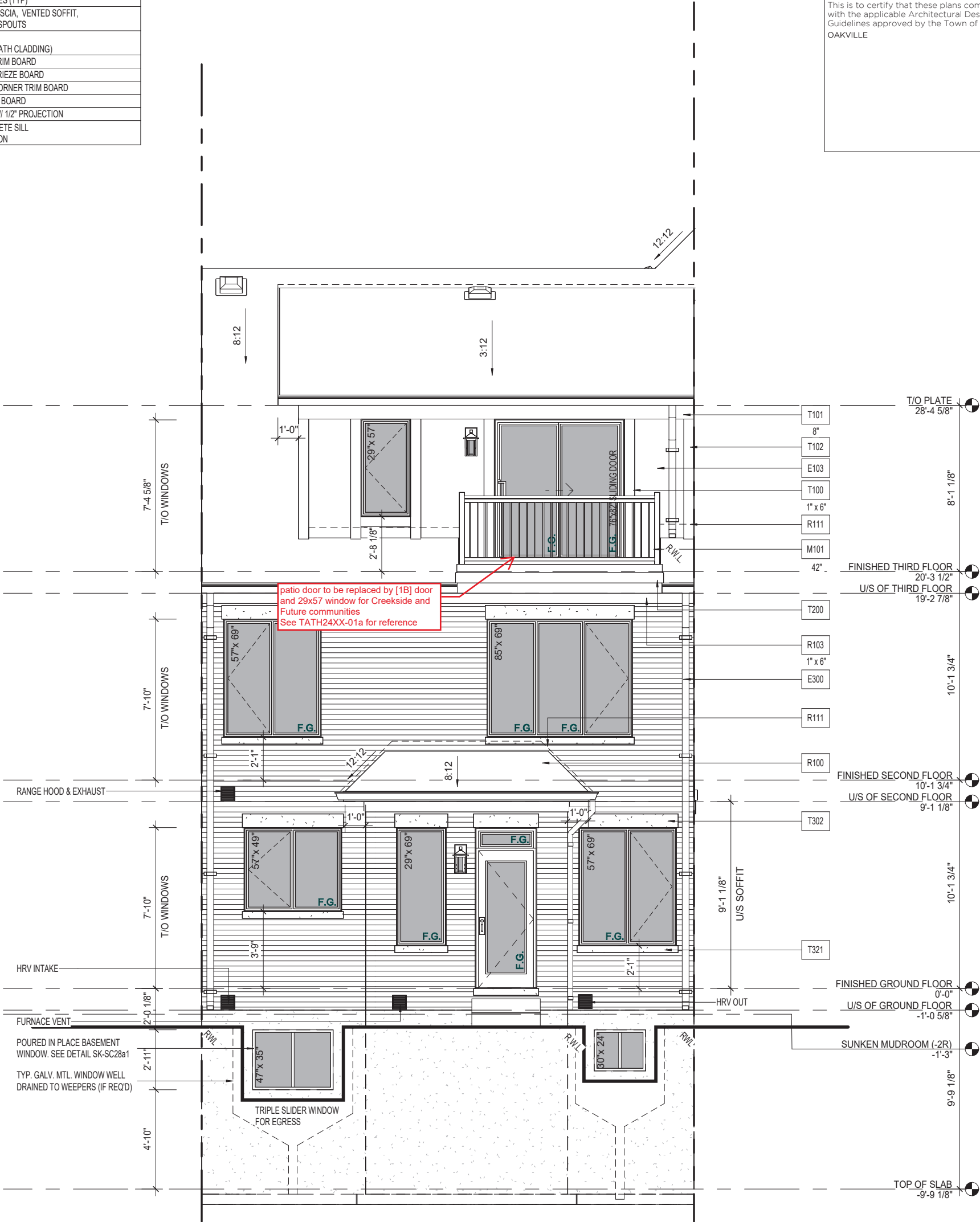
A2.01



Material Legend	
E103	SMOOTH FIBER-CEMENT PANEL (REFER TO SCOPE OF WORK)
E300	FACE BRICK (TYP.)
M101	42" ALUMINUM RAILING INSTALLED TO COMPLY W/ O.B.C. 9.8.7 & 9.8.8 & SB-7 (REFER TO SCOPE OF WORK)
R100	ASPHALT SHINGLES (TYP)
R103	PREFIN. ALUM. FASCIA, VENTED SOFFIT, GUTTER & DOWNSPOUTS
R111	METAL FLASHING (EXTEND 6" BENEATH CLADDING)
T100	FIBER-CEMENT TRIM BOARD
T101	FIBER-CEMENT FRIEZE BOARD
T102	FIBER-CEMENT CORNER TRIM BOARD
T200	ALUM. CLAD TRIM BOARD
T302	BRICK SOLDIER W/ 1/2" PROJECTION
T321	PRECAST CONCRETE SILL W/ 1/2" PROJECTION

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REAR ELEVATION - ENGLISH MANOR (A)



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Issued / Revision Chart			
16	REVISED PER ENGINEER REVIEW	10/12/22	PC
15	REVISED PER STAIR MANUF. COMMENTS	08/11/22	PC
14	ISSUED FOR PERMIT	04/27/22	JE
13	ISSUED FOR PERMIT WITH 9" FOUNDATIONS	04/08/22	PC
12	FINAL COORDINATION ISSUED FOR ENG. REVIEW	03/14/22	PC
11	ISSUED FOR CLIENT & ENGINEER REVIEW	02/18/22	PC
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7	REVISED ELEVATIONS & PLANS ISSUED FOR CLIENT REVIEW	05/13/21	LS
6	SCHEMATIC DESIGN W/ CONTEMPORARY ELEVATIONS	04/23/21	KB
5	SCHEMATIC DESIGN ISSUED FOR CLIENT REVIEW	04/16/21	KB

Sheet Title  
**REAR ELEVATION**

Project Title  
**CAIVAN - TATH24 MODULE**

Location  
**CAIVAN COMMUNITIES**

**TATH2401**

Project No. **20028**  
Scale **3/16" = 1'-0"**  
Drawn By **PC**  
Checked By **CT/KB**

**A2.02**

## THE CORPORATION OF THE TOWN OF OAKVILLE BY-LAW NUMBER 2025-XXX

A by-law to amend the Town of Oakville Zoning By-law 2009-189, as amended, to permit the development of a medium and low density residential subdivision, for the lands described as 1303 Dundas Street West (Argo Lions Valley Limited, File No. Z.XXXX.XX)

### COUNCIL ENACTS AS FOLLOWS:

1. Map 12(3) of By-law 2009-189, as amended, is further amended by rezoning the lands depicted on Schedule 'A' of this By-law.
2. Section 8, Special Provisions, of By-law 2009-189, as amended, is further amended by adding new Section 8.XX as follows:

XXX	1303 Dundas Street West	Parent Zone: GU
Map 12(3)	(Part of Lot 21, Concession 1, Trafalgar NDS)	(2025-XXX)
<b>8.XXX.1 Zone Provisions for Single Detached Dwelling Street Access</b>		
a)	Minimum Rear Yard Setback	6.0 m, except Lots 14, 15, 16 and 17 which shall have a rear yard setback of 4.5 m
b)	Minimum Front Yard Setback	2.0 m
c)	Minimum Flankage Yard Setback	1.2 m
d)	Notwithstanding Section 4.17.2.i of this By-law, no minimum amenity area shall apply	
e)	Notwithstanding Section 4.21.a) of this By-law, sills, belt courses, cornices, coves, pilasters, eaves, gutters, awnings, canopies or other non-structural architectural features shall be permitted to project into a required yard	1.0 m
f)	Notwithstanding Section 4.21 of this By-law, the maximum width of a Bay, Box Out and Bow Windows, with or without foundations which may be a maximum of three storeys in height and which may include a door	4.0 m
g)	Notwithstanding Section 4.21 of this By-law, a porch shall be permitted to have a minimum depth from the exterior of the building to the outside edge of the porch. Required depths shall be provided for a minimum 40% of the porch. However, steps and other obstructions may encroach a maximum of 0.3 metres into the required depth	1.5 m
h)	Notwithstanding Section 4.21.e) of this By-law, uncovered platforms 0.6 m or greater in height measured from	2.0 m

	surrounding grade and porches, exclusive of stairs, including a balcony on top of a porch including vertical supports and a roof above and landing shall be permitted to encroach into any required front and rear yard	
i)	Notwithstanding Section 4.21.d) of this By-law, a balcony shall be permitted within 1.2 m of any lot line	
j)	Notwithstanding Section 4.21 of this By-law, a covered porch (lanai) is permitted to project 3 metres into the required rear yard	
k)	Notwithstanding Section 5.4.1.2 of this By-law, a parking space in a private garage shall have an unobstructed area with a width of not less than 2.9 metres for a single car private garage or a tandem double car private garage, and one step may encroach into the width of a parking space within a garage at the side of the parking space	
l)	Minimum width of a landscape strip	1.0 m
m)	Minimum landscape area	5%
<b>8.XXX.2 Zone Provisions for Townhouse Dwelling with Street Access Private Garage [e.g., street-oriented towns]</b>		
a)	Minimum Lot Depth	22.0 m
b)	Minimum Flankage Yard Setback	1.5 m
c)	Notwithstanding Section 4.21.a) of this By-law, sills, belt courses, cornices, coves, pilasters, eaves, gutters, awnings, canopies or other non-structural architectural features shall be permitted to project into a required yard	1.0 m
d)	Notwithstanding Section 4.21.a) of this By-law, sills, belt courses, cornices, coves, pilasters, eaves, gutters, awnings, canopies or other non-structural architectural features shall be permitted to project into a required yard	1.0 m
e)	Notwithstanding Section 4.21 of this By-law, a porch shall be permitted to have a minimum depth from the exterior of the building to the outside edge of the porch. Required depths shall be provided for a minimum 40% of the porch. However, steps and other obstructions may encroach a maximum of 0.3 metres into the required depth	1.5 m
f)	Notwithstanding Section 5.4.1.2 of this By-law, a parking space in a private garage shall have an unobstructed area with a width of not less than 2.9 metres for a single car private garage or a tandem double car private garage, and one step may encroach into the width of a parking space within a garage at the side of the parking space	
g)	Notwithstanding Section 4.18.3 of this By-law, a maximum driveway width of 6.0 m is permitted	



h)	Minimum width of a landscape strip	1.0 m
i)	Notwithstanding Section 4.17.2.i of this By-law, no minimum amenity area shall apply	
<b>8.XXX.3 Zone Provisions for Townhouse Dwelling with Street Access Private Garage [e.g., dual front towns]</b>		
a)	The lot line abutting Harasym Trail shall be deemed the front lot line	
b)	Minimum Flankage Yard Setback	1.5 m
c)	Minimum Rear Yard Setback	5.5 m to garage face and 0.75 m to main wall
d)	Notwithstanding Section 4.21.a) of this By-law, sills, belt courses, cornices, coves, pilasters, eaves, gutters, awnings, canopies or other non-structural architectural features shall be permitted to project into a required yard	1.0 m
e)	Notwithstanding Section 4.21.e) of this By-law, uncovered platforms 0.6 m or greater in height measured from surrounding grade and porches, exclusive of stairs, including a balcony on top of a porch including vertical supports and a roof above and landing shall be permitted to encroach into any required front and rear yard	2.0 m
f)	Minimum width of a landscape strip	1.0 m
g)	Notwithstanding Section 4.17.2.i of this By-law, no minimum amenity area shall apply	
h)	Notwithstanding Section 4.21.d) of this By-law, a balcony shall be permitted within 1.2 m of any lot line	
i)	Notwithstanding Section 4.21 of this By-law, a porch shall be permitted to have a minimum depth from the exterior of the building to the outside edge of the porch. Required depths shall be provided for a minimum of 40% of the porch. However, steps and other obstructions may encroach a maximum of 0.3 metres into the required depth	1.5 m
j)	Notwithstanding Section 5.4.1.2 of this By-law, a parking space in a private garage shall have an unobstructed area with a width of not less than 2.9 metres for a single car private garage or a tandem double car private garage, and one step may encroach into the width of a parking space within a garage at the side of the parking space	
k)	Notwithstanding Section 4.18.3 of this By-law, a maximum driveway width of 6.0 m is permitted	
l)	Minimum landscaped area	5%
<b>8.XXX.4 Zone Provisions for Townhouse Dwelling Unit Back to Back</b>		
a)	Minimum Flankage Yard Setback	1.5 m

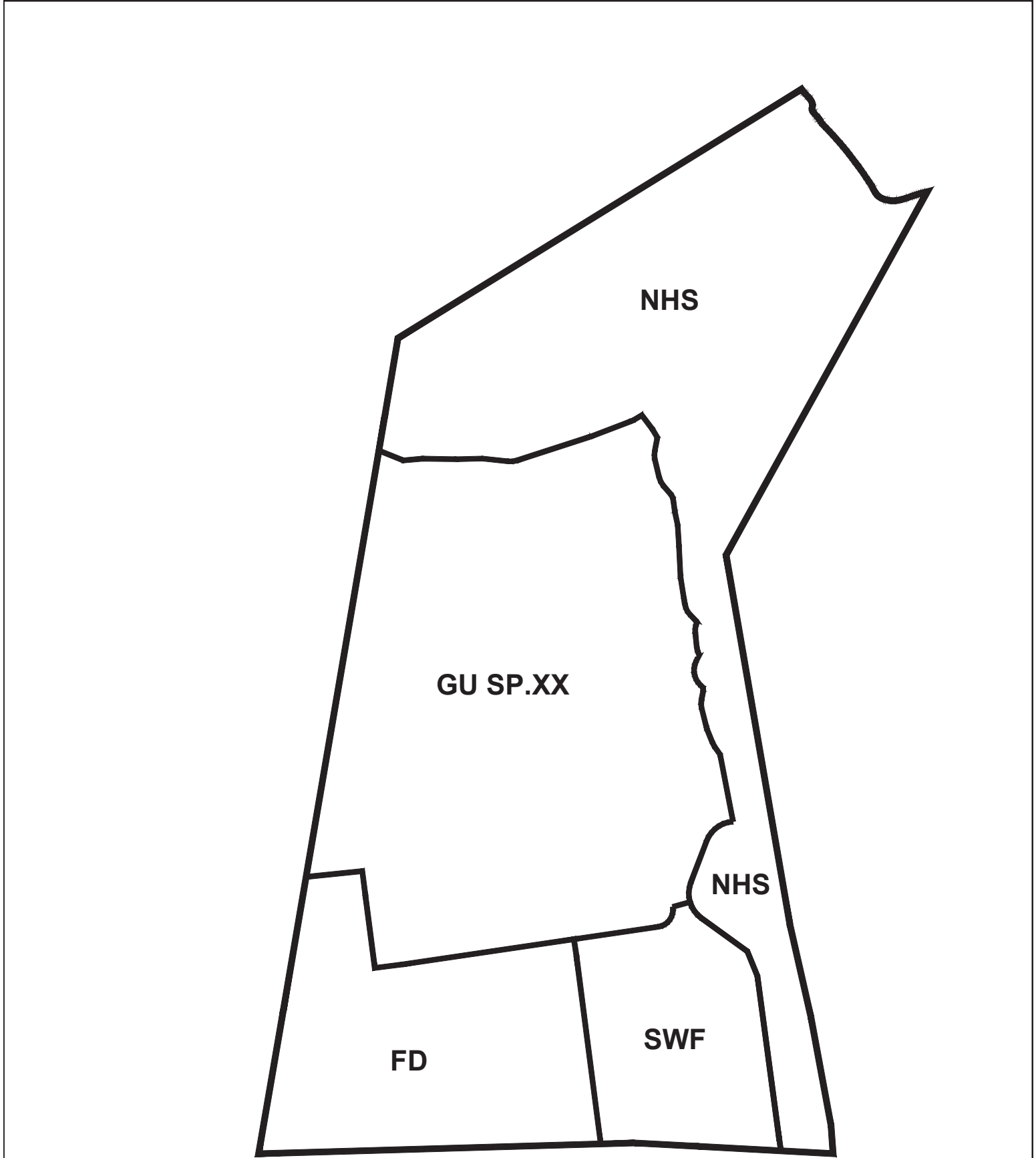
b)	Minimum Front Yard Setback	2.0 m
c)	Notwithstanding Section 4.21.a) of this By-law, sills, belt courses, cornices, coves, pilasters, eaves, gutters, awnings, canopies or other non-structural architectural features shall be permitted to projection into a required yard	1.0 m
d)	Notwithstanding Section 4.21.e) of this By-law, uncovered platforms 0.6 m or greater in height measured from surrounding grade and porches, exclusive of stairs, including a balcony on top of a porch including vertical supports and a roof above and landing shall be permitted to encroach into any required front yard	2.0 m
e)	Notwithstanding Section 4.21 of this By-law, a porch shall be permitted to have a minimum depth from the exterior of the building to the outside edge of the porch. Required depths shall be provided for a minimum of 40% of the porch. However, steps and other obstructions may encroach a maximum of 0.3 metres into the required depth	1.5 m
f)	Notwithstanding Section 5.4.1.2 of this By-law, a parking space in a private garage shall have an unobstructed area with a width of not less than 2.9 metres for a single car private garage or a tandem double car private garage, and one step may encroach into the width of a parking space within a garage at the side of the parking space	
g)	Minimum landscaped area	0.75 m by 0.75 m
<b>8.XXX.5 Zone Provisions for Rear Lane Dwellings</b>		
a)	The regulations of NC sp. 83 shall apply to any townhouse dwelling with rear lane access	

3. This By-law comes into force in accordance with Section 34 of the Planning Act, R.S.O. 1990, c.P. 13, as amended,

PASSED this XXth day of \_\_\_\_\_, 2025.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk



DUNDAS STREET W



SUBJECT LANDS TO BE REZONED FROM FD (FUTURE DEVELOPMENT) TO NHS (NATURAL HERITAGE SYSTEM), SWF (STORM WATER MANAGEMENT FACILITY) & GU SP.XX (GENERAL URBAN)

**LOCATION:**  
PART OF LOT 23, CONCESSION 1,  
NORTH OF DUNDAS STREET  
(GEOGRAPHIC TOWNSHIP OF TRAFALGAR)  
TOWN OF OAKVILLE  
REGIONAL MUNICIPALITY OF HALTON

**THIS IS SCHEDULE 'A'**  
**TO BY-LAW AMENDMENT \_\_\_\_\_**  
PASSED THE \_\_\_\_ DAY OF \_\_\_\_\_, 2024.

**SIGNING OFFICERS**

\_\_\_\_\_  
MAYOR

\_\_\_\_\_  
CLERK



Scale 1:2,500

North Oakville Secondary Plan

**7.2 COMMUNITY VISION**

**7.2.1 PURPOSE**

The community vision and objectives on which the Secondary Plan is based were developed when the North Oakville East lands were designated as "Urban Special Study Area". They have been modified to reflect the results of the Secondary Plan process and are outlined in the following sections.

The community vision and objectives represent expressions of general intent and are not to be interpreted as direct statements of planning policy, rather they form a basis for the policies contained in the Secondary Plan. In particular, it is not proposed that they be applied directly in the evaluation of any development or redevelopment proposal.

**7.2.2 VISION**

North Oakville's development as an urban community shall reflect Oakville's distinct historical roots and small-town heritage and Trafalgar Township's village rural heritage, with nodal development, prestige industry, and green linkages continuing to define Oakville's unique landscape.

North Oakville should also be forward-looking. It should be a model of smart growth and social diversity. It should enhance the Town's reputation for excellence and its capacity to link the past, present and future.

The design of North Oakville East will generally reflect the "Transect"<sup>1</sup>, a system of classification of human habitats from the most rural which is reflected in the natural heritage and open space system, to the most urban conditions, which is reflected in urban core areas. The goal of the transect-based system is to make it possible for North Oakville East to sustain a complete palette of neighbourhoods and employment opportunities.

In keeping with the Transect, North Oakville East is planned as a compact, pedestrian-oriented, urban community containing a broad range of housing opportunities ranging from executive housing on large lots to high rise apartment units.

The character and pattern of the community will be significantly influenced by a planned natural heritage and open space system. This natural heritage and open space system is designed to protect the natural environment, provide a balance between active and passive recreation needs and contribute to the quality of life in North Oakville and the Town as a whole. A key component of the system will be the provision of an opportunity for residents and employees to use an extensive open space trail system.

The natural heritage and open space system helps to create definable neighbourhoods. These neighbourhoods will be known for their walkable streets and their central nodes, which will include civic uses such as a transit stop and mail services, and may include a few small shops and services. These nodes will be popular neighbourhood meeting

---

<sup>1</sup> The Transect was developed by the firm of Duany Plater-Zyberk & Company

places and will generally be located within a five minute walk of the entire neighbourhood.

The community will be well served by an interconnected transit network which will provide residents and employees opportunities for an attractive alternative travel mode within North Oakville, and connections to the rest of the Town, as well as transit facilities which serve the Region and the Greater Golden Horseshoe.

A business park located along Highway 407 provides a range of employment opportunities to residents of Oakville including prestige employment and office development at Trafalgar Road. The jobs available in the North Oakville East Secondary Plan Area, in combination with those jobs in the North Oakville West Secondary Plan Area, help to create a live-work community.

### **7.2.3 GENERAL DEVELOPMENT OBJECTIVES**

The following general development objectives will guide the future urban development of the Planning Area.

#### **7.2.3.1 Environment and Open Space**

- a) To establish as a first priority of the Town, a natural heritage and open space system, within the context of an urban setting, the majority of which is in public ownership.
- b) To create a sustainable natural heritage and open space system which provides a balance between active and passive recreational needs and links to the existing open space system within the Town.
- c) To identify, protect and preserve natural heritage features within the natural heritage component of the natural heritage and open space system and ensure that their use respects their functional role as natural areas within the ecosystem.
- d) To incorporate measures intended to achieve the goals of environmental protection and enhancement including energy conservation, greenhouse gas reduction, and increased utilization of public transit.
- e) To preserve and protect ESA's, ANSI's, provincially significant wetlands and significant woodlands which form the core of the natural heritage component of the natural heritage and open space system, together with required buffers and adjacent lands intended to protect the function of those features and ensure the long term sustainability of the natural heritage component of the system within the urban context.

#### **7.2.3.2 Residential**

- a) To create residential communities which complement the existing built form elements that are intended to remain within the community, and incorporate the best community planning and urban design practices available while protecting, enhancing and integrating the area's natural heritage component of the natural heritage and open space system.

- b) To establish overall development densities that equal or exceed the density established by the Halton Urban Structure Plan and which are commensurate with the type and frequency of transit service planned for the area.
- c) To reflect the land use objectives as set out within the Halton Urban Structure Plan (April 1994).
- d) To minimize travel time, traffic, greenhouse gases, servicing costs and energy costs through a variety of mechanisms, and particularly by providing an efficient land use arrangement and a mix of housing forms and tenures.
- e) To encourage a closer relationship between the workplace and home through land use planning decisions.
- f) To create varied and distinguishable residential neighbourhoods which provide a strong, identifiable sense of place for the residents.
- g) To provide for a variety of residential densities and unit types throughout the planning area, responding to the varied needs of the future population, while directing the highest densities and intensity of use to the Trafalgar Road Corridor in support of a broad range of services including high frequency transit, shopping, personal services and community facilities.

### **7.3 COMMUNITY STRUCTURE**

#### **7.3.1 PURPOSE**

Figure NOE1, the Community Structure Plan, establishes a range of components which define the general arrangement of land use and activity for the Planning Area.

Each component of the Community Structure Plan has its own function which is described in the following sections, together with the ultimate population, housing and employment targets.



### 7.3.3 RESIDENTIAL NEIGHBOURHOODS

Residential neighbourhoods as designated on Figure NOE1 (Community Structure Plan) are comprised of a range of residential densities including significant areas appropriate for ground related housing and live/work opportunities:

- a) Neighbourhood Centre  
Neighbourhood Centres are located in the centre of each neighbourhood, within walking distance of most residents. While predominately residential in character, Neighbourhood Centres will permit a range of uses. These uses will be permitted throughout the area but will be focused at a central activity node for the neighbourhood. Neighbourhood Centres have denser development than other parts of the neighbourhood but are predominantly ground related, and, in addition to residential development, will include a range of convenience and service commercial, civic, institutional and live-work functions in buildings at a scale and with a design appropriate to the area.
- b) General Urban  
General Urban areas, while predominately residential, also provide for live-work functions. Development will be at lower densities than those found in the Neighbourhood Centre.
- c) Sub-urban  
The least dense and most purely residential context is found in areas in a neighbourhood termed "Sub-urban". While live-work functions are permitted, these areas are primarily residential in nature.
- d) High Density Residential  
Neighbourhood 14 on the west side of Sixteen Mile Creek shall include high density residential uses, which may be located adjacent to:
  - i) the valley of the Sixteen Mile Creek;
  - ii) other park or open space areas; or
  - iii) any road or roads within the Neighbourhood.

While the neighbourhood shall be predominantly residential in character, including live-work buildings, a range of non-residential uses shall also be permitted throughout the neighbourhood. These non-residential uses may include:

  - i) a range of convenience retail and service commercial uses,
  - ii) civic and institutional uses, and
  - iii) office uses

in buildings at a scale and with a design appropriate to the area. Both mixed use and single use buildings shall be permitted.

### 7.3.5 NATURAL HERITAGE AND OPEN SPACE SYSTEM

The Natural Heritage and Open Space System for North Oakville East is part of a larger system which is intended to extend through all of North Oakville. It forms a central feature of the North Oakville East Planning Area. It is comprised of two components, a Natural Heritage component and an Open Space component.

- a) The Natural Heritage component of the System is comprised of the following key areas as identified by the policies in the Plan:
  - i) Core Preserve Areas  
The Core Preserve Areas include key natural features or groupings of key natural features, together with required buffers and adjacent lands intended to protect the function of those features and ensure the long term sustainability of the Natural Heritage component of the System within the urban context.
  - ii) Linkage and Optional Linkage Preserve Areas  
Linkage and Optional Linkage Preserve Areas include areas which are designed to link the Core Preserve Areas together to maintain and enhance their environmental sustainability. They follow natural features whenever possible and are intended to be of sufficient size and character, including buffers, to ensure the functionality and sustainability of the Natural Heritage component of the System.
  - iii) High Constraint Stream Corridor Areas  
High Constraint Stream Corridor Areas include certain watercourses and adjacent riparian lands, including buffers measured from the stable top-of-bank or meander belts. These areas are located primarily inside Core and Linkage Preserve Areas, but are also found outside such areas. They must be protected in their existing locations for hydrological and ecological reasons.
  - iv) Medium Constraint Stream Corridor Areas  
Medium Constraint Stream Corridor Areas include certain watercourses and adjacent riparian lands, including buffers measured from the stable top-of-bank or meander belts. These areas are located primarily inside Core and Linkage Preserve Areas, but are also found outside such areas. They must be protected for hydrological and ecological reasons. These watercourses may be deepened and/or relocated in accordance with subsection 7.4.7.1 d).
  - v) Other Hydrological Features  
In addition, to the High and Medium Constraint Stream Corridor Areas, a number of other hydrological features have been identified in North Oakville East. These features are also part of the Natural Heritage component of the Natural Heritage and Open Space System to the extent they are maintained after development occurs.
- b) The Open Space component of the Natural Heritage and Open Space System includes open space areas such as stormwater facilities, cemeteries, public parks and schools. The Open Space component of the System will be designed, where possible, to connect to, and enhance the Natural Heritage component of the System.



## **7.4 SUSTAINABLE DEVELOPMENT STRATEGY**

### **7.4.1 PURPOSE**

In keeping with the objectives of this Secondary Plan, the Town of Oakville is committed to the principle of sustainable development<sup>2</sup> in the development of North Oakville East.

The Sustainable Development Strategy provides policies with respect to the implementation of this principle as it relates to development form, and specifically to the protection, conservation and enhancement of air, water and ecological features and functions, energy and other resources, and heritage resources.

### **7.4.2 DEVELOPMENT FORM**

The North Oakville East Secondary Plan has been based on a conceptual design which maximizes the potential for sustainable development through such features as mixed use development, a modified grid road system which enhances the opportunity to provide transit, and a Natural Heritage and Open Space System.

In addition to the general direction implicit in the Plan, the Town will actively encourage development which is specifically based on the principle of sustainable development, including the development of Town facilities. The Town will also work with other public agencies to encourage them to follow these principles. Such development will be designed to:

- a) reduce the consumption of energy, land and other non-renewable resources;
- b) minimize the waste of materials, water and other limited resources;
- c) create livable, healthy and productive environments; and,
- d) reduce greenhouse gases.

### **7.4.3 AIR/ENERGY EFFICIENCY**

The Town recognizes that air quality in North Oakville East will be significantly influenced by air pollutant emissions from outside the area. However, the Town will work to improve air quality and energy efficiency in North Oakville East:

- a) by directing through the Secondary Plan policies, and their implementation, through the evaluation of development applications, and through the initiation of municipal programs and works, consideration of the following land use and transportation strategies to minimize the amount of vehicular travel and emissions:

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<sup>2</sup> The Bruntland Commission (1983) defined sustainable development as “development that meets the needs of the present without compromising the ability of future generations to meet their own needs.”

- i) concentrate activity centres such as places of worship, recreation centres and schools;
  - ii) encourage mixed-use development;
  - iii) encourage increased density near transit stops/stations and along transit routes commensurate with the type and frequency of transit service planned for that area and/or corridor;
  - iv) provide pedestrian and bicycle facilities;
  - v) ensure an interconnected street network;
  - vi) develop a strategy for the provision of public parking facilities at key locations; and,
  - vii) provide convenient and efficient transit service.
- b) by directing where the Town has jurisdiction, including through its engineering standards, and encouraging where it does not, measures to reduce the "urban heat island effect" including installation of reflective (or high albedo) roofs, installation of green roofs, increasing the reflective nature of pavement and planting of shade trees; and,
- c) by encouraging at the development application stage, the reduction of energy and residential combustion emissions through a range of approaches including the development of R-2000 homes and similar commercial construction standards such as LEED or other published standards, the incorporation of Energy Star appliances, the physical layout of the plan having regard to energy conservation, and buildings powered by renewable energy sources, both passive and active.

#### **7.4.4 APPLICATION REVIEW**

- a) Development which incorporates initiatives to reflect the principle of sustainable development, including three or more matters set out in Section 7.4.4 b)i) and b)ii) or other initiatives, may proceed as soon as servicing can be made available, regardless of the development phase in which it is located, subject to approval by the Town and the Region of Halton.
- b) Such development must comply with the other policies of this Plan and shall also:
- i) implement efficient and effective methods of providing energy through:
    - 1) the incorporation of alternative energy systems including the use of renewable energy resources and/or district energy facilities; and/or,
    - 2) minimizing energy needs and flat-lining the energy profile of the design of the neighbourhood or development.

- ii) increase energy efficiency and minimize environmental impacts in building design through approaches including or similar to some or all of the following:
  - 1) utilizing the standard for residential construction at a minimum of R-2000 and for commercial buildings, similar commercial construction standards such as LEED or other published standards;
  - 2) incorporating opportunities for efficiencies through orientation and the use of passive solar energy and landscaping options;
  - 3) incorporating the potential for future alternative uses and life stages in building design;
  - 4) maximizing opportunities for waste reduction, reuse and recycling in the construction process, building design and community design;
  - 5) maximizing opportunities for stormwater and grey water reuse including the use of permeable materials for the surfaces of parking lots and internal driveways/roads;
  - 6) maximizing opportunities for application of stormwater management at the site level;
  - 7) incorporating water conservation measures in all buildings and landscaping; and,
  - 8) incorporating measures to reduce the "urban heat island effect" including installation of reflective (or high albedo) roofs, installation of green roofs, increasing the reflective nature of pavement and planting of shade trees.

#### **7.4.5 WATER MANAGEMENT**

The management of water resources within the North Oakville East Planning Area shall be undertaken in accordance with the directions established in the North Oakville Creeks Subwatershed Study. No amendments to the Secondary Plan shall be required to implement the recommendations of the Subwatershed Study or for changes to the number or location of stormwater management facilities in accordance with the policies of Section 7.6.2.2 a) of this Plan.

It is an objective of the Town that there be no net increase in phosphorus loadings as a result of development. It is recognized that this objective is achieved by requiring stormwater management ponds in North Oakville East to meet the MOE's Enhanced (Level 1) Guidelines. Provided the MOE's Enhanced (Level 1) Guidelines are met, there is no requirement to further analyze the total phosphorus during development approval.

Further, prior to draft approval of plans of subdivision, Environmental Implementation Reports will be required for each subcatchment area (See Appendix 7.2) in which they are located, in accordance with the policies in Section 7.8.3 a). The Environmental Implementation Reports, in concert with the North Oakville Creeks Subwatershed Study, fulfill the EIS requirements of the Region of Halton Official Plan.

#### **7.4.6 NATURAL HERITAGE AND OPEN SPACE SYSTEM**

##### **7.4.6.1 Context**

In accordance with objectives of this Secondary Plan, a Natural Heritage and Open Space System is to be established for North Oakville East. The Natural Heritage and Open Space System for North Oakville East is part of a larger system which is intended to extend through all of North Oakville. It forms a central feature of the North Oakville East Planning Area. It is comprised of two components, a Natural Heritage component, and an Open Space component.

##### **7.4.6.2 Purpose**

The purpose of the Natural Heritage and Open Space System is the establishment of a system, the majority of which is to be in public ownership, and the focal point of which is a linked natural heritage system enhanced by a range of open space facilities. The System is designed to protect the natural environment, provide a balance between active and passive recreational needs and contribute to overall quality of life in North Oakville and the Town as a whole.

The primary purpose of the Natural Heritage component of the Natural Heritage and Open Space System is to protect, preserve and, where appropriate, enhance the natural environment. The focus of the Natural Heritage component is on the protection of the key ecological features and functions of North Oakville. It will also contribute to the enhancement of air and water resources, and provide for limited, passive recreational needs.

The primary purpose of the Open Space component of the System is to provide for active recreational needs and community facilities. It also should be designed, where possible, to connect to, and enhance the Natural Heritage component of the Natural Heritage and Open Space System, as well as providing for passive recreational needs. Finally, the Open Space component of the System provides facilities which will assist in building social relationships within the North Oakville community.

##### **7.4.6.3 Subwatershed Study**

The policies of this Secondary Plan provide the framework for the Natural Heritage component of the System, however, the North Oakville Creeks Subwatershed Study provides the basis for its establishment and technical guidance for its implementation

#### **7.4.7 NATURAL HERITAGE COMPONENT OF THE NATURAL HERITAGE AND OPEN SPACE SYSTEM**

##### **7.4.7.1 Natural Heritage Designations**

The Natural Heritage component of the Natural Heritage and Open Space System, reflecting an alternative Greenlands System as intended by the Regional Plan, is comprised of lands designated "Natural Heritage System Area" on Figures NOE1, NOE2 and NOE4 and "Core Preserve Area," "Linkage Preserve Area," "Optional Linkage Preserve Area," "High Constraint Stream Corridor Area" and "Medium Constraint Stream Corridor Area" on Figure NOE3. It also includes watercourses and features designated as "Other Hydrological Features" on Figure NOE3, to the extent that they are maintained after development occurs, in accordance with the policies in Section 7.4.8.

The Natural Heritage System Area designation is comprised of the following key areas:

a) Core Preserve Areas

- i) The Core Preserve Area designation on Figure NOE3 includes key natural features or groupings of key natural features, together with required buffers and adjacent lands intended to protect the function of those features and ensure the long term sustainability of the Natural Heritage component of the System within the urban context.
- ii) The Core Preserve Areas were designated based on an evaluation which considered the following criteria:
  - Diversity – Areas with diverse habitats and/or supporting a rich assemblage of species;
  - Size – Sufficient size to protect interior habitat;
  - Contiguity – Designed to create contiguous units;
  - Connectivity – The unit can be linked to other units;
  - Significance – Areas supporting significant species or habitats;
  - Representativeness – Areas which include appropriate representational features associated with areas of natural and scientific interest (ANSI) designations or a candidate ANSI designation, including the Trafalgar Moraine candidate ANSI; and,
  - Overall watershed functionality including hydrologic processes which protect the flow regime of receiving streams.

b) Linkage Preserve Areas and Optional Linkage Preserve Areas

The Linkage Preserve Area and Optional Linkage Preserve Area designations on Figure NOE3 include areas which are designed to link the Core Preserve Areas together to maintain and enhance their environmental sustainability. They follow natural features whenever possible and are intended to be of sufficient size and character to ensure the functionality and sustainability of the Natural Heritage component of the System.



- i) The length, width and general location of the Linkage Preserve Areas and Optional Linkage Preserve Areas have been defined based on factors established through the North Oakville Creeks Subwatershed Study including:

- Composition of potential linkage feature;
- Character of the surrounding habitats;
- Presence and size of discontinuities; and,
- Required buffers.

In particular, the configuration of the Linkage Preserve Area which connects to Sixteen Mile Creek adjacent to the future 407 Transitway is based on a width of 70 metres west of the existing woodlot and 100 metres east of the woodlot. The woodlot is delineated as drip line plus 10 metres or wetland plus 30 metres. If the Transitway is reduced in width west of the woodlot there will be no reduction or shifting of the Linkage Preserve Area, and any surplus transitway lands will be added to the north side of the Linkage Preserve Area. East of the woodlot, the northern boundary of the Linkage will remain coincident with the southern boundary of the Transitway so that if at any time prior to an application for development being approved, the southern boundary of the Transitway adjacent to the Linkage moves northward, the Linkage Preserve Area will also move northward. However, all boundaries of this Linkage will remain subject to a 30 metre setback from any Provincially Significant Wetland. The area of any setback for the Provincially Significant Wetland located west of the woodlot, that extends beyond the southern boundary of the Linkage (the extended area), will be offset by a corresponding reduction in the width of the Linkage located west of the woodlot, that is equal in area to the extended area.

- ii) The Optional Linkage Preserve Areas have been established based on the potential to relocate adjacent Medium Constraint Streams into the area designated as "Optional Linkage Preserve Area". However, if the adjacent stream is not relocated into the lands designated "Optional Linkage Preserve Area", and remains in place in a manner which satisfies the requirements to serve a linkage function as set out in Subsection 7.4.7.1 d), then the lands in the Optional Linkage Preserve Area designation, without the need for amendment to this Plan, may be developed in accordance with the abutting land use designation. Otherwise the lands in the Optional Linkage Preserve Area designation shall be subject to the policies of the Linkage Preserve Area designation.

c) High Constraint Stream Corridor Areas

High Constraint Stream Corridor Areas as designated on Figure NOE3 include certain watercourses with associated riparian lands, including buffers measured from stable top-of-bank and meander belts. These areas are located primarily inside Core and Linkage Preserve Areas, but are also found outside such areas.

They must be protected in their existing locations for hydrological and ecological reasons in accordance with the directions established in the North Oakville Creeks Subwatershed Study.

d) Medium Constraint Stream Corridor Areas

Medium Constraint Stream Corridor Areas, as designated on Figure NOE3, include certain watercourses and adjacent riparian lands, including buffers measured from the stable top-of-bank or meander belts. These areas are located primarily inside Core and Linkage Preserve Areas, but are also found outside such areas. They must be protected for hydrological and ecological reasons. These watercourses may be deepened and/or relocated and consolidated with other watercourses provided that the watercourse feature, as well as the function of the watercourse, is maintained in accordance with the directions established in the North Oakville Creeks Subwatershed Study and Federal, Provincial and Conservation Authority regulations, and natural channel design is used. Where a Medium Constraint Stream Corridor Area is relocated, the land use designation of the abutting lands on Figure NOE2, not the Natural Heritage System Area designation, shall apply to the lands from which the stream is moved. Further, where a Medium Constraint Stream adjacent to an "Optional Linkage Preserve Area" is not being relocated into the "Optional Linkage Preserve Area", then that Medium Constraint Stream shall serve a linkage function similar to a Linkage Preserve Area, in addition to its role as a Medium Constraint Stream. This would include maintaining a minimum linkage width as established in the North Oakville Creeks Subwatershed Study along the Medium Constraint Stream. No modifications may be made to the location of such a stream unless that linkage function can be maintained.

#### **7.4.7.2 Boundaries**

The boundaries of the Core and Linkage Preserve Area, and High Constraint Stream Corridor Area designations shall be maintained generally in accordance with the designations on Figure NOE3 and the North Oakville Creeks Subwatershed Study.

As part of the detailed planning process, minor modifications to the boundaries of these designations may be considered to reflect differences in scale and levels of detail, or to better integrate natural and urban land uses to achieve a more compact, efficient urban form. However, such minor modifications will not negatively impact the Natural Heritage component of the Natural Heritage and Open Space System as determined by the Town, in consultation with the Region of Halton and Conservation Halton, nor shall such minor modifications result in any significant decrease in the size of the Natural Heritage component of the System.

The location and boundaries of the Medium Constraint Stream Corridor Area designations on Figure NOE3 shall be determined in accordance with the directions established in the North Oakville Creeks Subwatershed Study and Federal, Provincial and Conservation Authority regulations during the preparation of the Environmental Implementation Reports (EIR) required by Section 7.8.3 of this Plan. In addition, Reach

MUN-3, as identified on Figure 6.3.13 of the North Oakville Creeks Subwatershed Study, may be reexamined as part of the relevant EIR to confirm its classification. If all or part of MUN-3 is recommended for reclassification from a Medium Constraint Stream Corridor to a Low Constraint Stream Corridor and the basis for this reclassification is accepted by the Town and Conservation Halton, the change will not require an amendment to the Secondary Plan.

Stream Corridor Components: Appendix 7.4 illustrates the required components of Stream Corridors and provides direction on how they are measured.

#### **7.4.7.3 Permitted Uses, Buildings and Structures**

- a) The only permitted uses in the Natural Heritage System Area designation shall be legally existing uses, buildings and structures, and fish, wildlife and conservation management. Development or land disturbances shall generally be prohibited.
  - b) The only exceptions to the provisions of Section 7.4.7.3 a) shall be the uses in Subsection c), subject to the satisfaction of the Town, in consultation with the Region of Halton and Conservation Halton, provided that, prior to approving the location/construction of such uses a study shall be undertaken, except where an Environmental Assessment is required:
    - i) identifying potential negative impacts on the functions and features of the applicable designation during the construction and post-construction phases; and,
    - ii) demonstrating that alternative methods and measures for minimizing impacts have been considered and appropriate methods and measures will be applied.
  - c) The potential permitted uses include:
    - i) Development or land disturbance in accordance with the directions of the North Oakville Creeks Subwatershed Study and any related Environmental Implementation Report, and Federal, Provincial and Conservation Authority regulations:
      - for required flood and stream bank erosion control;
      - for fish, wildlife and conservation management;
      - to accommodate a stormwater outfall; or
      - in Medium Constraint Stream Corridor Areas, to relocate or deepen channels in accordance with Section 7.4.7.1 d);
- provided that any required reconstruction of a watercourse is completed in a way that utilizes bio-engineering principles and practices, and

maintains, and where possible, improves the form, characteristics and functions of the watercourse.

ii) Roads and related utilities which shall:

- use non-standard cross sections designed to minimize any impacts on the natural environment;
- only be permitted to cross the designation in the general area of the road designations shown on Figures NOE2 and NOE4 or as defined through an Environmental Assessment; and,
- be designed to minimize grading in accordance with the directions established in the North Oakville Creeks Subwatershed Study.

Provided that such corridors shall:

- be required as transit routes or utility corridors;
- be located outside natural features to the maximum extent possible, and where the applicable designation is narrowest and along the edges of applicable designations, wherever possible;
- provide for the safe movement of species in accordance with the directions established in the North Oakville Creeks Subwatershed Study in the design and construction of any road or utility;
- be kept to the minimum width possible; and,
- be designed to keep any related structures or parts of structures outside the High Constraint Stream Corridor Area designated on Figure NOE3 to the maximum extent possible or as defined through an Environmental Assessment.

iii) Expansion to existing Water and Wastewater services which are located on sites with existing facilities subject to any required Environmental Assessment;

iv) Trails, interpretative displays or signage or other similar passive recreation uses consistent with the purpose of the applicable designation and provided that:

- for lands in the Linkage Preserve Area designation on Figure NOE3, such uses shall generally be located in the Linkage Preserve Area, but adjacent to the boundary of the linkage;
- trails shall be permitted within the setback from the edge of the

Sixteen Mile Creek Valley, and may be permitted within the Valley subject to the review of their impact on any environmentally sensitive features;

- trails in stream corridors other than the Sixteen Mile Creek shall be permitted adjacent to the valley in the buffer; and,
- trails in the Natural Heritage System Area designation be designed and located to minimize any impact on the natural environment.

v) Stormwater management facilities established in accordance with the directions in the North Oakville Creeks Subwatershed Study provided that the final number, size and configuration of such facilities will be determined through any related Environmental Implementation Report or Functional Servicing Study and provided that generally such facilities shall, with respect to the designations on Figure NOE3:

- be limited in Core Preserve Areas to only four stormwater management facilities located as shown conceptually on Figure NOE3 ;
- be limited where located in or adjacent to High and Medium Constraint Stream Corridor Areas which are not located within Linkage Preserve or Optional Linkage Preserve Areas as designated conceptually on Figure NOE3, to areas:
  - outside the 100 year floodline;
  - outside the meanderbelt allowance which is the meanderbelt plus the factor of safety;
  - outside the erosion/access allowance measured from the meander belt or stable top-of-bank, except that some overlap of the access required for the stormwater management facility and the erosion/access allowance may be permitted in accordance with the directions established in the North Oakville Creeks Subwatershed Study, and to the satisfaction of the Town and Conservation Halton;
  - outside the confined valley; and,

provided that there is no loss of flood storage or conveyance;

- be limited where located in or adjacent to High and Medium Constraint Stream Corridors which are located within Linkage Preserve or Optional Linkage Preserve Areas as designated conceptually on Figure NOE3, to the stormwater management facilities as shown conceptually on Figure NOE3, to areas
  - outside the 100 year floodline;
  - outside the meanderbelt allowance which is the meanderbelt plus the factor of safety;



- outside the erosion/access allowance measured from the meander belt or stable top-of-bank, except that some overlap of the access required for the stormwater management facility and the erosion access allowance may be permitted in accordance with the directions established in the North Oakville Creeks Subwatershed Study, and to the satisfaction of the Town and Conservation Halton;
- outside the confined valley; and,

there is no loss of flood storage or conveyance; and,

provided that the encroachment into such Linkage Preserve or Optional Linkage Preserve Areas shall be limited to a width which is no greater than 50% of the width of the Linkage Preserve Area or Optional Linkage Preserve Area; and further,

provided that storm water management facilities shown conceptually on Figure NOE 3 which are located in the Linkage Preserve Area or Optional Linkage Preserve Area associated with Reach MOC-W1, MOC-W2 and MOC-W3 as identified of Figure 6.3.13 of the North Oakville Creeks Subwatershed Study shall only be permitted when provisions have been made which are satisfactory to the Town and Conservation Halton for the establishment of wooded nodes within the Linkage Preserve Area or Optional Linkage Preserve Area.

In addition, the stormwater management facilities shall be designed to:

- be naturalized and unfenced, except where the facility abuts private property it may be fenced;
  - be as small as necessary; and,
  - have minimum access for maintenance.
- vi) Grading in the Natural Heritage component of the Natural Heritage and Open Space System for facilities outside of, but adjacent to, the Natural Heritage component of the System, such as lots, roads and public facilities, shall be permitted in accordance with the directions established in the North Oakville Creeks Subwatershed Study or appropriate Environmental Assessment.
- vii) A 6 metre wide private driveway across the Linkage Preserve Area joining the north area and south area of the Core Preserve Area which is north of Burnhamthorpe Road and west of Trafalgar Road, in a location and with a design determined by the relevant Environmental Implementation Report, and site plan approval, and to the satisfaction of the Town, the Region and Conservation Halton.
- viii) The adaptive re-use of heritage buildings for institutional uses, including an art gallery and art school, in the Linkage Preserve Area associated with Reach JC-7, as identified on Figure 6.3.13 of the North Oakville Creeks Subwatershed Study is permitted. The extension of such buildings or the construction of new buildings and structures may also be permitted, subject to Conservation Halton's regulations and the preparation of an Environmental Implementation Report to the satisfaction of the Town and Conservation Halton, which addresses how the impact to the linkage will be minimized.

#### 7.4.10 NATURAL HERITAGE AND OPEN SPACE SYSTEM SECUREMENT

- a) The following designations on Figure NOE3 comprise the Natural Heritage component of the Natural Heritage and Open Space System. These designations do not imply that the lands will be purchased by the Town or a public agency or that they are free or open to the public:
  - i) Core Preserve Area;
  - ii) Linkage Preserve Area and Optional Linkage Preserve Area;
  - iii) High Constraint Stream Corridor Area; and,
  - iv) Medium Constraint Stream Corridor.

This policy is also applicable to the Other Hydrological Features designated on Figure NOE3 which are also part of the Natural Heritage component of the System.

The public portions of the Open Space component of the Natural Heritage and

Open Space System will generally be acquired in accordance with the provisions of the Planning Act and Section 7.7.4.5 of this Plan with respect to parkland acquisition.

- b) Notwithstanding the foregoing, the Town recognizes that public securement of the lands in the Natural Heritage component of the Natural Heritage and Open Space System will provide opportunities for enhanced management of the lands in the System. The Town will investigate all options for the securement of land in the Natural Heritage component of the Natural Heritage and Open Space System.
- c) Lands in the Natural Heritage component of the Natural Heritage and Open Space System, while recognized as part of the parkland hierarchy, shall not be acceptable as parkland dedication under the Planning Act unless:
  - i) the lands can be used without impact on environmental quality or function for some passive open space/recreational use such as a trail use or interpretative display in conjunction with adjacent active parkland which is located outside the Natural Heritage component of the Natural Heritage and Open Space System; and,
  - ii) the Town is satisfied that sufficient land has been dedicated, or cash-in-lieu has been provided, to satisfy the Town's requirements for active parkland for the development.

## **7.5 COMMUNITY DESIGN STRATEGY**

### **7.5.1 PURPOSE**

This section outlines general design policies for North Oakville East, as well as specific policies for the Neighbourhoods as designated on Schedule NOE1, and the Trafalgar Urban Core Area, Neyagawa Urban Core Area, the Dundas St. Urban Core Area, Employment Districts, and existing development.

### **7.5.2 MASTER PLAN**

- a) The North Oakville East Master Plan in Appendix 7.3 to the Official Plan is intended to illustrate graphically the design of the North Oakville East Planning Area and how the policies and Figures of the North Oakville East Secondary Plan are to be implemented. The spacing, function and design of intersections of Local Roads with Major Arterial/Transit Corridors (i.e. Regional arterials) shown on Appendix 7.3 have not been approved by the Region, and such intersections shown on Appendix 7.3 and on any subsequent area design plan, plan of subdivision, or other development plan, are subject to Regional approval.
- b) Prior to the commencement of the development of any:
  - i) neighbourhood in accordance with the neighbourhood boundaries established on Figure NOE1 and, where applicable, the portion of the Dundas Urban Core that abuts the neighbourhood;
  - ii) sub-area within the Trafalgar Urban Core Area identified on Figure NOE1, except that lands in any sub-area on one side of Trafalgar Road may proceed independently of the lands in that sub-area on the other side of Trafalgar Road;
  - iii) part of the Neyagawa Urban Core Area; or
  - iv) sub-area within the Employment Area or the Transitional Area identified by the Town, in consultation with all affected landowners, based on boundaries created by Arterial, Avenue or Connector roads or natural features, provided that a sub-area may include adjacent Employment and Transitional Areas.

The Town shall determine, after consultation with all affected landowners in the specific area, whether proposed plans of subdivision or other development plans for the affected lands are generally consistent with the Master Plan in Appendix 7.3. Where such plans are determined to be generally consistent with the Master Plan, development may be permitted to proceed without the preparation of an area design plan.

- c) The Town shall require the preparation of an area design plan to the satisfaction of the Town, prior to draft plan approval or approval of other development plans in a specific area identified in subsection b), where:

- i) proposed plans of subdivision or other development plans for the area are not generally consistent with the Master Plan in Appendix 7.3;
  - ii) the Town after consultation with all affected landowners, determines that an area design plan is required to address coordination issues between landowner plans;
  - iii) the Town, after consultation with all affected landowners, determines that an area design plan is required to address coordination issues between areas identified in 7.5.2 b) i) to iv); or
  - iv) any significant development is proposed in the Transitional Area designation.
- d) The area to be addressed by the area design plan will include, as applicable, one or more of the areas identified in subsection b).
- e) The area design plan will be designed to demonstrate conformity with the policies and Figures of the Secondary Plan and will provide details including:
  - i) the size and location of schools, neighbourhood parks, village squares and urban squares;
  - ii) the location, size and general configuration of stormwater management ponds;
  - iii) the detailed road pattern;
  - iv) the specific boundaries of neighbourhood land use categories and other designations;
  - v) the density and distribution of housing types;
  - vi) how the proposal addresses the Town's Implementation Strategy;
  - vii) the location, alignment and boundaries of Medium Constraint Streams;
  - viii) co-ordination with land uses and road patterns for lands outside, but adjacent to the lands which are the subject of the area design plan; and
  - ix) the requirements for Transitional Areas as set out in Section 7.6.9.3 a).
- f) Such area design plans shall be prepared in accordance with terms of reference approved by the Town and the applicant(s), by a consultant approved by the Town and the applicant(s), and retained by, and at the cost of, the applicant(s).
- g) In the preparation of the terms of reference, the Development Review provisions of Section 7.8 of this Plan will be used as a guide.
- h) A proposed plan of subdivision or other development plan shall be considered to



be inconsistent with the Master Plan, as referenced in subsection c) i), if it does not conform to the General Design Directions in Section 7.5.4, or results in coordination issues between areas identified in subsection b. The following will generally not be considered to be inconsistent with the Master Plan, provided that the Town determines that the plan of subdivision or other development plan conforms with the policies and Figures of the Secondary Plan:

- i) modifications to, or relocations of portions of the road pattern;
  - ii) modifications to Medium or Low Constraint Stream Corridors or Hydrologic Features "A" and "B";
  - iii) relocation of public facilities including parks, schools and stormwater ponds; or
  - iv) reconfiguration of the neighbourhood land use categories generally in accordance with the neighbourhood land use category requirements of Table 1. However, any reconfiguration of neighbourhood land use categories proposed by a plan of subdivision or other development plan that necessitates significant changes to the distribution of neighbourhood land use categories on other land ownerships, in order to maintain general conformity with Table 1, will require the preparation of an area design plan.
- i) The approval of an area design plan by the Town shall not require an amendment to this Plan.

#### **7.5.3 DESIGN GUIDELINES**

Urban Design and Open Space Guidelines, which will include guidelines with respect to matters set out in Section 7.5 as well as heritage resources, will be prepared by the Town as part of the North Oakville East Implementation Strategy prior to the approval of any development applications. All development applications will be evaluated by the Town to ensure that they are consistent with the Urban Design and Open Space Guidelines.

#### **7.5.4 GENERAL DESIGN DIRECTIONS**

- a) All development, particularly in the Urban Core Areas, Neighbourhood Centre and General Urban Areas, shall be designed to be compact, pedestrian and transit friendly in form. Mixed use development will be encouraged.
- b) A hierarchy of civic, institutional and commercial uses shall be established in locations which form part of mixed use areas (rather than isolated in single-use complexes) which allow them to serve as focal points for the Planning Area as a whole, neighbourhoods and sub-neighbourhoods. In particular, the ordinary activities of daily living shall occur within walking distance of most dwelling units.
- c) Development shall be based on a modified grid road system with interconnected networks of roads designed to disperse and reduce the length of vehicular trips



and support the early integration and sustained viability of transit service. For local roads not shown on Figure NOE4, the modified grid road system will respond to topography and the Natural Heritage System component of the Natural Heritage and Open Space System. Cul-de-sacs will generally be permitted only when warranted by natural site conditions

- d) The Natural Heritage component of the Natural Heritage and Open Space System forms a central feature of the Planning Area and the development form should reflect this fact. In addition, an associated comprehensive, interconnected system of trails will be developed which will generally reflect the major trail system on Figure NOE4.
- e) Public safety, views and accessibility, both physically and visually, to the Natural Heritage component of the Natural Heritage and Open Space System, as well as to parks, schools and other natural and civic features, will be important consideration in community design. This will be accomplished through a range of different approaches including, but not limited to, the use of single loaded roads, crescent roads, combining public open space with other public or institutional facilities (e.g. school/park campuses, easements, stormwater ponds adjacent to the Natural Heritage component of the System) and the location of high density residential and employment buildings. With respect to the Natural Heritage component of the Natural Heritage and Open Space System, priority will be given to maintaining views and accessibility at key trail access points where Arterial, Avenue and Connector roads are adjacent to the System. Where there is no significant Natural Heritage edge exposed at Arterial, Avenue or Connector roads, Neighbourhood Parks, Village Squares or local roads will be encouraged to provide access and visibility.

With respect to other public facilities particularly schools and parks, of the total linear perimeter distance around such uses in the order of 50% of the perimeter will be bounded by a combination of roads and open space which will allow public access or, at a minimum private open space which will allow significant views of the feature or facility. Where only road frontage is provided the frontage shall be in the order of 40% of the perimeter of the features and facilities.

- f) Parks, neighbourhood activity nodes, and other civic areas will serve as central "meeting places" for residents, particularly within neighbourhoods and sub-neighbourhoods
- g) Building densities and land uses designed to support the use of transit and the level of transit service proposed for specific areas shall be located within walking distances of transit stops and lines.
- h) The incorporation of cultural heritage resources into the community, including their use and adaptive reuse, shall be encouraged.
- i) The location of *active transportation* facilities should conform with Schedule D, Active Transportation Plan, to the Livable Oakville Plan, recognizing that the alignments are diagrammatic. An official plan amendment will not be required for changes to the alignments of *active transportation* facilities provided that the general intent and purpose of Schedule D to the Livable Oakville Plan is

maintained. In the event of a conflict between Schedules D to the Livable Oakville Plan and the North Oakville East Secondary Plan, the North Oakville East Secondary Plan shall prevail.

### **7.5.5 STREETScape**

The streetscape consists of streets and public frontages. The streets provide the traffic and parking capacity. The public frontages provide for pedestrian circulation and contribute to the character of the streetscape. The design of the streetscape is defined by the type of sidewalk, curbing, planters and street trees. The policies of Section 7.7.2 of this Plan provide detailed direction with respect to the design of typical streets, while the Urban Design and Open Space Guidelines will address directions for public frontages and other design treatments.

The Urban Design and Open Space Guidelines will establish five types of streets with respect to typical design treatment. Variations may be considered by the Town based on circumstances such as topography, proposed abutting land use, relationship to the Natural Heritage and Open Space System and achievement of other design objectives:

#### **7.5.5.1 Arterial/Transit Corridors**

Arterial/Transit Corridors as designated on Figure NOE4 are high capacity roads which serve as major gateways into the community, including both the Town as a whole, and North Oakville East. A balance must be achieved between their transportation function, including accommodation for transit, and their ability to provide access to flanking land uses, and to act as socially vibrant public space. To assist in achieving this balance, these streets will have the highest form of design treatment, including wide sidewalks, special tree and feature planting, paving, lighting and signage design.

#### **7.5.5.4 Local Streets**

Local streets play a dual role as neighbourhood socialization spaces, as well as supporting transportation needs. The design requirements, while less substantial than for connector roads, must support the dual role of the local streets.

#### **7.5.5.6 Pedestrian/Cyclist Orientation**

Pedestrian/cyclist comfort and safety shall be considered in the streetscape design for roads under the control of the Town of Oakville. In commercial and mixed use areas, the design will provide for an enhanced streetscape and sidewalk environment for pedestrians. The Town will also work with the Region to encourage a similar approach.

#### **7.5.5.9 Sidewalks**

- a) Sidewalks shall generally be provided on both sides of all streets with the exception of:
  - i) residential streets with less than ten dwelling units or cul-de-sacs, where sidewalks shall be required on only one side of the street;
  - ii) lanes, where no sidewalks shall be required;
  - iii) character roads, where a rural cross-section is being maintained, where sidewalks may not be required, provided that pedestrian and bicycle circulation is accommodated on a separate trail system; and,
  - iv) a road flanking the Natural Heritage and Open Space System, where a sidewalk shall be provided on the developed side only, subject to the availability of a trail facility on the other side of the street.
- b) In addition to the exceptions above, consideration may be given to permitting only one sidewalk for some Local Roads. Such roads would be permitted only where the Town is satisfied through the submission of a pedestrian circulation plan that only one sidewalk is necessary and provided that the road would:
  - i) have a maximum ROW of 16 metres; and,
  - ii) be located in the Neighbourhood Area designation in a Sub-urban land use category
  - iii) not provide direct access to a school, Neighbourhood Park, or Village Square.

The geographical extent of the pedestrian circulation plan shall be broad enough to demonstrate how the above conditions are met as well as the relationship to the transit system, community facilities and shopping areas.

#### **7.5.5.14 Block Size**

The length of the block makes a significant difference in creating a pedestrian-friendly environment. Blocks should be short and regular in length to make walking efficient and allow for variation in routes. Where it is impossible or undesirable to provide short blocks, wide public mid-block pathways should be provided to shorten walking distances.

#### **7.5.6 BUILDING LOCATION**

- a) Buildings, structures and landscaping shall be designed to provide visual interest to pedestrians, as well as a "sense of enclosure" to the street. Generally, heights of buildings shall also be related to road widths to create a more comfortable pedestrian environment, so that the wider the road width, the higher the building height.
- b) Buildings on corner lots at the intersections of Arterials, Avenues and Connector streets shall be sited and massed toward the intersection.
- c) The rear and side building elevations of all buildings on corner lots shall be designed to take advantage of their extra visibility.
- d) In residential areas, garages shall be designed so that they are not the dominant feature in the streetscape. In particular, attached garages shall not:
  - i) project beyond the façade of the dwelling or the façade (front face) of any porch; or
  - ii) contain garage doors that occupy more than 50% of the frontage of a lot unless the Town is satisfied through the submission of detailed plans by the applicant that the garage doors can be appropriately integrated with the streetscape.
- e) Higher-density housing shall be located close to Arterial Roads, Avenues, Connectors and transit stops, within Neighbourhood Centres and the Urban Core Areas.

## **7.5.7 PARKING AREAS**

### **7.5.7.1 Off-Street Parking**

Off-street parking areas shall be designed to reduce their visual impact on both the adjoining streetscape and on people using the facility by:

- a) Screening of the parking lot at the street and adjacent to residential development through the use of features such as low fences, walls and landscaping and in a manner which reflects the safe community design policies of Section 7.5.10;
- b) Locating the parking lot, within commercial or mixed use developments, to the side or rear of the main building and permitting no or only minimal parking in front of the main building. However, in sub-area 2 of the Trafalgar Urban Core, within the Dundas Urban Core Area at Neyagawa and within the Neyagawa Urban Core, where large retail stores are proposed, design alternatives designed to contribute to the creation of a vibrant and active streetscape, may include, among other possible alternatives, locating large retail stores in the interior or at the rear of commercial or mixed use development blocks with smaller stores and building oriented to surrounding Arterial, Avenue or Connector Roads to create a strong street presence. Alternatively, the frontage of the large retail store facing the Arterial, Avenue or Connector Road should be lined with smaller stores or multiple building entrances. Parking areas will be integrated with development associated with large retail stores, in a manner designed to contribute to the objective of a vibrant and active streetscape.
- c) The use of landscaping or decorative paving to reduce the visual expanse of large parking areas;
- d) Joint access to parking lots on adjoining properties where feasible; and,
- e) Provision of pedestrian walkways adjacent to stores, between building clusters, and to provide pedestrian access to transit stops, public sidewalks and other developments. These walkways may need to cross parking lots to provide the required access.

### **7.5.7.2 Parking Options**

In addition, options to replace at-grade parking areas will be encouraged including:

- a) On-street parking  
On-street parking should be permitted wherever possible to increase animation, reduce vehicle speeds and serve as a protective buffer between pedestrians and moving vehicles. To encourage the provision of such parking, appropriate engineering design standards for roadways, including laybys, shall be developed and reduced off-street parking requirements will be established for specific areas where appropriate, particularly along transit routes and in the Urban Core designations.
- b) Municipal parking  
The Town shall work with landowners in mixed use areas to establish municipal parking at strategic nodes.



#### **7.5.9 LANDSCAPE DESIGN**

The applicable policies of Part C, Section 10.3, Urban Forests of the Official Plan shall apply and the Town shall establish specific landscaping requirements in the Urban Design and Open Space Guidelines to ensure:

- a) the creation of a human scale within new development;
- b) the enhancement of pedestrian comfort;
- c) the provision of features which contribute to the definition of public open space, framing of views and focal points, direction of pedestrian movement and demarcation of areas with different functions; and,
- d) landscape design that promotes the use of native species and enhancement of ecological stability and integrity.

#### **7.5.10 SAFE COMMUNITY DESIGN**

The Town shall work with Halton Region Police to promote safety and security and accessibility in public places through urban design including design and the siting of buildings and structures that:

- a) encourages continuous occupancy of public spaces by ensuring the proximity of spaces, activities and institutions which provide public presence at various times and by the sharing of facilities;
- b) provides for opportunities for visual overlook and ease of public access to adjacent streets, parks and other public areas;
- c) results in clear, unobstructed views of parks, school grounds, and open spaces from adjacent streets;
- d) ensures appropriate lighting, visibility and opportunities for informal surveillance are provided for walkways, parking lots, parking garages and open space areas;
- e) results in the selection and siting of landscape elements in a manner which maintains views for safety and surveillance;
- f) encourages the provision of views into, out of and through publicly accessible interior spaces;
- g) precludes entrapment or the perception of entrapment through properly identified exits and signage; and,
- h) results in accessibility for the disabled and elderly.

### **7.5.11 COMMUNITY LINKAGES**

North Oakville East will be connected to other parts of the Town whenever possible through road, transit, pedestrian and bicycle links, to ensure that the community functions in an integrated manner. In particular:

- a) the design of Dundas Street and abutting uses should allow it to become a point of transition rather than the current "division" or "barrier" effect of the street. Design should be used to encourage visual and physical links between the north and south sides of the street;
- b) the intersections of Trafalgar Road and Dundas St. and Neyagawa Blvd and Dundas St. should be marked by the siting and design of prominent buildings, strategic building placement and other gateway features. Minor gateways should be established at other intersections with Dundas St;
- d) Trafalgar Road provides a major physical north/south link through the Town from a transportation perspective. The design of the road itself and the Trafalgar Core Area development should strengthen this link and ensure a strong relationship to the Uptown Core, as well as the Midtown Core, and Downtown areas.

### **7.5.12 NEIGHBOURHOODS**

Figure NOE1 identifies the neighbourhood structure for North Oakville East. Each neighbourhood will have distinctive characteristics, but with the following common features:

- a) Each neighbourhood will include at its centre, approximately a five minute walk from most areas of the neighbourhood, a neighbourhood activity node which would include a transit stop and other public facilities which serve the neighbourhood such as central mail boxes or mail pickup facilities. In addition, convenience commercial facilities or similar uses will be encouraged to locate at the neighbourhood activity node.
- b) Neighbourhoods shall be primarily residential in character, but will include mixed use development including commercial, institutional, live-work and civic facilities;
- c) Within neighbourhoods, a range of lot sizes, building types, architectural styles and price levels shall be provided to accommodate diverse ages and incomes;
- d) The Town shall work with the School Boards to try to ensure that elementary schools are located to allow as many children within a neighbourhood as possible to walk or bicycle to them; and,
- e) A range of open space including parks, squares and playgrounds shall be distributed through a neighbourhood in locations which are easily accessible and which can act as "meeting places" for residents.

Zoning By-law 2009-189

**2.1 Zones** (2012-001), (2013-065)

This By-law establishes the following *zones* and places all lands subject to this By-law in one or more of the following *zones*:

Table 2.1.1: Zones in the North Oakville Zoning By-law		
Zone	Symbol	Uses / Standards (non-operative)
Trafalgar Urban Core	TUC	5.1 and 7.1
Dundas Urban Core	DUC	5.1 and 7.2
Neyagawa Urban Core Area	NUC	5.1 and 7.3
Palermo Village North Urban Core	PUC	5.1 and 7.4
Neighbourhood Centre	NC	5.1 and 7.5
General Urban	GU	5.1 and 7.6
Sub-urban	S	5.1 and 7.7
High Density Residential	HDR	5.1 and 7.8
Light Employment	LE	5.1 and 7.9
General Employment	GE	5.1 and 7.10
Service Area - Employment	SA	5.1 and 7.11
Institutional	I	5.1 and 7.12
Future Development (2022-007)	FD	5.1 and 7.13
Natural Heritage System	NHS	5.1 and 7.14
Cemetery	CE	5.1 and 7.15
Park	P	5.1 and 7.12
Stormwater Management Facility	SMF	5.1 and 7.12
Automotive Service	AS	5.1 and 7.17

## Future Development Zone

### 7.13 Future Development (FD) Zone Regulations *(2012-001)* *(2022-007)*

#### 7.13.1 Permitted Uses

- i. The uses as permitted in Section 4.3.1.
- ii. Infrastructure for which an Environmental Assessment has been completed or which are required as a condition of approval under the Planning Act, subject to the following provisions:
  - a. Notwithstanding any other provision of this By-law, any *building* or *structure* for the purpose of providing or sheltering infrastructure shall not be subject to Section 7.13.2;
  - b. Any *building* or *structure* used for a *general office use*, *public works yard*, or *warehouse* shall not be permitted.
- iii. The *use* of legally existing *buildings* and *structures* by new *accessory uses*.

#### 7.13.2 Permitted Expansion of Legal Buildings and Structures

The existing *lot coverage* on the day of the passage of this by-law, for all existing *buildings* or *structures* and existing *accessory buildings* or *structures*, may be increased by a maximum of 10% subject to regulations of this section and section 5.

#### 7.13.3 Maximum Height

10 metres except for *agricultural buildings*

#### 7.13.4 Minimum Yards

*Front Yard* and *Flankage* - 9 m  
*Side Yard* - 2.4 m  
*Rear Yard* - 7.5 m

*(Performance zones relocated by 2012-001 and 2013-065)*

## General Urban Zone

### 7.6 General Urban (GU) Zone Regulations

#### 7.6.1 Uses Permitted

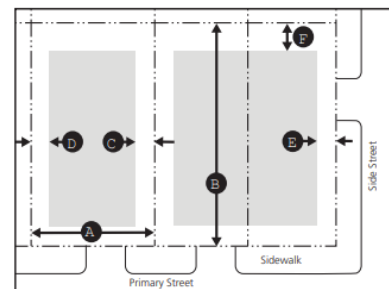
See Section 6.

7.6.2 Building Types Permitted and Related Standard						
Building Type	Minimum Lot Frontage	Minimum Lot Depth	Minimum Interior Side Yard Setback One Side	Min. Interior Side Yard Setback Opposite Side	Minimum Flankage Setback	Minimum Rear Yard Setback
	A	B	C	D		
<i>A single detached dwelling street access attached private garage</i>	8.5 m	22 m	1.2 m	0.6 m	2 m	7 m <sup>i</sup>
<i>A single detached dwelling street access detached private garage or parking pad</i>	8.5 m	22 m	3 m	0.6 m	2 m <sup>E</sup>	7 m <sup>E</sup>
<i>A single detached dwelling attached rear private garage accessed from the front or side</i>	9 m	26 m	3 m	0.6 m	2 m	0.3 m
<i>A single-detached dwelling with lane access</i>	8 m	17 m	1.2 m	0.6 m	2 m	0.75 m
<i>Semi-detached dwelling unit street access attached private garage</i>	5.5 m / unit	22 m	0.9 m	0.0 m	2 m	7 m
<i>Semi-detached dwelling unit street access detached private garage or attached rear private garage accessed from front or side</i>	5.5 m / unit	26 m	3 m	0.0 m	2 m	0.3 m



7.6.2 Building Types Permitted and Related Standard						
Building Type	Minimum Lot Frontage <b>(A)</b>	Minimum Lot Depth <b>(B)</b>	Minimum Interior Side Yard Setback One Side <b>(C)</b>	Min. Interior Side Yard Setback Opposite Side <b>(D)</b>	Minimum Flankage Setback <b>(E)</b>	Minimum Rear Yard Setback <b>(F)</b>
<i>Semi-detached dwelling unit with lane access</i>	5.5 m / unit	17 m	0.9 m	0.0 m	2 m	0.75 m
<i>Triplex or duplex</i>	7 m	23 m	1.5 m	1.5 m	2 m	0.75 m
<i>Townhouse dwelling unit street access private garage</i>	4.9 m / unit	23 m	Interior Unit 0.0 m Exterior Unit 1.2 m	Interior Unit 0.0 m Exterior Unit 1.2 m	2 m	6 m
<i>Townhouse dwelling unit with lane access</i>	5.5 m / unit	17 m	Interior Unit 0.0 m Exterior Unit 1.2 m	Interior Unit 0.0 m Exterior Unit 1.2 m	2 m	0.75 m
<i>townhouse dwelling unit back to back</i>	5.5 m / unit	12 m	Interior Unit 0.0 m Exterior Unit 1.2 m	Interior Unit 0.0 m Exterior Unit 1.2 m	2 m	0.0 m

<sup>1</sup> The minimum *rear yard setback* may be reduced to 3.0 m for a one *storey* addition for a maximum of 45% of the dwelling width measured at the rear of the *main building*. The one *storey* addition shall have a maximum vertical distance of 4.0 m measured between the finished floor level of the *first storey* and the highest point of the roof of the one *storey* addition. (2022-007)



The grey represents potential building area. The internal dashed line represents the maximum yard.

### 7.6.3 Front Yard Types Permitted - Minimum Setback

*Common Yard* - 2.5 m

*Porch Yard* - 2.5 m

*Inset Porch Yard* - 2.5 m

### 7.6.4 Permitted Yard Encroachments

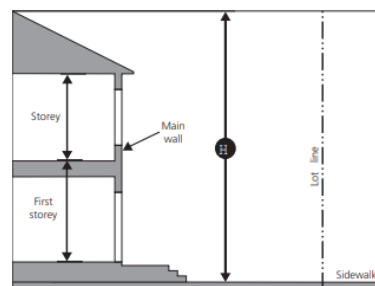
See Section 4.21

### 7.6.5 Maximum Height

H 3 storeys

### 7.6.6 Parking

See Section 5



*Height is measured to the tallest point of a building.*

### 7.6.7 Accessory Buildings and Structures

**See the following sections with respect to standards for:**

<i>Accessory buildings and structures other than private garages and walkways</i>	See Section 4.14
<i>Detached private garages accessed by a driveway from a street</i>	See Section 4.15
<i>Detached private garages accessed by a lane</i>	See Section 4.16
<i>Attached private garages accessed by a lane</i>	See Section 4.17
<i>Attached private garages accessed by a street</i>	See Section 4.18
<i>Standards for parking pads</i>	See Section 4.19
<i>Enclosed and roofed walkways</i>	See Section 4.20

### 7.6.8 Minimum Landscape Area

*Minimum landscape area* - 10 %

### 7.6.9 Performance Zone Categories – General Urban (GU) Zone

#### 7.6.9.1 General Urban Performance (GU-1) Zone 1

The regulations of the *GU Zone* shall apply in the General Urban Performance (GU-1) *Zone 1*, however, in addition an elementary *public school* shall be a permitted use subject to the regulations of the Institutional (I) *Zone*.

#### 7.6.9.2 General Urban Performance (GU-2) Zone 2

The permitted *uses, buildings* and regulations of the *GU Zone* shall apply in the General Urban Performance (GU-2) *Zone 2*, however, notwithstanding the provisions of Section 5.1.3, Location of *Park-*

*ing Spaces*, the required *parking spaces* for any *use* may be located on the *lot* on which the *use* is located, and/or on one half of the *street* abutting the *lot* where the on-street parking is permitted.

### 7.6.9.3 General Urban Performance (GU-3) Zone 3

The permitted *uses, buildings* and regulations of the *GU Zone* shall apply in the General Urban Performance (GU-3) *Zone 3*, however, notwithstanding the provisions of Sections 4.15.2 and 4.16.3, Maximum *Height for Private Garages*, the maximum *height* if the *lot frontage* is 9 metres or more, for a roof, other than a flat roof, shall be 9 metres.

## Natural Heritage System Zone

### 7.14 Natural Heritage System (NHS) Zone Regulations

(2012-001)

#### 7.14.1 Permitted Uses

- i. The *uses* as permitted in Section 4.3.1;
- ii. *Infrastructure* located within one of:
  - a. A *public street*; or,
  - b. A future *public street* for which an Environmental Assessment has been completed or a Planning Act approval has been received.
- iii. The *use* of legally existing *buildings* and *structures* by new *accessory uses*.

#### OMB Appeals

7.14.1(ii) Appeal seeks additional permitted locations for infrastructure. Section is otherwise in effect.

PL100041  
(2009-189 original passage)

#### 7.14.2 Building Types Permitted

Legally existing *buildings* and *structures*.

#### 7.14.3 Parking Standards

See Section 5

#### 7.14.4 Performance Zone Categories – Natural Heritage System (NHS) Zone

##### 7.14.4.1 Natural Heritage System Performance (NHS-1) Zone 1

In addition to the *uses* permitted in the Natural Heritage System (NHS) *Zone* in Section 6, *stormwater management facilities* and *accessory uses, accessory buildings* and *accessory structures* shall be permitted in the Natural Heritage Performance (NHS-1) *Zone 1*.

##### 7.14.4.2 Natural Heritage System Performance (NHS-2) Zone 2

In addition to the *uses* permitted in the Natural Heritage System (NHS) *Zone* in Section 6, a private *driveway* with a maximum width of 6 m shall be permitted in the Natural Heritage Performance (NHS-2) *Zone 2*.

#### 7.14.4.3 Natural Heritage System Performance (NHS-3) Zone 3

In addition to the *uses* permitted in the Natural Heritage System (NHS) Zone in Section 6, the legally existing *buildings* and *structures* in the Natural Heritage Performance (NHS-3) Zone 3 may be used, in addition to any legally existing *uses*, for institutional *uses* including an *art gallery* and *art school*.

### 4.21 Encroachments Permitted in Required Yards

Every *yard* shall be open and unobstructed from the ground to the sky by any *structures*, except for:

- i. lawful fences, signs, and retaining, noise and ornamental walls;
- ii. *driveways* and walks; and,
- iii. the encroachments listed in Table 4.21 which are permitted in association with either the main *building*, or an *accessory buildings* or *structures*, subject to compliance with the regulations listed in the columns of the Table 4.21.

**Table 4.21 - Permitted Encroachments into Required Yards**

	<b>Structure</b>	<b>Yard in which Structure is Permitted</b>	<b>Maximum projection into required yard</b>	<b>Minimum Distance from Lot Line<sup>1</sup></b>
(a)	Sills, belt courses, cornices, coves, pilasters, eaves, gutters, awnings, canopies or other non-structural architectural features	All	0.6m	n/a
(b)	Fire Escapes	<i>Rear &amp; Side</i>	1.5m	0.6m from the <i>lot line</i>
(c)	Exterior stairways	<i>Rear &amp; Side</i>	1.5m	0.6m from the <i>lot line</i>
(d)	<i>Balconies</i>	<i>All</i>	1.5m	1.5m unless overtop a <i>porch</i> in which case the <i>setbacks</i> shall be the same <i>setbacks</i> as the <i>setback</i> for a <i>porch</i> under sub-section (e)

**Table 4.21 - Permitted Encroachments into Required Yards**

	<b>Structure</b>	<b>Yard in which Structure is Permitted</b>	<b>Maximum projection into required yard</b>	<b>Minimum Distance from Lot Line<sup>1</sup></b>
(e)	<i>Uncovered platforms 0.6 m or greater in height measured from surrounding grade and porches, including a balcony on top of a porch including vertical supports and a roof above and landings. However, any exterior stair required to provide access to an uncovered platform or porch shall be permitted to encroach into any required yard.</i>	<i>Front</i>	1.5m	1m from the <i>front lot line</i> with exception of any exterior stair which may encroach to within 0.5m of the <i>front lot line</i> .
		<i>Rear</i>	2.5m	0.6m from the <i>rear lot line</i>
		<i>Interior Side</i>	n/a	0.6m from the <i>interior side lot line</i> except: i. where the platform is designed to allow drainage to occur underneath in which case the min. <i>setback</i> shall be 0m; and, ii. for <i>townhouse</i> and <i>semi detached</i> units in which case it may be 0m from an interior common wall
		<i>Flankage</i>	n/a	1m from the <i>flankage</i> with the exception of any exterior stair which may encroach to within 0.5m of the <i>flankage</i>
(f)	Chimneys and gas fireplace projections and chases (with a maximum width of 1.8m)	<i>Front &amp; Flankage</i>	0.6m	1m from the <i>front lot line</i> and <i>flankage</i>
		<i>All other yards</i>	0.6m	0.6m from the <i>lot line</i>
(g)	Bay, Box out and Bow Windows with or without foundations with a maximum width of 3m which may be a maximum of three storeys in height and which may include a door.	<i>Front &amp; Flankage</i>	0.6m	1m from the <i>front lot line</i> and <i>flankage</i>
		<i>All other yards</i>	0.6m	0.6m from the <i>lot line</i>
(h)	Uncovered stairs below grade accessing a main building	<i>Rear</i>	1.5m	0.6m from the <i>lot line</i>
		<i>Side</i>	0m	1.5m
(i)	<i>Uncovered platforms less than 0.6m in height measured from surrounding grade</i>	<i>Front</i>	1.5m	1m from the <i>front lot line</i> and <i>flankage</i>
		<i>Interior Side</i>	n/a	0.6m from the <i>interior side lot line</i> except: i. where the platform is designed to allow drainage to occur underneath in which case the minimum <i>setback</i> shall be 0m; and, ii. for <i>townhouse</i> and <i>semi detached</i> units in which case it may be over 0m from an interior common wall
		<i>All other yards</i>	n/a	0.6m from the <i>lot line</i>
(j)	Unenclosed barrier free ramps	All	n/a	0.6m from the <i>lot line</i> unless the ramp is designed to allow drainage to occur underneath in which case the minimum <i>setback</i> shall be 0m

Minimum distance from lot line does not apply where minimum building setback is 0 metres.



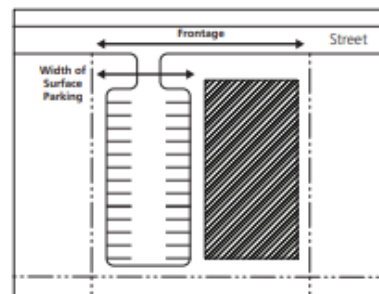
## Parking and Loading Regulations

### 5.1.6 Surface Parking Area Location

Where *surface parking areas* are permitted on lots which abut Trafalgar Road, the maximum *lot frontage* or *flankage* on Trafalgar Road occupied by *surface parking area* shall be:

<i>Office building</i>	50%
<i>Mixed use and apartment buildings</i>	20%
<i>Commercial buildings</i>	20%
<i>Parking garage</i>	5%
<i>Institutional building</i>	5%
<i>Hotel</i>	5%
<i>Commercial/Residential buildings</i>	5%

This shall be measured as the maximum percentage of *lot frontage* or *flankage* occupied by *surface parking area* where no *building edge* is located within 6 metres of the Trafalgar Road *lot frontage* or *flankage*. The only exception shall be where the By-law permits a greater *front yard* or *flankage setback*, in which case that setback shall apply.



Surface Parking Frontage.

### 5.1.7 Hardscape Surface Treatment (2022-007)

All *parking spaces*, *tandem parking spaces*, *parking pad*, *loading dock*, *aisles* and *driveways* in any Zone other than a Natural Heritage Zone shall be surface treated with asphalt, concrete, interlocking brick, similar hardscaped surface, or other material sufficient to provide stability, prevent erosion, be usable in all seasons, and allow infiltration of surface water.

**Table 5.1A - Parking Requirements For Residential Uses**

(4)	<i>Apartment - More than 4 storeys</i>	Up to 1.25 <i>parking spaces</i> per <i>dwelling unit</i> , plus 0.2 <i>parking spaces</i> per <i>dwelling unit</i> for visitors. Additional <i>parking spaces</i> shall not be permitted. In the Trafalgar Urban Core Zone, no <i>parking spaces</i> shall be permitted in a <i>surface parking area</i> , with the exception of visitor <i>parking spaces</i> which may be located underground, in a <i>parking garage</i> or in a <i>surface parking area</i> .
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Table 5.1B - Parking Requirements For Non-Residential Uses		
(4)	<i>Café</i>	<p>No <i>parking spaces</i> shall be required and a maximum of 1 <i>parking space</i> per 20 square metres of <i>leasable floor area</i> shall be permitted.</p> <p>In the Trafalgar, Dundas, Neyagawa and Palermo Village North Urban Core Zones, no additional <i>parking spaces</i> above the minimum shall be permitted, unless such parking is located in a <i>parking garage</i> or <i>on-street</i>.</p>
(5)	<i>Club</i>	<p>1 <i>parking space</i> per 30 square metres of <i>leasable floor area</i> minimum; and,</p> <p>1 <i>parking space</i> per 20 square metres of <i>leasable floor area</i> maximum.</p> <p>In the Trafalgar Urban Core Zone, no additional <i>parking spaces</i> above the minimum shall be permitted, unless such parking is located in a <i>parking garage</i>.</p>
(6)	<i>Commercial fitness centre; or, Place of amusement</i>	<p>1 <i>parking space</i> per 30 square metres of <i>leasable floor area</i> minimum; and,</p> <p>1 <i>parking space</i> per 20 square metres of <i>leasable floor area</i> maximum.</p> <p>In the Trafalgar Urban Core Zone, no additional <i>parking spaces</i> above the minimum shall be permitted, unless such parking is located in a <i>parking garage</i>.</p>
(7)	<i>Commercial residential -combined (mixed use)</i>	The applicable regulations for <i>non-residential uses</i> apply to the <i>non-residential floor area</i> , while the applicable residential regulations apply to the <i>dwelling units</i> .
(8)	<i>Commercial residential -connected (live-work)</i>	<p>Residential Component: 1 <i>parking space</i> minimum and 2 <i>parking spaces</i> maximum per <i>dwelling unit</i> which may be provided in tandem; and,</p> <p>Commercial Component: i) up to 90 square metres - 2 <i>parking spaces</i> ii) over 90 square metres - the applicable regulations for <i>non-residential uses</i> apply to the <i>non-residential floor area</i></p>
(9)	<i>Commercial school</i>	<p>1 <i>parking space</i> per 30 square metres of <i>leasable floor area</i> minimum; and,</p> <p>1 <i>parking space</i> per 20 square metres of <i>leasable floor area</i> maximum.</p> <p>In the Trafalgar Urban Core Zone, no additional <i>parking spaces</i> above the minimum shall be permitted, unless such parking is located in a <i>parking garage</i>.</p>
(10)	<i>Commercial self storage</i>	1 <i>parking space</i> per 650 square metres of <i>leasable floor area</i> minimum.

## Section 3.0

# Definitions

“**stormwater management facility**” means an end-of-pipe, managed detention or retention basin, which may include a permanent pool, designed to temporarily store and treat collected stormwater runoff and release it at a controlled rate or direct it for an intended reuse. (2012-001)



February 4, 2025

Project Name: Argo Lions Valley Ltd.

DOCA Project Number: 2025-0123

Proponent: Whiteoaks Communications Group Ltd.

Project Location: 1303 Dundas Street West

Approval Authority: Town of Oakville

Dear Vicky Giang,

This letter is to confirm receipt of the project-related correspondence sent by The Town of Oakville, on January 28, 2025, regarding Argo Lions Valley Ltd.

The Mississaugas of the Credit First Nation (MCFN) are the Treaty Holders of the land on which the project will take place – specifically, the Head of the Lake Treaty, No. 14. The MCFN holds Indigenous and Treaty Rights specific to the project location and its environs, which may be adversely impacted by it. The Department of Consultation and Accommodation (DOCA) is designated by the MCFN to handle consultation matters on its behalf.

The DOCA consultation team has reviewed the project-related correspondence identified above. **This project has been flagged for review.**

It is our expectation that the engagement relationship will be carried out by the project's proponent. Therefore, we request that the Town of Oakville immediately share this letter with the project's proponent outlining DOCA's request for next steps.

Until the requested engagement has been carried out by the proponent with DOCA, the MCFN asserts that Argo Lions Valley Ltd. requires further review by DOCA. Until that review has been completed and any questions, concerns, and/or interests have been satisfactorily addressed, the project must not receive approval from the Town of Oakville or proceed with any ground-altering activities.

From the proponent, we would like to request an introductory meeting. This meeting can be held virtually or at the DOCA office. We ask that they reach out to us at their earliest convenience to make these arrangements. We would also like to share the following expectations for meaningful engagement:



Mississaugas of the Credit First Nation



Phone: [REDACTED]

Fax: [REDACTED]



- DOCA must be notified of, invited to participate in, and provided the opportunity to review any environmental and/or archaeological assessments. Please let us know as soon as possible if these studies are forthcoming and when they are anticipated to occur. If these studies have been completed, please provide us with copies of the reports prior to the introductory meeting.
- At its discretion, DOCA may request capacity funding from the proponent for its consultation and engagement activities relating to the project. We are happy to discuss this item in further detail at the upcoming introductory meeting.

DOCA expects to be notified of any and all future project updates and/or changes.

If you have any questions for the DOCA consultation team, please feel free to contact us.

Thank you,

*Megan DeVries*

Megan DeVries  
Manager of Consultations  
Department of Consultation and Accommodation  
Phone: [REDACTED]  
Email: [REDACTED]

CC  
Payton Mitchell, Consultation Assistant, [REDACTED]



Mississaugas of the Credit First Nation  
[REDACTED]



Phone: [REDACTED]  
Fax: [REDACTED]



## REPORT

### Planning and Development Council

Meeting Date: March 17, 2025

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**FROM:** Planning and Development Department

**DATE:** March 4, 2025

**SUBJECT:** Public Meeting Report – Plan of Subdivision, Official Plan Amendment and Zoning By-law Amendment, Neatt (16 Mile Creek) Inc., 3056 Neyagawa Boulevard, File No. OPA 1321.02, Z.1321.02 and 24T-24006/1321

**LOCATION:** 3056 Neyagawa Boulevard

**WARD:** Ward 7

Page 1

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### RECOMMENDATION

1. That comments from the public with respect to the proposed Plan of Subdivision, Official Plan Amendment and Zoning By-law Amendment submitted by Neatt (16 Mile Creek) Inc., (File No.: 24T-24006/1321, OP.1321.02 and Z.1321.02), be received.
2. That staff consider such comments as may be provided by Council.

### KEY FACTS

The following are key points for consideration with respect to this report:

- **Nature of Application:** The applicant has applied for an official plan amendment, zoning by-law amendment and plan of subdivision. The purpose of the applications is to amend the North Oakville East Secondary Plan (NOESP) (the “**Official Plan**”) and Zoning By-law 2009-189 (the “**Zoning By-law**”) and register a plan of subdivision for the subject property to permit the proposed development.
- **Proposal:** The proposal consists of a high-density mixed-use development comprised of 7 buildings in 3 development blocks reflected in the draft plan of subdivision. The proposal is for 2,278 residential units



as well 1,551 square metres of retail space, in addition to new public street conveyances.

- **Public Consultation:** The applicant hosted an in-person Public Information Meeting (“**PIM**”) on October 16, 2024. Councillors Nanda and Xie and 18 members of the public attended.
- **Public Comments:** As of the print date over 100 letters have been received from the public regarding the proposed development. The matters raised are listed under Matters Under Review within this report. Letters received as of March 3, 2025 are attached under **Appendix “G”**.

## **BACKGROUND**

A portion of the subject property was previously planned for a high school however in 2021 the Halton District School Board selected an alternate location at the northwest corner of Sixth Line and Burnhamthorpe Road. There are radio towers located to the west of the subject property and 16 Mile Creek. While the radio towers are proposed to be removed as part of a separate development application, staff are reviewing the impact of the radio towers as part of matters to be considered which will be reported to Council as part of the future recommendation report.

## **APPLICATION SUMMARY**

**Applicant / Owner:** Neatt (16 Mile Creek) Inc.

**Purpose of Application(s):** The purpose of the official plan, zoning by-law amendment and plan of subdivision application is to change the Official Plan and Zoning By-law to allow for a high-density mixed-use development comprising of 7 buildings in 3 development blocks reflected in the draft plan of subdivision. The proposal is for 2,278 residential units as well 1,551 square metres of retail space, in addition to new public street conveyances.

An aerial photograph, existing Official Plan land use schedules, and an existing zoning excerpt from Zoning By-law 2009-189 are included in **Appendix ‘A’**.

**Effect of Application(s):** The effect of the plan of subdivision, official plan and zoning by-law amendment applications is to designate the entire property Dundas Urban Core (DUC) and increase maximum height from 8 storeys to 28 storeys and rezone the subject property from the Future Development (FD) zone to the Dundas Urban Core (DUC) zone along with site specific amendments to permit:

- 7 apartment buildings across three phases of development
- A unit mix of approximately 70% 1-bedroom units and 30% 2-bedroom units

- A range of tower heights from 15-28 storeys
- An overall project density of 607 units per gross hectare
- A total of 1,551 square metres of commercial-retail space and 2,278 residential apartments units
- Proposed land dedications for three new public streets, right-of-way widening along Neyagawa Boulevard
- A variety of publicly accessible open spaces

A copy of the applicant's concept plan, 3D rendering and elevations are included as **Appendix 'B'**.

A copy of the applicant's draft Official Plan Amendment is included as **Appendix 'C'**.

A copy of the applicant's draft Zoning By-law is included as **Appendix 'D'**.

#### **Submitted Plans / Reports:**

The proponent has provided technical supporting studies which are currently under review by various public agencies and internal town departments. A full circulation and assessment of the application is being undertaken. The following studies and supporting documentation are accessible on the Town's website by visiting [www.oakville.ca](http://www.oakville.ca) and searching Z.1321.02.

#### **Property Location:**

The subject land is located on the north-west corner of Neyagawa Boulevard and Dundas Street West.

#### **Surrounding Land Uses:**

The surrounding land uses are as follows:

- North – Sixteen Mile Sports Complex and western terminus of Sixteen Mile Drive
- South – across Dundas Street West is a neighbourhood of detached dwellings with a gas station and commercial pad at the southeast corner of Neyagawa Boulevard and Dundas Street West
- East – corner of Dundas Street West and Neyagawa Boulevard includes lands not subject to the development applications currently occupied by a detached dwelling. Across Neyagawa Boulevard to the east is a commercial plaza

- West – lands owned by St. Peter and Paul Parish, occupied by a church and open space

### **Key Milestones:**

Pre-Consultation Meeting	June 2023 and September 2024
Public Information Meeting	October 16, 2024
Pre-submission Review	N/A
Application Deemed Complete	December 20, 2024
P & D Council - Public Meeting	March 17, 2025
Date Eligible for Appeal	April 19, 2025

### **PLANNING POLICY & ANALYSIS**

The following policy and regulatory framework applies:

- Provincial Planning Statement, 2024
- Halton Region Official Plan
- North Oakville East Secondary Plan
- Zoning By-law 2009-189

A full analysis of the Provincial Planning Statement (2024), Halton Regional Official Plan, and North Oakville East Secondary Plan will be included within the future recommendation report.

Official Plan extracts are attached as **Appendix ‘E’** and Zoning By-law extracts are attached as **Appendix ‘F’**.

### **MATTERS UNDER REVIEW**

This application was recently received and is under review by Town departments and agencies. The following are the general issues that will be addressed in a future recommendation report, in addition to any comments from the public, Council and commenting agencies:

- Public & Council Comments/Concerns
- Climate Change/Sustainability Goals
- Consideration of applicable Provincial and Regional policy
- Conformity with North Oakville East Secondary Plan
- Zoning performance standards
- Proposed use and density

- 
- Context and transition to adjacent properties and built form
  - Integration/Impact on adjoining and adjacent properties
  - Proposed height and setbacks
  - Urban design (including an assessment of the proposed public realm and transition of building height)
  - Shadow/sun impacts
  - Proportion of commercial uses
  - Proportion of residential unit types (i.e. unit breakdown by size)
  - Transportation implications (including travel demand management strategies, and parking utilization)
  - Pedestrian and active transportation connections and walkability
  - Tree preservation
  - Stormwater management
  - Functional servicing
  - Noise impacts
  - Consideration of radio tower impacts

## **CONCLUSION**

Planning staff will continue to review and analyze the subject plan of subdivision, official plan and zoning by-law amendment applications and address all technical matters, if any, along with submitted public comments. No further notice is required; however, written notice of any future public meetings will be provided to those who have made written and/or verbal submissions.

## **CONSIDERATIONS**

### **(A) PUBLIC**

The applicant held a Public Information Meeting on October 16, 2024, and Councillors Nanda and Xie and 18 member of the public attended. The applicant has included a Public Information Meeting Report which includes a summary of the comments received. All other public comments received as of the date of this report are included as “**Appendix ‘G’**”.

Notice of complete application and public meeting were distributed to property owners within 240m of the subject property in accordance with the town’s current notice requirements and *Planning Act*.

### **(B) FINANCIAL**

None.

### **(C) IMPACT ON OTHER DEPARTMENTS & USERS**

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The application was circulated to internal and external departments and agencies for review. The application remains in technical review.

**(D) COUNCIL STRATEGIC PRIORITIES**

This report addresses Council's strategic priority/priorities:

- Growth Management, Community Belonging, Environmental Sustainability and Accountable Government.

**(E) CLIMATE CHANGE/ACTION**

The proposed development will be reviewed to ensure compliance with the Town's sustainability objectives of the Oakville Official Plan

**APPENDICES**

Appendix "A": Mapping

Appendix "B": 3D Rendering and Elevations

Appendix "C": Applicant's Draft Official Plan Amendment

Appendix "D": Applicant's Draft Zoning By-law Amendment

Appendix "E": Official Plan Extracts

Appendix "F": Zoning By-law Extracts

Appendix "G": Public Written Submissions

Prepared by:

Catherine Buckerfield, M.Pl., MCIP, RPP

Senior Planner, Current Planning - West

Recommended by:

Paul Barrette, MCIP RPP

Manager of Current Planning – West

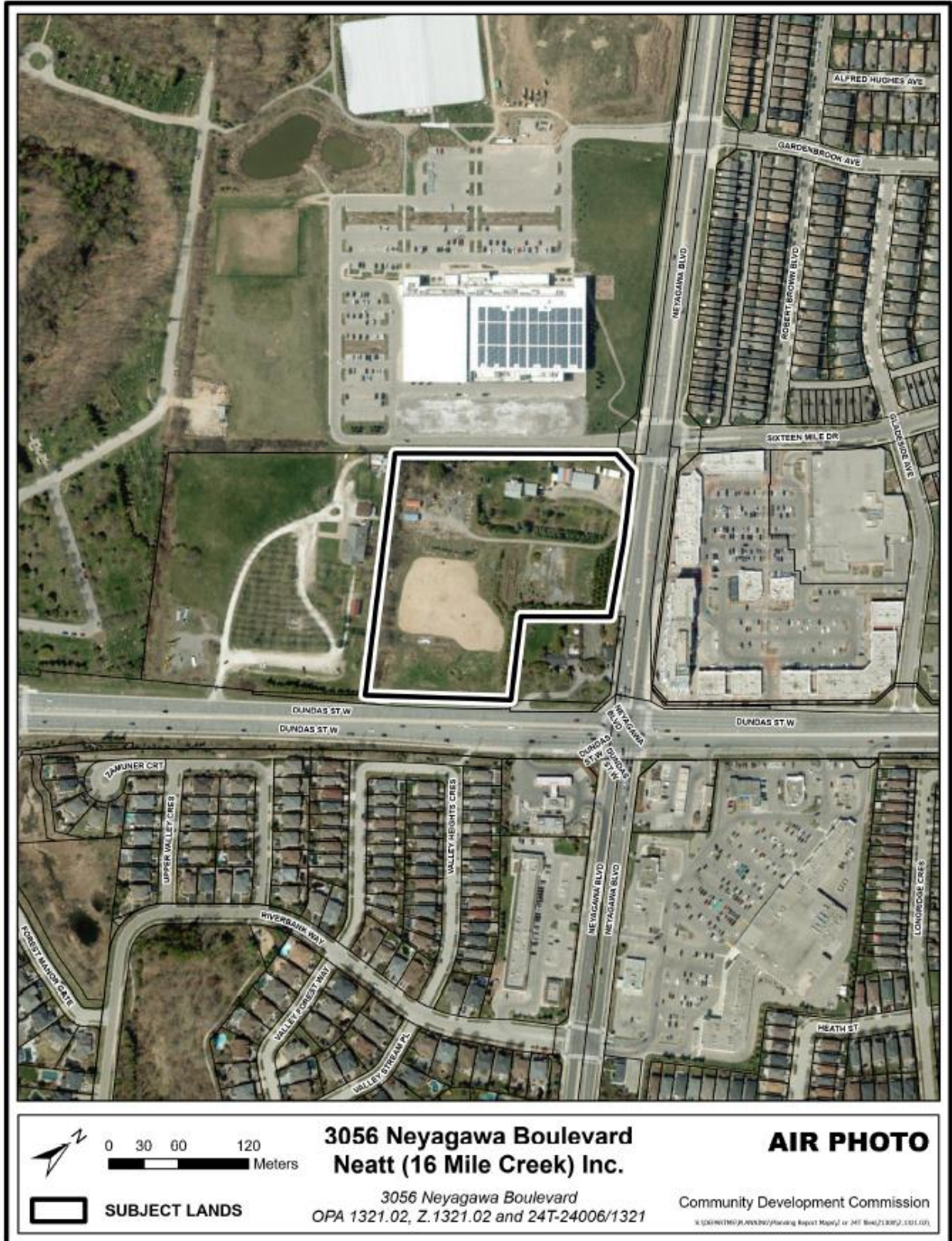
Submitted by:

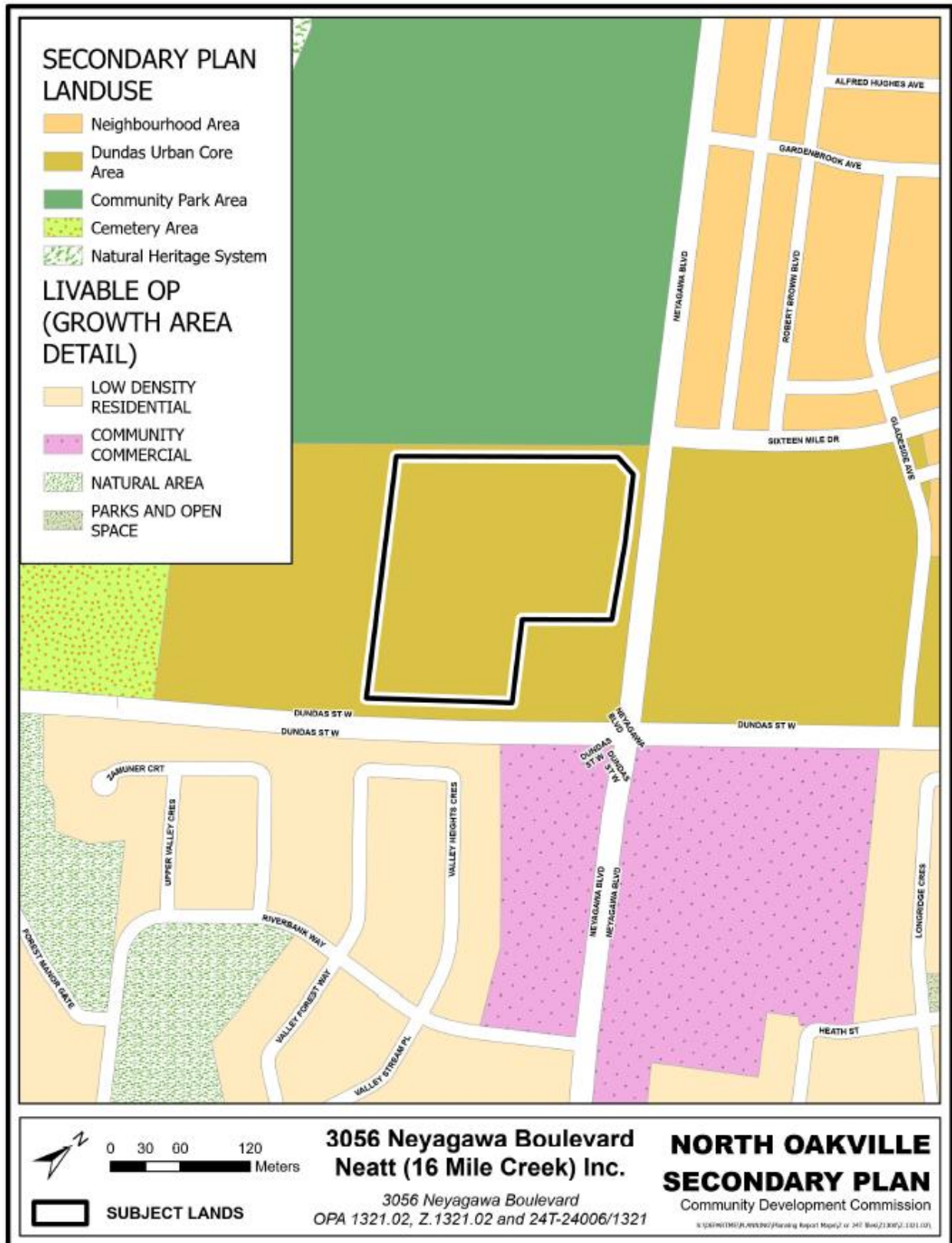
Gabriel A.R. Charles, MCIP RPP

Director, Planning & Development

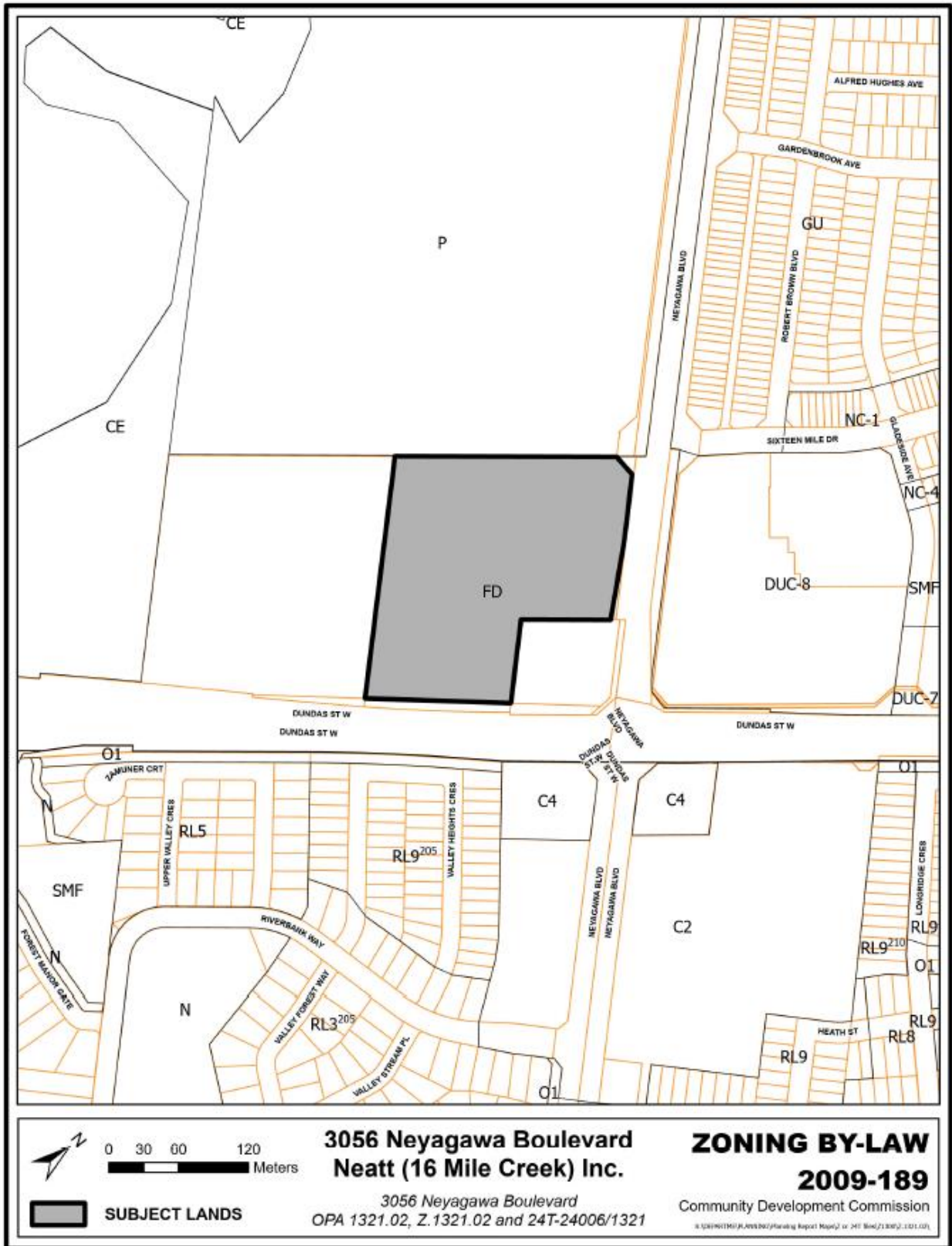


## APPENDIX A: Mapping

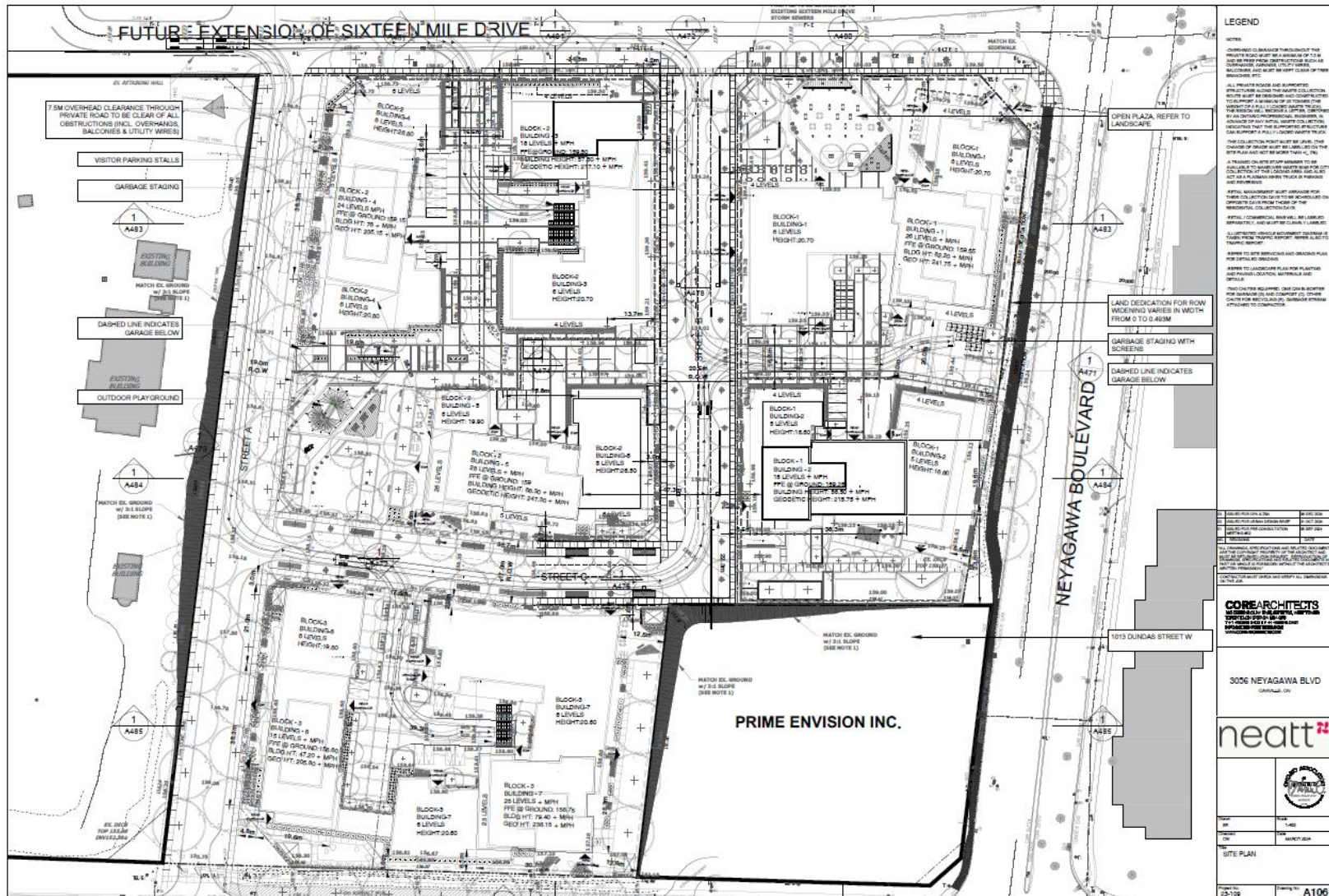




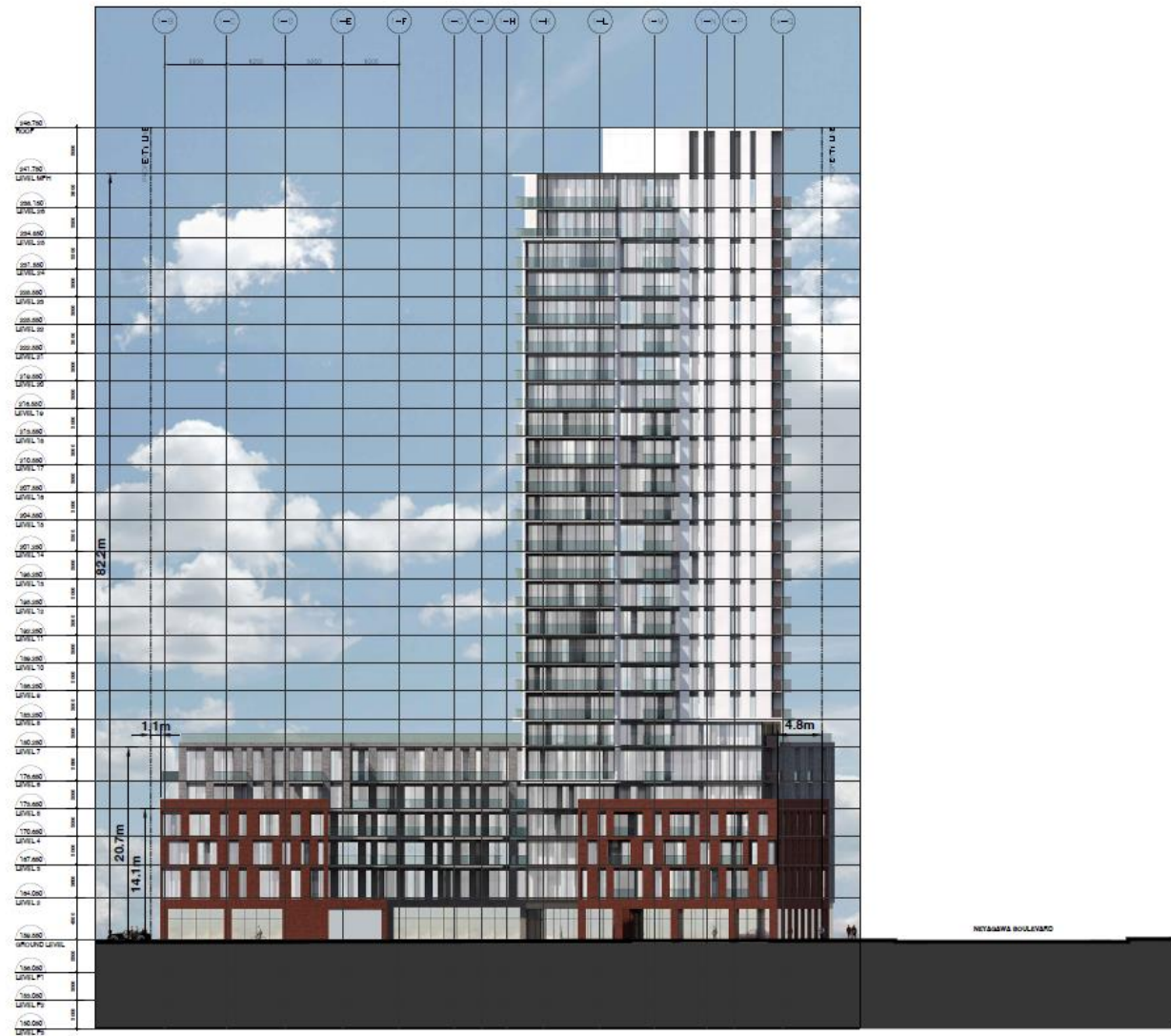




## APPENDIX B: Renderings and Elevations

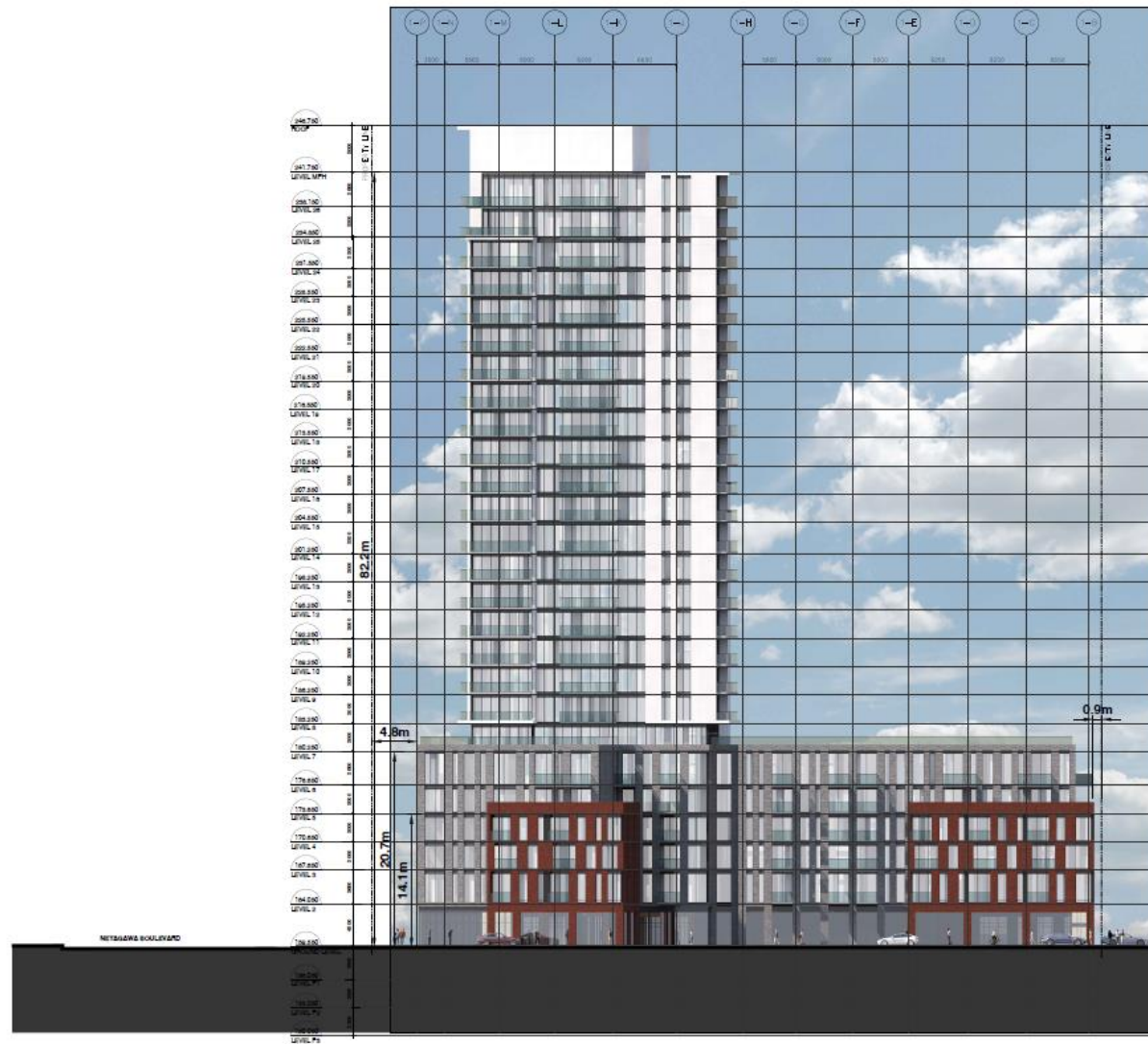






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<p>THIS DOCUMENT IS THE PROPERTY OF CORE ARCHITECTS. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. IT IS NOT TO BE REPRODUCED, COPIED, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF CORE ARCHITECTS.</p> <p>CONSTRUCTION NOT TO BE STARTED UNTIL ALL PERMITS ARE OBTAINED.</p>	
<p><b>CORE ARCHITECTS</b>          3036 NEYAGAWA BLVD          SUITE 200          CHICAGO, IL 60641          TEL: 773.331.1000          WWW.COREARCHITECTS.COM</p>	
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CHICAGO, IL	
neatt <sup>2</sup>	
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CHECKED BY: CORE	DATE: 01/10/2018
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Project No: 23-106	Drawing No: A400





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**COREARCHITECTS**  
 3056 NEVAGAWA BLVD  
 CHICAGO, IL 60640  
 TEL: 773.333.3333  
 WWW.COREARCHITECTS.COM

3056 NEVAGAWA BLVD  
 CHICAGO, IL 60640

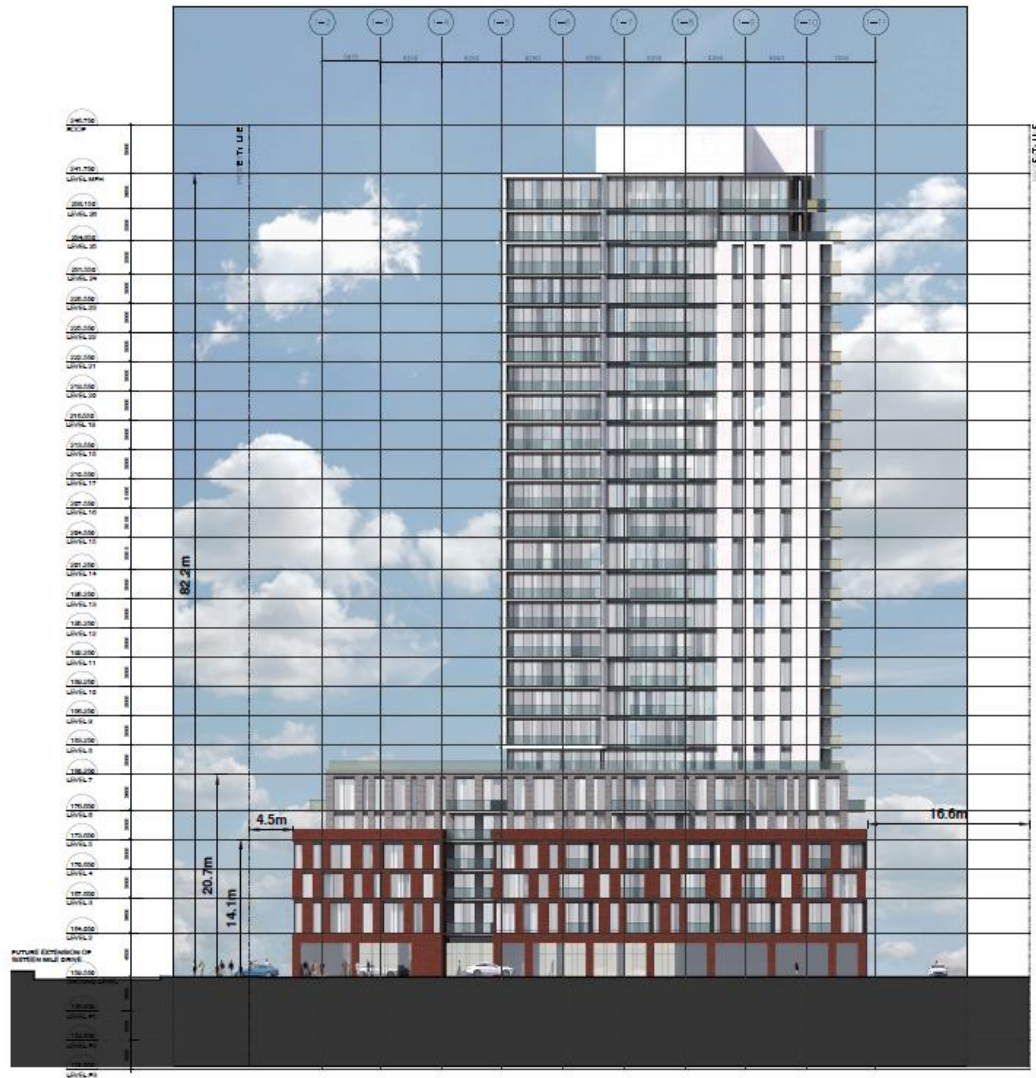
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BUILDING 1-NORTH ELEVATION  
 Project No. 25-108 Drawing No. A401





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DATE FOR THIS SHEET	07/27/2023
DESIGNED BY	NEATT
CHECKED BY	NEATT
DATE	07/27/2023

**CONCRETE ARCHITECTS**  
 3000 NE 10th Ave, Suite 100  
 Fort Lauderdale, FL 33304  
 (954) 561-1111  
 www.concretearchitects.com

3006 NEYAGAWA BLVD  
 (SHEET 20 OF 20)

**neatt**

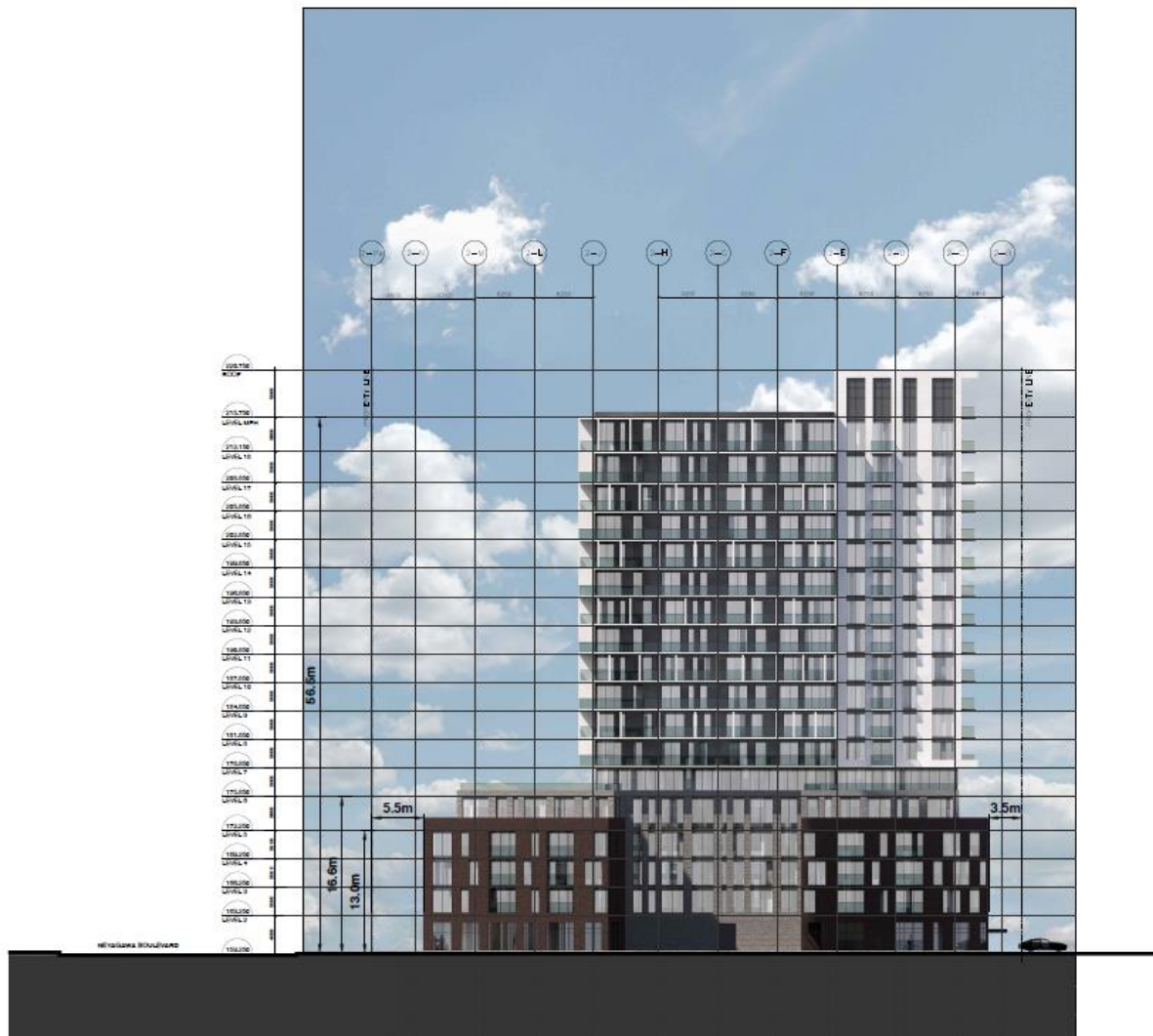


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 CHECKED BY: NEATT  
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PROJECT NO.: 23-109 SHEET NO.: A403







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NO.	REVISION FOR CHG. 03/01/2024	BY	REV. 004
NO.	REVISION FOR CHG. 03/01/2024	BY	REV. 005

**CONEARCHITECTS**  
 3000 NEVAGAWA BLVD, SUITE 100  
 OAKVILLE, ONTARIO L6H 6H6  
 TEL: 905.881.1111  
 WWW.CONEARCHITECTS.COM

3006 NEVAGAWA BLVD  
 OAKVILLE, ON

**neatt** 



DATE: 01/01/2024  
 DRAWN: T. J. J.  
 CHECKED: T. J. J.  
 DATE: 01/01/2024

BUILDING 2-NORTH ELEVATION

PROJECT: 23-108 DRAWING NO: A411







Project No. 23-108	Drawing No. A413
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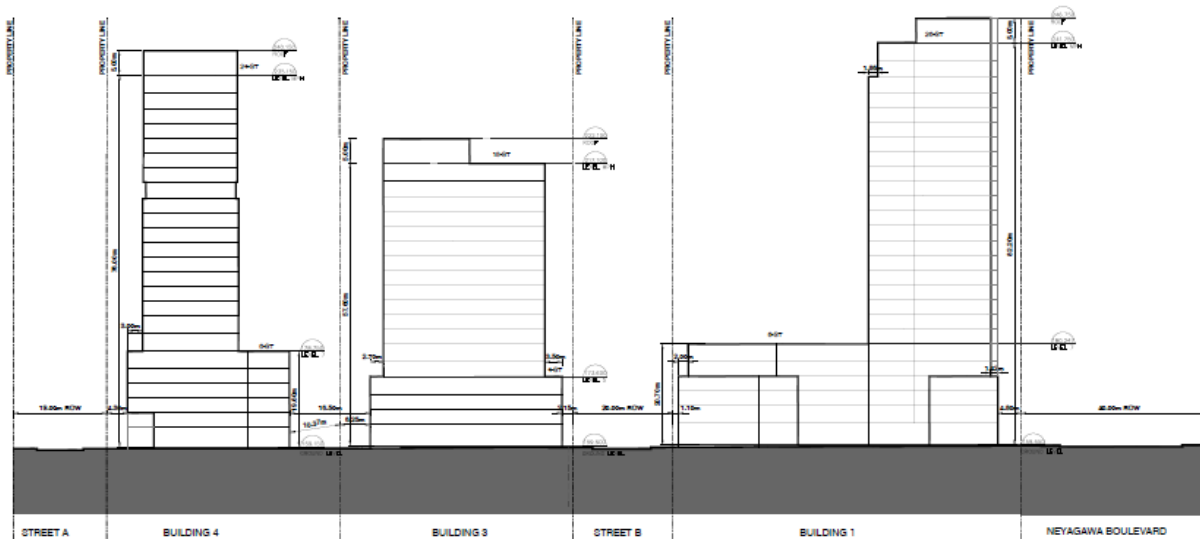












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**CONARCHITECTS**  
 3006 NEYAGAWA BLVD  
 SUITE 100  
 NEYAGAWA, NJ 07065  
 TEL: 201-261-1111  
 FAX: 201-261-1112  
 WWW.CONARCHITECTS.COM

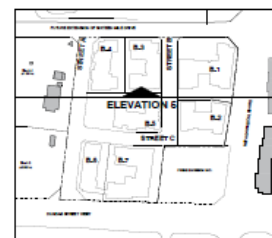
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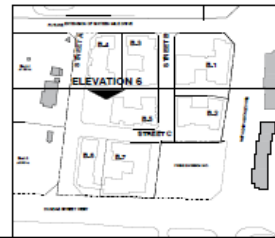
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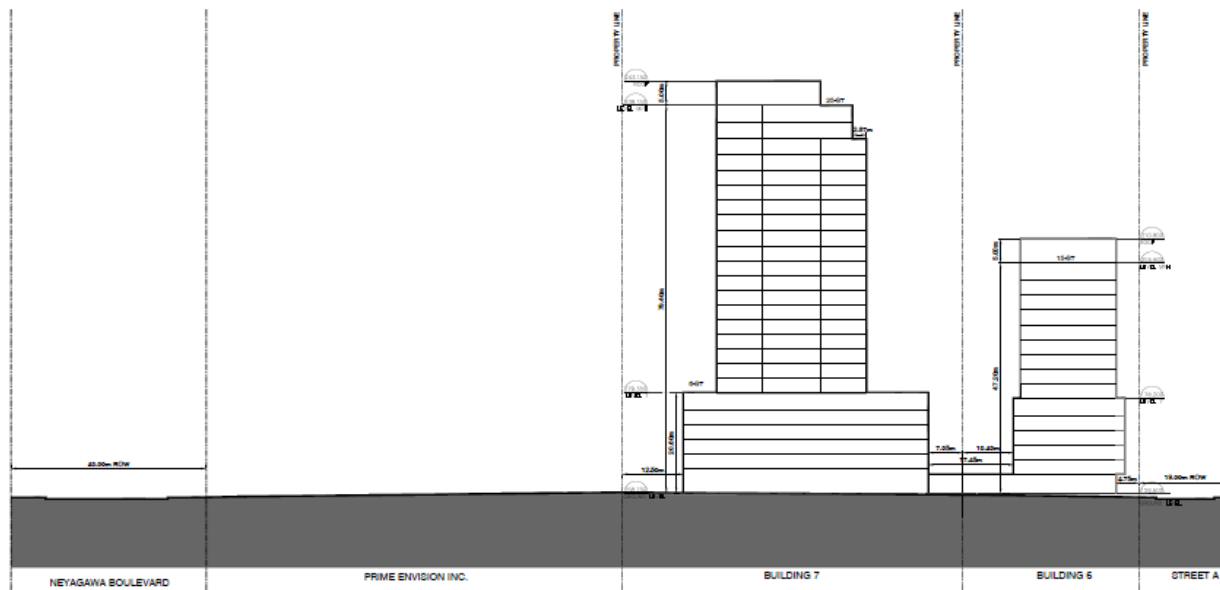
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 SHEET NO.: A474



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**CORARCHITECTS**  
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 SUITE 100  
 NEVAGAWA, ONTARIO  
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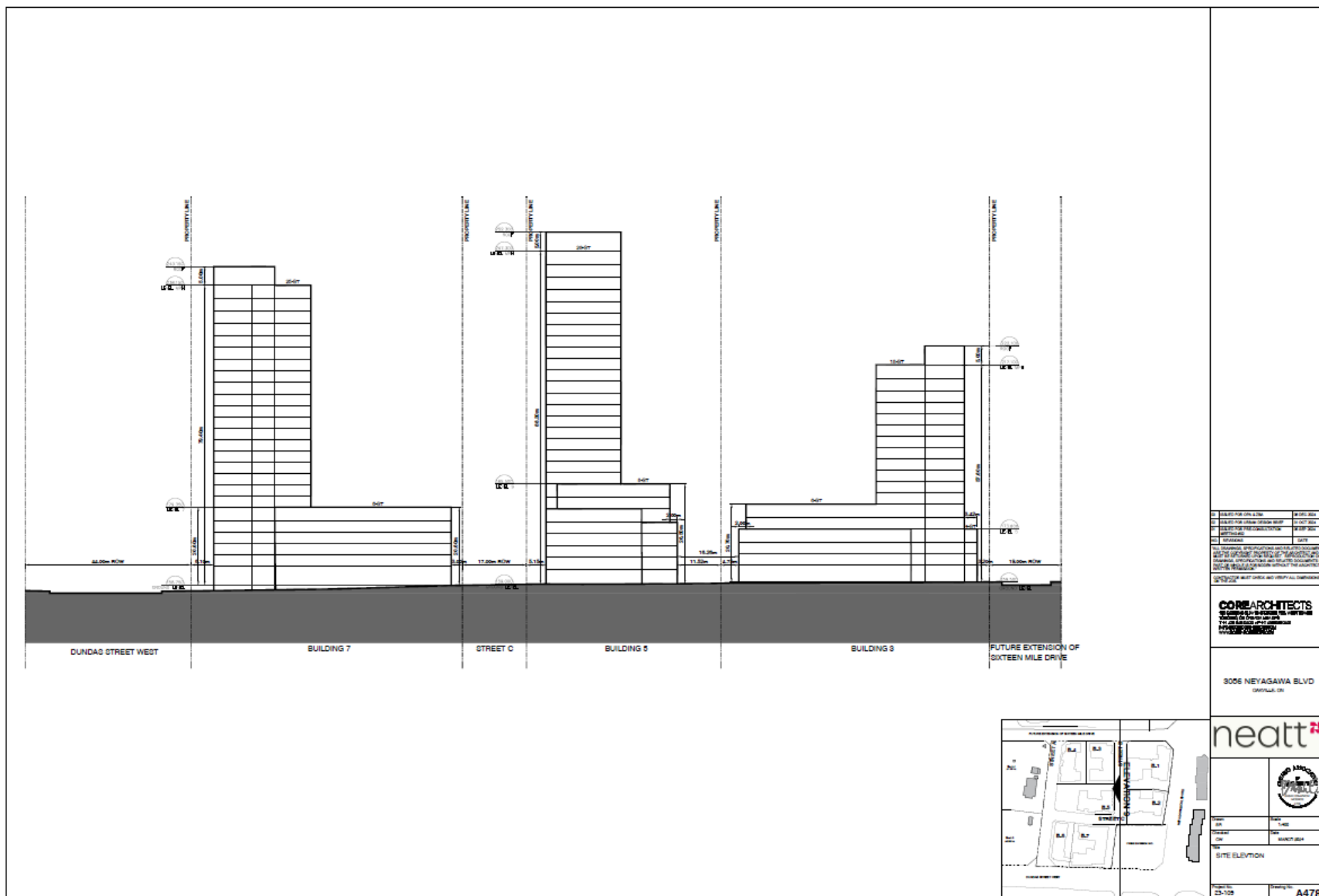
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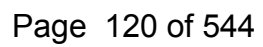


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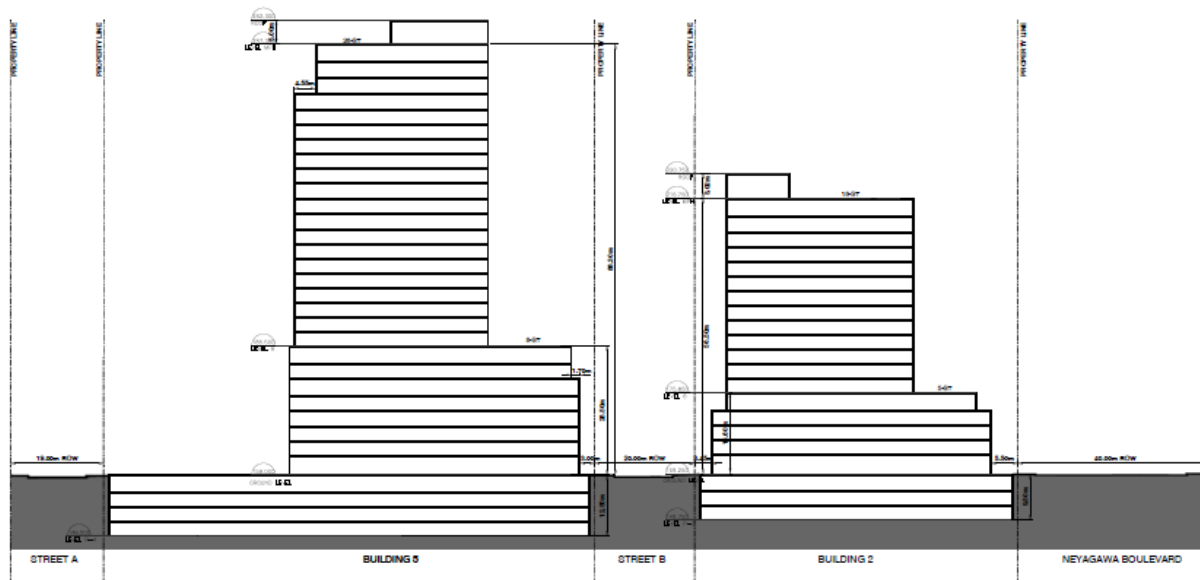












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9.0	SECTION FOR CITY & STATE	9.0	SECTION FOR CITY & STATE
10.0	SECTION FOR CITY & STATE	10.0	SECTION FOR CITY & STATE

**COREARCHITECTS**  
 3006 NEYAGAWA BLVD  
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 M3H 5K5  
 TEL: (416) 291-1111  
 WWW.COREARCHITECTS.COM

3006 NEYAGAWA BLVD  
 SUITE 100

neatt<sup>2</sup>



DATE: 11/11/2019  
 DRAWN BY: J. J. J. J. J.  
 CHECKED BY: J. J. J. J. J.  
 SCALE: 1/8" = 1'-0"

SITE ELEVATION

Project No: 23-109 Drawing No: A484



**THE CORPORATION OF THE TOWN OF OAKVILLE**

**BY-LAW NUMBER 20XX-XX**

**Official Plan Amendment XX**

A by-law to adopt an amendment to the North Oakville East Secondary Plan, Official Plan Amendment Number XX (Neatt Communities; 3056 Neyagawa Boulevard; File Z.XXXX.XX)

WHEREAS the North Oakville East Secondary Plan, which applies to the lands north of Dundas Street and south of Highway 407, was adopted by City Council as Official Plan Amendment No. 272 to the 2006 Town of Oakville Official Plan; and

WHEREAS subsection 22(1) of the Planning Act, R.S.O. 1990 c.P.13, as amended, permits a person or public body to request a council of a municipality to amend its Official Plan, and Section 17, 21 and 22 applies to any such amendment; and

WHEREAS it is deemed necessary to pass an amendment to the North Oakville East Secondary Plan to add a new site-specific policy applicable to the lands at 3056 Neyagawa Boulevard regarding maximum permitted building height, and to remove the Secondary School and Stormwater Management Facility symbols from the Site as shown on Appendix 7.3.

**COUNCIL ENACTS AS FOLLOWS**

1. The attached Amendment No. XX to the North Oakville East Secondary Plan, is hereby adopted.
2. Pursuant to subsection 17(27) of the Planning Act, R.S.O. 1990, c.P.13, as amended, this Official Plan Amendment comes into effect upon the day after the last day for filing a notice of appeal, if no appeal is filed pursuant to subsections 17(24) and (25). Where one or more appeals have been filed under subsection 17(24) and (25) of the said Act, as amended, this Official Plan Amendment comes into effect when all such appeals have been withdrawn or finally disposed of in accordance with the direction of the Ontario Land Tribunal.
3. PASSED this XX day of XX, 20XX.

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MAYOR

---

CLERK

# **Official Plan Amendment Number XX to the North Oakville East Secondary Plan**

## **Constitutional Statement**

The details of the Amendment, as contained in Part 2 of this text, constitute Amendment Number XX to the North Oakville East Secondary Plan.

## **Part 1 – Preamble**

### **1.0 Location:**

The lands affected by this Amendment are located west of Neyagawa Boulevard, north of Dundas Street West, known as Part of Lot 21, Concession 1, North of Dundas Street (municipally known as 3056 Neyagawa Boulevard).

### **2.0 Purpose & Effect:**

The purpose of this Amendment is to amend the text in the North Oakville East Secondary Plan to permit increased building heights for the property at 3056 Neyagawa which is within the Dundas Urban Core land use designation. Whereas Policy 7.6.5.3(b)(ii) states that a maximum building height of 8 storeys shall be permitted within the Dundas Urban Core, this Amendment would permit a maximum building height of 28 storeys. This Amendment also revises Appendix 7.3 (North Oakville Master Plan) to remove the Secondary School and Stormwater Management Facility symbols from the Site.

### **3.0 Background & Basis:**

- The subject land is 37,462 square metres (approximately 3.75 hectares) in size, with 125.67 metres of frontage along Dundas Street West and 126.26 metres of frontage along Neyagawa Boulevard. It is located within the Dundas Urban Core Area in the North Oakville East Secondary Plan and is adjacent to future Higher Order Transit along Dundas.
- Within the Town of Oakville Urban Structure (Livable Oakville Schedule A1) and North Oakville East Secondary Plan the subject land is designated Dundas Urban Core Area, which is intended to accommodate transit-supportive mixed-use development at high and medium densities. Schedule A1 also identifies Dundas Street as a Regional Transit Priority Corridor, and Neyagawa Boulevard as a Mobility Link.

- The owner of the subject land has requested relief from the applicable building height policy in the North Oakville East Secondary Plan to permit a high-density mixed-use development with tower heights of up to 28 storeys, containing approximately 1,550 square metres of commercial retail space and 2,278 residential units.
- The proposed development will optimize the use of the subject land and contribute to the achievement of the minimum density target of 160 combined residents and jobs per hectare for the Dundas Urban Core Area as directed in the Halton Regional Official Plan, Table 2B. (**Note:** As of July 1, 2024, the Halton Region Official Plan is deemed an official plan of each of the lower-tier municipalities in Halton, including the Town of Oakville, until it is revoked or amended by the Town).
- The proposed development will support the Town's planned urban structure, contributing to the achievement of a complete community by providing compact and high-quality multi-unit dwellings as well as additional locally-oriented commercial space. The subject land is immediately adjacent to the Town of Oakville's North Park community hub which accommodates significant parkland and recreational facilities, and is within a short walk of Lions Valley Park and the Sixteen Mile Creek public valleylands. A full range of retail and commercial amenities are also located in close proximity.
- The proposed development is feasible from a servicing and transportation perspective, and will not have adverse impacts on the surrounding area. From an urban design perspective, the subject land is well-buffered from surrounding residential uses by virtue of the Dundas and Neyagawa rights-of-way, and there are no adverse impacts anticipated with regard to shadowing, overlook, and loss of light or privacy. The proposal promotes the urban design objectives of the Town and will contribute to a vibrant and distinct sense of place with a high-quality public realm.
- Appendix 7.3 (North Oakville Master Plan) of the North Oakville East Secondary Plan includes symbols on the Site that signify a future Secondary School and Stormwater Management Facility (final location tbd). Halton District School Board has since abandoned its plans for a Secondary School in this location and is pursuing alternative lands in the North Oakville East area for this purpose. The proposed development does not include an on-site stormwater management facility but instead proposes upgrades to the existing downstream Riverside Way Pond to accommodate stormwater flows from the subject land.



## Part 2 – The Amendment

Item No.	Section	Description of Change
1.	7.6.18.X	Section 7.6.18 – Land Use Strategy – Exceptions – is amended by adding a new exception policy 7.6.18.X:  <b>On the lands located at 3056 Neyagawa Boulevard, a maximum building height of 28 storeys shall be permitted.</b>
2.	Appendix 7.3	Appendix 7.3 is updated to remove the Secondary School and Stormwater Management Facility symbols from the subject land, as indicated in the map below.



## THE CORPORATION OF THE TOWN OF OAKVILLE

### BY-LAW NUMBER 20XX-XX

A by-law to amend the North Oakville Zoning By-law 2009-189, as amended, to permit the use of lands described as Part of Lot 21, Concession 1, NDS (Neatt Communities; 3056 Neyagawa Boulevard; File Z.XXXX.XX)

#### COUNCIL ENACTS AS FOLLOWS:

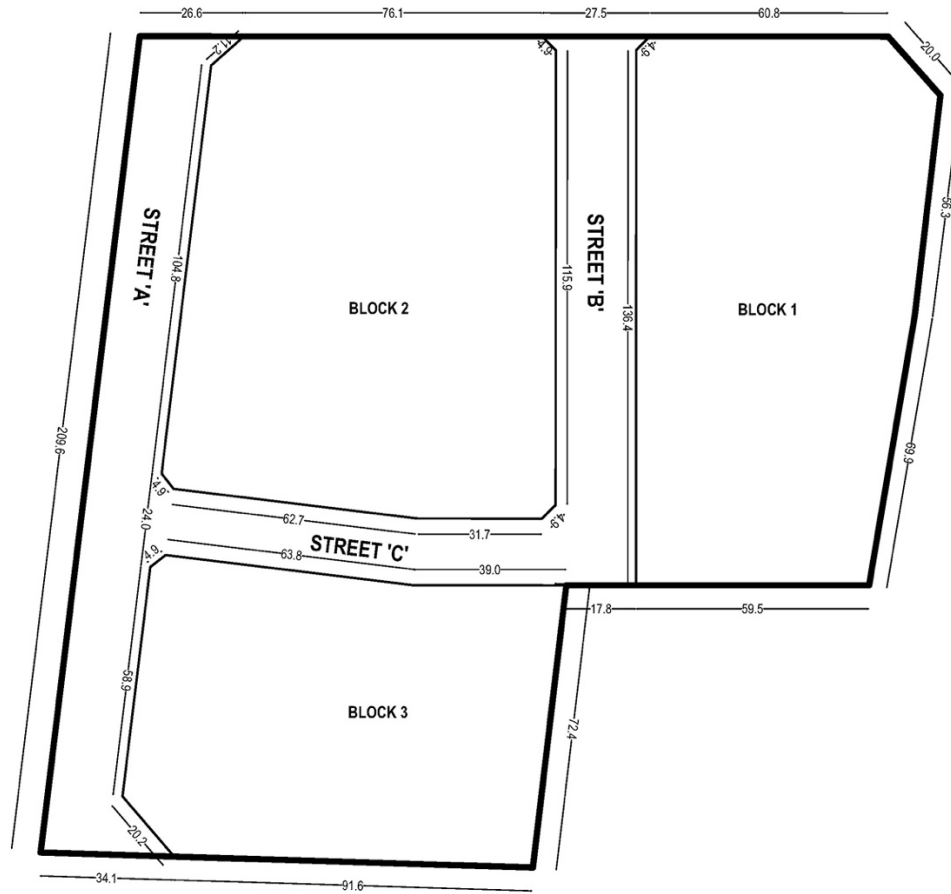
1. Map 12(3) of By-law 2009-189, as amended, is further amended by rezoning the lands depicted on Schedule 'A' to this By-law.
2. Part 8, Special Provisions, of By-law 2009-189, as amended, is further amended by adding a new Section 8.XX as follows:

XX		3065 Neyagawa Boulevard (Part of Lot 21, Concession 1, NDS)	Parent Zone: DUC
Map 12(3)			(BY-LAW #)
8.XX.1 – Zone Regulations for All Lands			
a)	Minimum and maximum <i>floor space index</i> shall not apply.		
b)	Maximum <i>front yard</i> and <i>flankage yard</i> shall not apply.		
c)	Minimum <i>height</i> of the <i>first storey</i> for an <i>apartment building</i> or <i>mixed use building</i> .	4.0 m	
d)	Notwithstanding any severance, partition or division of the lands subject to this Special Provision, the regulations of each Block identified in Figure 8.XX.6 shall apply to the whole of such lands as if no severance, partition or division had occurred.		
8.XX.2 – Additional Zone Regulations for Block 1			
The following additional regulations apply to all lands identified as Block 1 identified in Figure 8.XX.1			
a)	Minimum building height	5 storeys	
b)	Maximum building height	26 storeys	
c)	Maximum <i>floor space index</i>	4.8	
d)	Minimum non-residential <i>floor area</i>	1,000 square metres	
e)	Minimum north building <i>setback</i>	7.0 metres	
f)	Minimum east building <i>setback</i>	4.5 metres	
g)	Minimum south building <i>setback</i>	12.5 metres	
h)	Minimum west building <i>setback</i>	1.0 metres	
8.XX.3 – Additional Zone Regulations for Block 2			
The following additional regulations apply to all lands identified as Block 2 identified in Figure 8.XX.1			
a)	Minimum building height	6 storeys	
b)	Maximum building height	28 storeys	

c)	Maximum <i>floor space index</i>	6.2
d)	Minimum non-residential <i>floor area</i>	400 square metres
e)	Minimum north building <i>setback</i>	3.0 metres
f)	Minimum east building <i>setback</i>	2.0 metres
g)	Minimum south building <i>setback</i>	3.5 metres
h)	Minimum west building <i>setback</i>	2.0 metres
<b>8.XX.4 – Additional Zone Regulations for Block 3</b>		
The following additional regulations apply to all lands identified as Block 3 identified in Figure 8.XX.1		
a)	Minimum building height	6 storeys
b)	Maximum building height	25 storeys
c)	Maximum <i>floor space index</i>	5.7
d)	Minimum north building <i>setback</i>	3.0 metres
e)	Minimum east building <i>setback</i>	12.5 metres
f)	Minimum south building <i>setback</i>	3.0 metres
g)	Minimum west building <i>setback</i>	3.0 metres
<b>8.XX.5 – Parking Regulations</b>		
a)	Despite Table 5.1B – Parking Requirements for Non-Residential Uses, <i>parking spaces</i> for all non-residential uses shall be provided at a minimum rate of 1 <i>parking spaces</i> per 30 square metres of <i>leasable floor area</i> and at a maximum rate of 1 <i>parking space</i> per 20 square metres of <i>leasable floor area</i> .	
b)	Required <i>parking spaces</i> for any <i>use</i> may be located on the <i>lot</i> on which the <i>use</i> is located, and/or on one half of the <i>street</i> abutting the <i>lot</i> where the on-street parking is permitted.	
c)	Designated residential visitor <i>parking spaces</i> may be counted toward non-residential <i>parking spaces</i> and may be provided in any combination.	

## 8.XX.6 – Special Site Figures for All Lands

Figure 8.XX.1

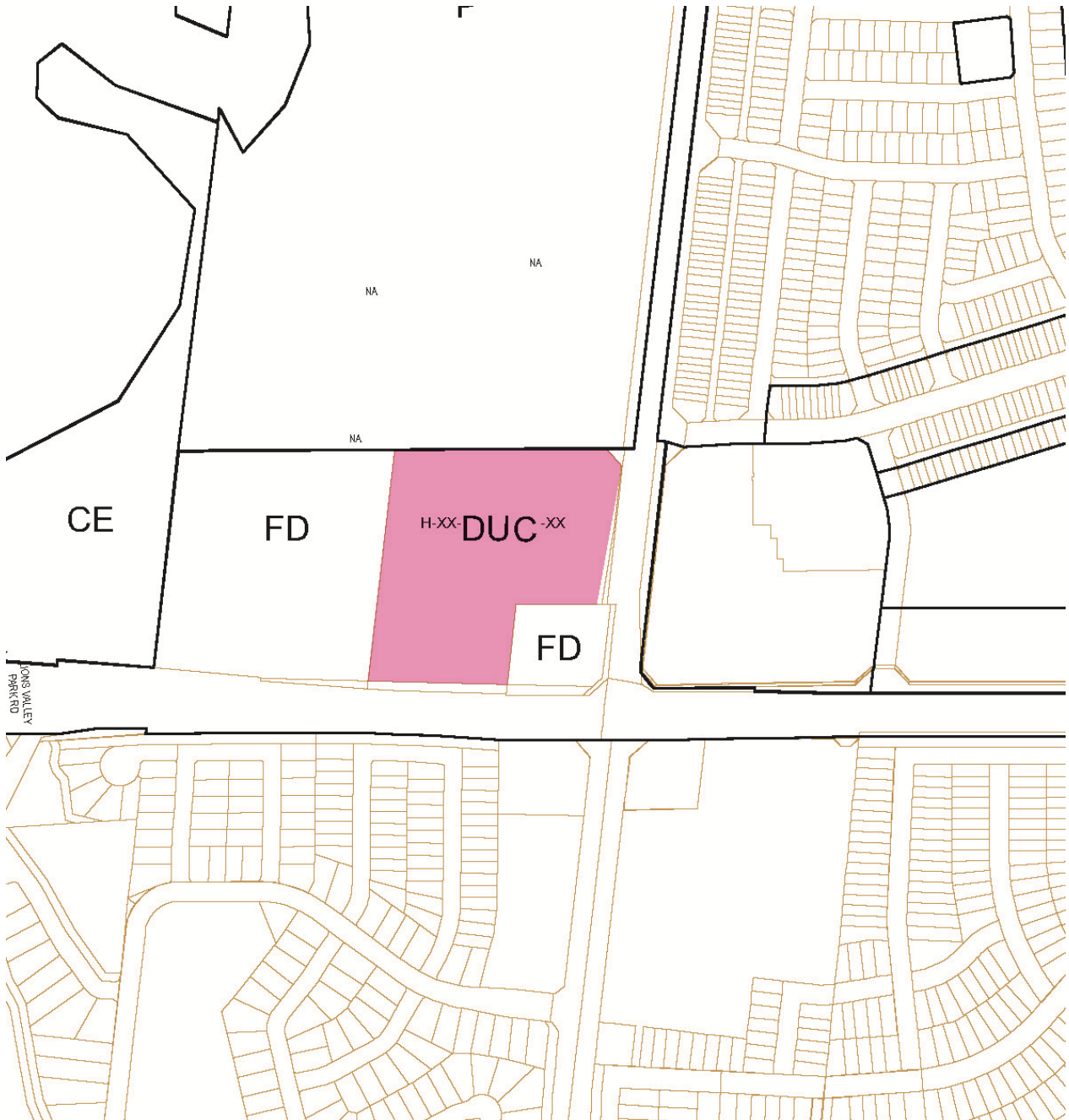


3. Section 9, Holding Provisions, of By-law 2009-189 is amended by adding a new Section 9.3.XX as follows:

HXX	3065 Neyagawa Boulevard (Part of Lot 21, Concession 1, NDS)	Parent Zone: DUC
Map 12(3)		(BY-LAW #)
<b>9.3.XX.1 Conditions for Removal of the “H”</b>		
The “H” symbol shall, upon application by the Owner, be removed for all or part of a block by <i>Town Council</i> passing a By-law under Section 36 of the <u>Planning Act</u> . The following condition(s) shall first be completed to the satisfaction of the <i>Town of Oakville</i> :		
a)	<p>Prior to any development proceeding on Block 2, as identified in Figure 8.XX.1, the Owner shall provide written confirmation from the Region of Halton that:</p> <ul style="list-style-type: none"> <li>i. The Owner has secured the appropriate amount of water and wastewater Servicing Allocation under the Region of Halton Allocation Program;</li> <li>ii. The Owner has signed the applicable Allocation Agreement or any required Amending Agreements;</li> <li>iii. The Owner has made all required payments associated with the Allocation Program; and</li> <li>iv. The Owner is in receipt of the Region of Halton Public Works Commissioner’s Notice (PWCN) letter.</li> </ul>	
b)	<p>Prior to any development proceeding on Block 3, as identified in Figure 8.XX.1, the Owner shall provide written confirmation from the Region of Halton that:</p> <ul style="list-style-type: none"> <li>i. The Owner has secured the appropriate amount of water and wastewater Servicing Allocation under the Region of Halton Allocation Program;</li> <li>ii. The Owner has signed the applicable Allocation Agreement or any required Amending Agreements;</li> <li>iii. The Owner has made all required payments associated with the Allocation Program; and</li> <li>iv. The Owner is in receipt of the Region of Halton Public Works Commissioner’s Notice (PWCN) letter.</li> </ul>	



## SCHEDULE A TO BY-LAW NUMBER 20XX-XX



North Oakville Secondary Plan

**7.3 COMMUNITY STRUCTURE**

**7.3.1 PURPOSE**

Figure NOE1, the Community Structure Plan, establishes a range of components which define the general arrangement of land use and activity for the Planning Area.

Each component of the Community Structure Plan has its own function which is described in the following sections, together with the ultimate population, housing and employment targets.

**7.3.2 URBAN CORE**

The Urban Core designations reflect the most urban part of the North Oakville East Plan Area. These areas provide for the densest development and the highest order activities including a full range of residential, retail and service commercial, entertainment, cultural, business and institutional uses. Mixed use development is encouraged. Ultimately it is intended that Urban Core lands will become true mixed use urban areas. The primary focus of this development is along Trafalgar Road, with the north side of Dundas St. and the intersection of Neyagawa Boulevard and Burnhamthorpe Road having an important, but more secondary role.

**7.4 SUSTAINABLE DEVELOPMENT STRATEGY**

**7.4.1 PURPOSE**

In keeping with the objectives of this Secondary Plan, the Town of Oakville is committed to the principle of sustainable development<sup>2</sup> in the development of North Oakville East.

The Sustainable Development Strategy provides policies with respect to the implementation of this principle as it relates to development form, and specifically to the protection, conservation and enhancement of air, water and ecological features and functions, energy and other resources, and heritage resources.

**7.4.2 DEVELOPMENT FORM**

The North Oakville East Secondary Plan has been based on a conceptual design which maximizes the potential for sustainable development through such features as mixed use development, a modified grid road system which enhances the opportunity to provide transit, and a Natural Heritage and Open Space System.

In addition to the general direction implicit in the Plan, the Town will actively encourage development which is specifically based on the principle of sustainable development, including the development of Town facilities. The Town will also work with other public agencies to encourage them to follow these principles. Such development will be designed to:

- a) reduce the consumption of energy, land and other non-renewable resources;
- b) minimize the waste of materials, water and other limited resources;
- c) create livable, healthy and productive environments; and,
- d) reduce greenhouse gases.

#### **7.4.3 AIR/ENERGY EFFICIENCY**

The Town recognizes that air quality in North Oakville East will be significantly influenced by air pollutant emissions from outside the area. However, the Town will work to improve air quality and energy efficiency in North Oakville East:

- a) by directing through the Secondary Plan policies, and their implementation, through the evaluation of development applications, and through the initiation of municipal programs and works, consideration of the following land use and transportation strategies to minimize the amount of vehicular travel and emissions:

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<sup>2</sup> The Bruntland Commission (1983) defined sustainable development as “development that meets the needs of the present without compromising the ability of future generations to meet their own needs.”

- i) concentrate activity centres such as places of worship, recreation centres and schools;
  - ii) encourage mixed-use development;
  - iii) encourage increased density near transit stops/stations and along transit routes commensurate with the type and frequency of transit service planned for that area and/or corridor;
  - iv) provide pedestrian and bicycle facilities;
  - v) ensure an interconnected street network;
  - vi) develop a strategy for the provision of public parking facilities at key locations; and,
  - vii) provide convenient and efficient transit service.
- b) by directing where the Town has jurisdiction, including through its engineering standards, and encouraging where it does not, measures to reduce the “urban heat island effect” including installation of reflective (or high albedo) roofs, installation of green roofs, increasing the reflective nature of pavement and planting of shade trees; and,
- c) by encouraging at the development application stage, the reduction of energy and residential combustion emissions through a range of approaches including the development of R-2000 homes and similar commercial construction standards such as LEED or other published standards, the incorporation of Energy Star appliances, the physical layout of the plan having regard to energy conservation, and buildings powered by renewable energy sources, both passive and active.

## **7.5 COMMUNITY DESIGN STRATEGY**

### **7.5.1 PURPOSE**

This section outlines general design policies for North Oakville East, as well as specific policies for the Neighbourhoods as designated on Schedule NOE1, and the Trafalgar Urban Core Area, Neyagawa Urban Core Area, the Dundas St. Urban Core Area, Employment Districts, and existing development.

### **7.5.2 MASTER PLAN**

- a) The North Oakville East Master Plan in Appendix 7.3 to the Official Plan is intended to illustrate graphically the design of the North Oakville East Planning Area and how the policies and Figures of the North Oakville East Secondary Plan are to be implemented. The spacing, function and design of intersections of Local Roads with Major Arterial/Transit Corridors (i.e. Regional arterials) shown on Appendix 7.3 have not been approved by the Region, and such intersections shown on Appendix 7.3 and on any subsequent area design plan, plan of subdivision, or other development plan, are subject to Regional approval.
- b) Prior to the commencement of the development of any:
  - i) neighbourhood in accordance with the neighbourhood boundaries established on Figure NOE1 and, where applicable, the portion of the Dundas Urban Core that abuts the neighbourhood;
  - ii) sub-area within the Trafalgar Urban Core Area identified on Figure NOE1, except that lands in any sub-area on one side of Trafalgar Road may proceed independently of the lands in that sub-area on the other side of Trafalgar Road;
  - iii) part of the Neyagawa Urban Core Area; or
  - iv) sub-area within the Employment Area or the Transitional Area identified by the Town, in consultation with all affected landowners, based on boundaries created by Arterial, Avenue or Connector roads or natural features, provided that a sub-area may include adjacent Employment and Transitional Areas.

The Town shall determine, after consultation with all affected landowners in the specific area, whether proposed plans of subdivision or other development plans for the affected lands are generally consistent with the Master Plan in Appendix 7.3. Where such plans are determined to be generally consistent with the Master Plan, development may be permitted to proceed without the preparation of an area design plan.

- c) The Town shall require the preparation of an area design plan to the satisfaction of the Town, prior to draft plan approval or approval of other development plans in a specific area identified in subsection b), where:



- i) proposed plans of subdivision or other development plans for the area are not generally consistent with the Master Plan in Appendix 7.3;
  - ii) the Town after consultation with all affected landowners, determines that an area design plan is required to address coordination issues between landowner plans;
  - iii) the Town, after consultation with all affected landowners, determines that an area design plan is required to address coordination issues between areas identified in 7.5.2 b) i) to iv); or
  - iv) any significant development is proposed in the Transitional Area designation.
- d) The area to be addressed by the area design plan will include, as applicable, one or more of the areas identified in subsection b).
- e) The area design plan will be designed to demonstrate conformity with the policies and Figures of the Secondary Plan and will provide details including:
  - i) the size and location of schools, neighbourhood parks, village squares and urban squares;
  - ii) the location, size and general configuration of stormwater management ponds;
  - iii) the detailed road pattern;
  - iv) the specific boundaries of neighbourhood land use categories and other designations;
  - v) the density and distribution of housing types;
  - vi) how the proposal addresses the Town's Implementation Strategy;
  - vii) the location, alignment and boundaries of Medium Constraint Streams;
  - viii) co-ordination with land uses and road patterns for lands outside, but adjacent to the lands which are the subject of the area design plan; and
  - ix) the requirements for Transitional Areas as set out in Section 7.6.9.3 a).
- f) Such area design plans shall be prepared in accordance with terms of reference approved by the Town and the applicant(s), by a consultant approved by the Town and the applicant(s), and retained by, and at the cost of, the applicant(s).
- g) In the preparation of the terms of reference, the Development Review provisions of Section 7.8 of this Plan will be used as a guide.
- h) A proposed plan of subdivision or other development plan shall be considered to

be inconsistent with the Master Plan, as referenced in subsection c) i), if it does not conform to the General Design Directions in Section 7.5.4, or results in coordination issues between areas identified in subsection b. The following will generally not be considered to be inconsistent with the Master Plan, provided that the Town determines that the plan of subdivision or other development plan conforms with the policies and Figures of the Secondary Plan:

- i) modifications to, or relocations of portions of the road pattern;
  - ii) modifications to Medium or Low Constraint Stream Corridors or Hydrologic Features "A" and "B";
  - iii) relocation of public facilities including parks, schools and stormwater ponds; or
  - iv) reconfiguration of the neighbourhood land use categories generally in accordance with the neighbourhood land use category requirements of Table 1. However, any reconfiguration of neighbourhood land use categories proposed by a plan of subdivision or other development plan that necessitates significant changes to the distribution of neighbourhood land use categories on other land ownerships, in order to maintain general conformity with Table 1, will require the preparation of an area design plan.
- i) The approval of an area design plan by the Town shall not require an amendment to this Plan.

### **7.5.3 DESIGN GUIDELINES**

Urban Design and Open Space Guidelines, which will include guidelines with respect to matters set out in Section 7.5 as well as heritage resources, will be prepared by the Town as part of the North Oakville East Implementation Strategy prior to the approval of any development applications. All development applications will be evaluated by the Town to ensure that they are consistent with the Urban Design and Open Space Guidelines.

### **7.5.4 GENERAL DESIGN DIRECTIONS**

- a) All development, particularly in the Urban Core Areas, Neighbourhood Centre and General Urban Areas, shall be designed to be compact, pedestrian and transit friendly in form. Mixed use development will be encouraged.
- b) A hierarchy of civic, institutional and commercial uses shall be established in locations which form part of mixed use areas (rather than isolated in single-use complexes) which allow them to serve as focal points for the Planning Area as a whole, neighbourhoods and sub-neighbourhoods. In particular, the ordinary activities of daily living shall occur within walking distance of most dwelling units.
- c) Development shall be based on a modified grid road system with interconnected networks of roads designed to disperse and reduce the length of vehicular trips



and support the early integration and sustained viability of transit service. For local roads not shown on Figure NOE4, the modified grid road system will respond to topography and the Natural Heritage System component of the Natural Heritage and Open Space System. Cul-de-sacs will generally be permitted only when warranted by natural site conditions

- d) The Natural Heritage component of the Natural Heritage and Open Space System forms a central feature of the Planning Area and the development form should reflect this fact. In addition, an associated comprehensive, interconnected system of trails will be developed which will generally reflect the major trail system on Figure NOE4.
- e) Public safety, views and accessibility, both physically and visually, to the Natural Heritage component of the Natural Heritage and Open Space System, as well as to parks, schools and other natural and civic features, will be important consideration in community design. This will be accomplished through a range of different approaches including, but not limited to, the use of single loaded roads, crescent roads, combining public open space with other public or institutional facilities (e.g. school/park campuses, easements, stormwater ponds adjacent to the Natural Heritage component of the System) and the location of high density residential and employment buildings. With respect to the Natural Heritage component of the Natural Heritage and Open Space System, priority will be given to maintaining views and accessibility at key trail access points where Arterial, Avenue and Connector roads are adjacent to the System. Where there is no significant Natural Heritage edge exposed at Arterial, Avenue or Connector roads, Neighbourhood Parks, Village Squares or local roads will be encouraged to provide access and visibility.

With respect to other public facilities particularly schools and parks, of the total linear perimeter distance around such uses in the order of 50% of the perimeter will be bounded by a combination of roads and open space which will allow public access or, at a minimum private open space which will allow significant views of the feature or facility. Where only road frontage is provided the frontage shall be in the order of 40% of the perimeter of the features and facilities.

- f) Parks, neighbourhood activity nodes, and other civic areas will serve as central "meeting places" for residents, particularly within neighbourhoods and sub-neighbourhoods
- g) Building densities and land uses designed to support the use of transit and the level of transit service proposed for specific areas shall be located within walking distances of transit stops and lines.
- h) The incorporation of cultural heritage resources into the community, including their use and adaptive reuse, shall be encouraged.
- i) The location of *active transportation* facilities should conform with Schedule D, Active Transportation Plan, to the Livable Oakville Plan, recognizing that the alignments are diagrammatic. An official plan amendment will not be required for changes to the alignments of *active transportation* facilities provided that the general intent and purpose of Schedule D to the Livable Oakville Plan is

maintained. In the event of a conflict between Schedules D to the Livable Oakville Plan and the North Oakville East Secondary Plan, the North Oakville East Secondary Plan shall prevail.

#### **7.5.5 STREETScape**

The streetscape consists of streets and public frontages. The streets provide the traffic and parking capacity. The public frontages provide for pedestrian circulation and contribute to the character of the streetscape. The design of the streetscape is defined by the type of sidewalk, curbing, planters and street trees. The policies of Section 7.7.2 of this Plan provide detailed direction with respect to the design of typical streets, while the Urban Design and Open Space Guidelines will address directions for public frontages and other design treatments.

The Urban Design and Open Space Guidelines will establish five types of streets with respect to typical design treatment. Variations may be considered by the Town based on circumstances such as topography, proposed abutting land use, relationship to the Natural Heritage and Open Space System and achievement of other design objectives:

##### **7.5.5.1 Arterial/Transit Corridors**

Arterial/Transit Corridors as designated on Figure NOE4 are high capacity roads which serve as major gateways into the community, including both the Town as a whole, and North Oakville East. A balance must be achieved between their transportation function, including accommodation for transit, and their ability to provide access to flanking land uses, and to act as socially vibrant public space. To assist in achieving this balance, these streets will have the highest form of design treatment, including wide sidewalks, special tree and feature planting, paving, lighting and signage design.

##### **7.5.5.2 Character Roads**

The Character Road designation on Figure NOE4 is applicable to portions of existing Burnhamthorpe Road, which will be designed in accordance with the policies of Section 7.7.2, except for the section identified to serve an arterial roadway function as part of the New North Oakville Transportation Corridor.

##### **7.5.5.3 Avenue and Connector/Transit Corridors**

Avenue/Transit Corridors and Connector/Transit Corridors as designated on Figure NOE4 connect neighbourhoods together and to Urban Core Areas and other major focal points of the community. These roads will have a higher level of design than the Local Streets through the extended use of tree and feature planting, paving, lighting and signage design. The design will complement the planned abutting land uses. For example, where these streets provide access to street related retail and mixed use development, in the Urban Cores and Neighbourhood Centres, their design shall include on-street parking, wider sidewalks, and street furniture such as benches.

##### **7.5.5.4 Local Streets**

Local streets play a dual role as neighbourhood socialization spaces, as well as supporting transportation needs. The design requirements, while less substantial than for connector roads, must support the dual role of the local streets.

#### **7.5.5.6 Pedestrian/Cyclist Orientation**

Pedestrian/cyclist comfort and safety shall be considered in the streetscape design for roads under the control of the Town of Oakville. In commercial and mixed use areas, the design will provide for an enhanced streetscape and sidewalk environment for pedestrians. The Town will also work with the Region to encourage a similar approach.

#### **7.5.5.9 Sidewalks**

- a) Sidewalks shall generally be provided on both sides of all streets with the exception of:
  - i) residential streets with less than ten dwelling units or cul-de-sacs, where sidewalks shall be required on only one side of the street;
  - ii) lanes, where no sidewalks shall be required;
  - iii) character roads, where a rural cross-section is being maintained, where sidewalks may not be required, provided that pedestrian and bicycle circulation is accommodated on a separate trail system; and,
  - iv) a road flanking the Natural Heritage and Open Space System, where a sidewalk shall be provided on the developed side only, subject to the availability of a trail facility on the other side of the street.
- b) In addition to the exceptions above, consideration may be given to permitting only one sidewalk for some Local Roads. Such roads would be permitted only where the Town is satisfied through the submission of a pedestrian circulation plan that only one sidewalk is necessary and provided that the road would:
  - i) have a maximum ROW of 16 metres; and,
  - ii) be located in the Neighbourhood Area designation in a Sub-urban land use category
  - iii) not provide direct access to a school, Neighbourhood Park, or Village Square.

The geographical extent of the pedestrian circulation plan shall be broad enough to demonstrate how the above conditions are met as well as the relationship to the transit system, community facilities and shopping areas.

#### **7.5.5.10 Cyclists**

Provision for cyclists to travel either on the roadway or on bicycle paths separated from the roadway, which may or may not include a designated lane, shall be recognized in the design of all streets. In addition, bicycle parking standards for other than freehold ground related housing, will be prepared and implemented through the zoning by-law.



#### **7.5.5.11 Bicycle/Pedestrian Trail System**

An extensive system of recreational trails will be developed related to the Natural Heritage and Open Space System as well as along certain public road rights of way. A conceptual major trail system which will form a basis for the development of this more extensive system is identified on Figure NOE4. However, any proposed trail development within the Natural Heritage and Open Space System shall be subject to further study as part of the Implementation Strategy to the satisfaction of the Town, in consultation with the Region of Halton and Conservation Halton. The system may be refined through the preparation of an Environmental Implementation Report in accordance with the provisions of Section 7.8.3 a) of this Plan.

#### **7.5.5.12 Lighting**

Lighting shall provide suitable illumination for vehicles, pedestrians and cyclists.

#### **7.5.5.13 Street Furniture**

Street furniture such as lighting, signage, parking meters, cycle parking facilities, newspaper boxes, utilities, and garbage facilities shall be designed and placed within a consistent and integrated system of form, pattern, shape, colour, and texture to avoid clutter and facilitate readability. Utilities will be grouped or clustered wherever possible.

#### **7.5.5.14 Block Size**

The length of the block makes a significant difference in creating a pedestrian-friendly environment. Blocks should be short and regular in length to make walking efficient and allow for variation in routes. Where it is impossible or undesirable to provide short blocks, wide public mid-block pathways should be provided to shorten walking distances.

### **7.5.6 BUILDING LOCATION**

- a) Buildings, structures and landscaping shall be designed to provide visual interest to pedestrians, as well as a "sense of enclosure" to the street. Generally, heights of buildings shall also be related to road widths to create a more comfortable pedestrian environment, so that the wider the road width, the higher the building height.
- b) Buildings on corner lots at the intersections of Arterials, Avenues and Connector streets shall be sited and massed toward the intersection.
- c) The rear and side building elevations of all buildings on corner lots shall be designed to take advantage of their extra visibility.
- d) In residential areas, garages shall be designed so that they are not the dominant feature in the streetscape. In particular, attached garages shall not:

- i) project beyond the façade of the dwelling or the façade (front face) of any porch; or
  - ii) contain garage doors that occupy more than 50% of the frontage of a lot unless the Town is satisfied through the submission of detailed plans by the applicant that the garage doors can be appropriately integrated with the streetscape.
- e) Higher-density housing shall be located close to Arterial Roads, Avenues, Connectors and transit stops, within Neighbourhood Centres and the Urban Core Areas.

#### **7.5.7 PARKING AREAS**

##### **7.5.7.1 Off-Street Parking**

Off-street parking areas shall be designed to reduce their visual impact on both the adjoining streetscape and on people using the facility by:

- a) Screening of the parking lot at the street and adjacent to residential development through the use of features such as low fences, walls and landscaping and in a manner which reflects the safe community design policies of Section 7.5.10;
- b) Locating the parking lot, within commercial or mixed use developments, to the side or rear of the main building and permitting no or only minimal parking in front of the main building. However, in sub-area 2 of the Trafalgar Urban Core, within the Dundas Urban Core Area at Neyagawa and within the Neyagawa Urban Core, where large retail stores are proposed, design alternatives designed to contribute to the creation of a vibrant and active streetscape, may include, among other possible alternatives, locating large retail stores in the interior or at the rear of commercial or mixed use development blocks with smaller stores and building oriented to surrounding Arterial, Avenue or Connector Roads to create a strong street presence. Alternatively, the frontage of the large retail store facing the Arterial, Avenue or Connector Road should be lined with smaller stores or multiple building entrances. Parking areas will be integrated with development associated with large retail stores, in a manner designed to contribute to the objective of a vibrant and active streetscape.
- c) The use of landscaping or decorative paving to reduce the visual expanse of large parking areas;
- d) Joint access to parking lots on adjoining properties where feasible; and,
- e) Provision of pedestrian walkways adjacent to stores, between building clusters, and to provide pedestrian access to transit stops, public sidewalks and other developments. These walkways may need to cross parking lots to provide the required access.

#### **7.5.7.2 Parking Options**

In addition, options to replace at-grade parking areas will be encouraged including:

- a) On-street parking  
On-street parking should be permitted wherever possible to increase animation, reduce vehicle speeds and serve as a protective buffer between pedestrians and moving vehicles. To encourage the provision of such parking, appropriate engineering design standards for roadways, including laybys, shall be developed and reduced off-street parking requirements will be established for specific areas where appropriate, particularly along transit routes and in the Urban Core designations.
- b) Municipal parking  
The Town shall work with landowners in mixed use areas to establish municipal parking at strategic nodes.

#### **7.5.9 LANDSCAPE DESIGN**

The applicable policies of Part C, Section 10.3, Urban Forests of the Official Plan shall apply and the Town shall establish specific landscaping requirements in the Urban Design and Open Space Guidelines to ensure:

- a) the creation of a human scale within new development;
- b) the enhancement of pedestrian comfort;
- c) the provision of features which contribute to the definition of public open space, framing of views and focal points, direction of pedestrian movement and demarcation of areas with different functions; and,
- d) landscape design that promotes the use of native species and enhancement of ecological stability and integrity.



#### **7.5.10 SAFE COMMUNITY DESIGN**

The Town shall work with Halton Region Police to promote safety and security and accessibility in public places through urban design including design and the siting of buildings and structures that:

- a) encourages continuous occupancy of public spaces by ensuring the proximity of spaces, activities and institutions which provide public presence at various times and by the sharing of facilities;
- b) provides for opportunities for visual overlook and ease of public access to adjacent streets, parks and other public areas;
- c) results in clear, unobstructed views of parks, school grounds, and open spaces from adjacent streets;
- d) ensures appropriate lighting, visibility and opportunities for informal surveillance are provided for walkways, parking lots, parking garages and open space areas;
- e) results in the selection and siting of landscape elements in a manner which maintains views for safety and surveillance;
- f) encourages the provision of views into, out of and through publicly accessible interior spaces;
- g) precludes entrapment or the perception of entrapment through properly identified exits and signage; and,
- h) results in accessibility for the disabled and elderly.

#### **7.5.11 COMMUNITY LINKAGES**

North Oakville East will be connected to other parts of the Town whenever possible through road, transit, pedestrian and bicycle links, to ensure that the community functions in an integrated manner. In particular:

- a) the design of Dundas Street and abutting uses should allow it to become a point of transition rather than the current "division" or "barrier" effect of the street. Design should be used to encourage visual and physical links between the north and south sides of the street;
- b) the intersections of Trafalgar Road and Dundas St. and Neyagawa Blvd and Dundas St. should be marked by the siting and design of prominent buildings, strategic building placement and other gateway features. Minor gateways should be established at other intersections with Dundas St;
- d) Trafalgar Road provides a major physical north/south link through the Town from a transportation perspective. The design of the road itself and the Trafalgar Core Area development should strengthen this link and ensure a strong relationship to the Uptown Core, as well as the Midtown Core, and Downtown areas.

#### 7.5.13 URBAN CORE AREAS – INTERIM USES & PHASED DEVELOPMENT

Urban Core Areas, particularly the Trafalgar Urban Core Area, are intended to ultimately provide for a primarily mixed use development which is the densest in North Oakville. In order to ensure that the ultimate development form is not constrained by interim land uses and the development of initial phases, the following planning and design principles will form the basis for development:

- a) Buildings shall be planned on the basis that intensification will occur, either by future phases of development around them, by intensification or redevelopment of the buildings themselves, or both. Nevertheless, except for minor buildings and structures, buildings and other facilities shall be viewed as if permanent (i.e., potentially remaining for the long term). Accordingly, buildings shall be located on the site to the urban standards set out in this section and planned so that future phases of intensification are not inhibited.
- b) Development shall be designed to be pedestrian and transit friendly from the outset. In particular, development shall generally be oriented to the street and designed to promote a vital and safe street life and to support the early provision of transit. However, large retail stores, which are permitted within the Urban Core Areas, are often not conducive to a vibrant and active street life. Design alternatives to address such stores may include, among other possible alternatives, locating these stores in the interior of commercial or mixed use development blocks with smaller stores and buildings oriented to surrounding Arterial, Avenue or Connector Roads to create a strong street presence. Alternatively, the frontage of the large retail store facing the Arterial, Avenue or Connector Road could be lined with smaller stores or multiple entrances.
- c) The highest density development should be concentrated on Trafalgar Road in the Trafalgar Urban Core Area.
- d) Design guidelines shall be developed to ensure appropriate relationships between different forms of development recognizing that development in the Urban Core Areas will include a variety of built-forms, and will change and evolve over time.
- e) A street and open space network that fosters connections within the Urban Core Areas and between the Urban Core Areas and the wider community will be created.
- f) Design excellence shall be promoted and shall convey aspects of the Town's local character throughout the Urban Core Areas.

#### 7.5.15 DUNDAS URBAN CORE

The Dundas Urban Core Area as designated on Figures NOE1 and NOE2 is intended to allow the creation of a band of mixed use development at high and medium densities along the Dundas St. corridor. Key design elements of this area include:

- a) Mixed use development shall be permitted throughout the Urban Core Area, however, retail and service commercial development shall be restricted to locations at the intersections with north/south streets.
- b) The full range of uses and densities of development, including live/work units, would be permitted throughout the Urban Core Area, with the exception that retail and service commercial development shall generally be located at the intersections of Minor Arterials, Avenues and Connectors with Dundas Street. It is also anticipated that the highest density uses would be encouraged to locate at these intersections.
- c) Where retail and service commercial development is permitted it will be encouraged in a "main street" format where retail and service commercial uses are oriented to the street creating a pleasant, pedestrian shopping environment. These retail and service commercial uses may be in stand alone stores or in the ground floor of mixed use buildings. In areas of commercial development:
  - i) the principal public entrance should provide direct access onto the public sidewalk;
  - ii) the primary windows and signage should face the street;
  - iii) buildings facing the street should be encouraged to have awnings, canopies, arcades or front porches to provide weather protection;
  - i) no parking, driveways, lanes or aisles should be permitted between the buildings and public sidewalks;
  - v) buildings should have a consistent setback and parking lots abutting the street should be limited and designed in accordance with the provisions of Section 7.5.7.1;
  - vi) the location and design of any large retail stores shall consider the design alternatives set out in Section 7.5.13 b); and,
  - vii) any commercial nodes including large retail stores should be integrated into the pattern of streets and blocks of which they are a part. The pattern of blocks and the physical design of the buildings in relation to the street should encourage pedestrian circulation to, from and within this commercial area. Streets, sidewalks and the orientation of buildings shall

## **7.6.5 DUNDAS URBAN CORE AREA**

### **7.6.5.1 Purpose**

The Dundas Urban Core Area designation on Figure NOE2 is intended to allow the creation of a band of mixed use development at medium and high densities with a clustering of retail and service commercial development and/or high density buildings at the intersections with north/south streets.

### **7.6.5.2 Permitted Uses, Buildings and Structures**

- a) The permitted uses shall be the full range of office, commercial, including retail and service commercial, health and medical, institutional and medium and high density residential uses.
- b) Permitted uses shall be primarily located in medium and high density residential, office and institutional buildings. Both mixed use and single use buildings shall be permitted. Commercial uses shall be restricted to locations in nodes primarily in mixed use buildings at the intersections with north-south roads, although single use retail and service commercial buildings will be permitted at such nodes in accordance with the applicable policies of Section 7.6.5, including 7.6.5.2 (c) and 7.6.5.3(c) and the design policies of Section 7.5, particularly 7.5.15.
- c) The total retail commercial development in this designation shall not exceed a total of 32,000 sq. metres gross leaseable floor area. Other commercial development such as financial institutions, service stations, restaurants and service commercial uses shall not be subject to this floor area limit. No single user or retail store shall exceed a maximum of 7,000 sq. metres of gross leaseable floor area. Of the total 32,000 sq. metres gross leaseable floor area, a maximum of 19,000 sq. metres of gross leaseable floor area, which may include supermarkets located at the Dundas/Neyagawa intersection.
- d) High density residential uses shall generally be clustered at the nodes.
- e) Stand-alone townhouses shall not be permitted adjacent to Dundas Street.

### **7.6.5.3 Land Use Policies**

- a) Development will be visually connected by establishing a coherent streetscape along Dundas Street through a number of design features and mechanisms, identified in the applicable urban design guidelines, including provisions for landscaping, signage, street furniture and other features in the public right of way, and guidelines for the siting and massing of adjacent buildings. Development will also be physically connected by road, transit, pedestrian and bicycle linkages.



- b) Building Heights
  - i) A minimum height of 3 storeys shall be permitted.
  - ii) A maximum building height of 8 storeys shall be permitted.
  - iii) A commercial building permitted as part of a comprehensive development, including implementing zoning, shall be a minimum height of 5 metres.
  - iv) In accordance with Section 7.6.2.2.a.v) increases of up to 4 storeys beyond the maximum permitted building height, exclusive of bonusing, may be considered where adjacent to a stormwater management pond.
  - v) Increases of up to 4 storeys beyond the maximum permitted building height may be considered through bonusing, subject to Section 7.10.2.

## **7.8 DEVELOPMENT REVIEW**

### **7.8.1 PURPOSE**

All development applications in the North Oakville East Planning Area shall be subject to review in accordance with the policies of this section and the other applicable policies of this Secondary Plan. In addition, the Town may require development to be subject to the site plan control provisions of the *Planning Act* as set out in Section 7.10.6.

### **7.8.2 GENERAL**

The policies of this section establish the background information requirements and the criteria which must be satisfied for approval of applications for official plan amendments, zoning by-law amendments, plans of subdivision, plans of condominium, variances and consents and site plans with the exception that:

- a) where the magnitude of an application for an official plan amendment application warrants it, the Town may establish specific additional requirements;
- b) where an application is for a technical or minor change, including applications for variances or consents, the Town may modify or eliminate a requirement or requirements; and,
- c) where certain information is not applicable to the application, the Town may modify or eliminate a requirement or requirements.

### 7.8.3 INFORMATION REQUIREMENTS

The following information and studies shall be submitted as a basis for the evaluation of development applications for new urban development and area design plans where required by Section 7.5.2:

a) Environmental Implementation Reports (EIR)

- i) An Environmental Implementation Report shall be prepared for each subcatchment area, in accordance with the directions established in the North Oakville Creeks Subwatershed Study Implementation Report for each subcatchment area identified in Appendix 7.2;
- ii) The Environmental Implementation Report must demonstrate how the submissions address the overall North Oakville Creeks Subwatershed Management Report.
- iii) The Environmental Implementation Report shall be prepared in accordance with Terms of Reference approved by the Town, the Region and the applicant(s), in consultation with Conservation Halton, by a consultant acceptable to the Town and the applicant(s) and retained by and at the cost of the applicant(s). The Study shall be prepared to the satisfaction of the Town, the Region, Conservation Halton, and other agencies. The Environmental Implementation Report, in concert with the North Oakville Creeks Subwatershed Study fulfills the EIA requirements of the Region of Halton Official Plan.
- iv) All subsequent applications, within a subcatchment area shall conform with the initial EIR prepared for that subcatchment area or a revised EIR shall be prepared in accordance with the provisions of this section.

b) Functional Servicing Studies

A Functional Servicing Study (FSS) shall be prepared for each plan of subdivision, or other major development application. The FSS will include a preferred servicing plan based on an analysis of servicing requirements, in accordance with any approved Class Environmental Assessment Studies, Halton Transportation Master Plan and the Master Servicing Plan for the North Oakville East Planning Area and including:

- i) servicing design requirements;
- ii) preliminary sizing of water and wastewater infrastructure;
- iii) layout for roads and other transportation systems including transit and trails; and,
- iv) preliminary sizing and location of stormwater management facilities and integration with environmental features and development areas.



- c) Transportation Studies  
Where an FSS is not required, but concerns with transportation are identified by the Town or Region, a transportation study may be required. The Transportation Study shall be prepared in accordance with terms of reference approved by the Town and the applicant(s), or where appropriate approved by the Region, by a consultant approved by the Town and the applicant, at the cost of the applicant. The Study shall be prepared to the satisfaction of the Town, and the Region where applicable.
- d) Noise Studies  
Proponents of new residential or residential mixed use development within the following noise sensitive areas may be required to engage an acoustical consultant to undertake an analysis of noise levels which may affect such development and to make recommendations, recognizing the direction proposed by the policies of the Secondary Plan and Provincial requirements, and subject to the approval of the Town and other appropriate agencies, as to the measures to be incorporated into the development to reduce the received noise level:

<u>Road Type</u>	<u>Noise Sensitive Area – Distance from Centre Line</u>
Provincial Freeway	300 m
Major Arterial/Transit Corridor	50 m
Minor Arterial/Transit Corridor	15 m
Employment Area	300 m

Further, when reviewing such applications the Town shall have regard for the applicable Ministry of Environment policies and guidelines with respect to noise and compatibility of uses. In particular, where new commercial or industrial development which are potential major noise sources, such as, but not limited to, uses which have associated with them out door on-going construction activity, outdoor delivery and unloading activity, outdoor heat rejection systems (including cooling towers) and outdoor exhaust fans, the requirements of the Ministry of Environment guidelines "Noise Assessment Criteria in Land Use Planning, October 1995" or any subsequent guidelines or any successor thereto, with respect to the need for a noise impact study shall be carried out to the satisfaction of the Town or the Region where appropriate.

- e) Vibration Studies  
Where new industrial development which is a potential major source of vibration, such as a metal forming industry including punch presses or drop forges, is proposed within 75 metres of existing or designated residential development, a vibration study shall be carried out to the satisfaction of the Town.
- f) Heritage Resources  
Heritage resources shall be subject to the policies of Section 7.4.14 of this Secondary Plan.
- g) Archaeological Resources  
Archaeological Resources shall be subject to the policies of Section 7.4.15 of this Secondary Plan.
- h) Soil Contamination  
The policies of Part C, Section 10.2 of the Official Plan shall apply with respect to required studies and other action with respect to contaminated soils.

#### 7.8.4 DEVELOPMENT EVALUATION CRITERIA

- a) Design  
The appropriateness of the design of the proposed development shall be evaluated in accordance with the policies of Section 7.5 of this Secondary Plan.
- b) Heritage Resources  
Heritage resources shall be evaluated in accordance with the policies of Section 7.4.14 of this Plan.
- c) Archaeological Resources  
Archaeological resources shall be evaluated in accordance with the policies of Section 7.4.15 of this Plan.
- d) Natural Heritage component of the Natural Heritage and Open Space System  
Development shall only be permitted on lands in or adjacent to areas which form part of the Natural Heritage component of the Natural Heritage and Open Space System on Figures NOE1, 2, 3 and 4 in accordance with the policies of this Plan and the results of the Environmental Implementation Report.
- e) Water Management  
The management of water resources shall be carried out in accordance with the policies of this Plan, the directions established in the North Oakville Creeks Subwatershed Study and the relevant environmental implementation report
- f) Services  
New development shall be serviced in accordance with the recommendations of the Town's and the Region's Master Servicing Study and the relevant Functional Servicing Study.
- g) Transportation  
Transportation facilities shall be provided in accordance with the Town's, and where appropriate the Halton Transportation Master Plan, any relevant Environmental Assessment and the relevant Functional Servicing Study and/or any required transportation study.
- h) Financial Impacts  
Only development which can be accommodated within the financial capabilities of the Town and the Region will be permitted.
- i) Soil Capability  
The policies of Part C, Section 10.2 of the Official Plan shall apply with respect to required studies and other action with respect to contaminated soils.
- j) Noise and Vibration Measures  
Where the lands are adjacent to a major noise or vibration source, the Town shall be satisfied that appropriate measures to mitigate adverse impacts, having regard for any Ministry of Environment policies and guidelines, can and will be implemented.

#### **7.10.2 ZONING BY-LAW**

This Secondary Plan will be implemented by appropriate amendments to the Town's zoning by-law in accordance with the policies of this Secondary Plan and Part F, Section 1.3 of the Official Plan, in conjunction with draft plans of subdivision or other specific development applications. With respect to the bonus provisions of Section 1.3 b), in addition to the matters listed, bonusing for development in the Trafalgar Urban Core may be considered for the provision of urban squares where the provision of such facilities would exceed the 5% parkland dedication requirements, municipal parking structures, underground parking, structured parking, and enhanced streetscape facilities. However, the maximum height of any building in Trafalgar Urban Core Area, including any bonus for height, shall be 30 storeys.

#### **7.10.4 PLANS OF SUBDIVISION OR CONDOMINIUM**

Only those plans of subdivision or condominium shall be approved for development which:

- a) comply with the designations and policies of this Plan;
- b) can be supplied with adequate infrastructure, services and community facilities; and,
- c) will not adversely affect the financial status of the Town or the Region of Halton.

## Zoning By-law 2009-189

## Section 7.0

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## Future Development Zone

### 7.13 Future Development (FD) Zone Regulations *(2012-001)* *(2022-007)*

#### 7.13.1 Permitted Uses

- i. The uses as permitted in Section 4.3.1.
- ii. Infrastructure for which an Environmental Assessment has been completed or which are required as a condition of approval under the Planning Act, subject to the following provisions:
  - a. Notwithstanding any other provision of this By-law, any *building* or *structure* for the purpose of providing or sheltering infrastructure shall not be subject to Section 7.13.2;
  - b. Any *building* or *structure* used for a *general office use*, *public works yard*, or *warehouse* shall not be permitted.
- iii. The *use* of legally existing *buildings* and *structures* by new *accessory uses*.

#### 7.13.2 Permitted Expansion of Legal Buildings and Structures

The existing *lot coverage* on the day of the passage of this by-law, for all existing *buildings* or *structures* and existing *accessory buildings* or *structures*, may be increased by a maximum of 10% subject to regulations of this section and section 5.

#### 7.13.3 Maximum Height

10 metres except for *agricultural buildings*

#### 7.13.4 Minimum Yards

*Front Yard* and *Flankage* - 9 m  
*Side Yard* - 2.4 m  
*Rear Yard* - 7.5 m

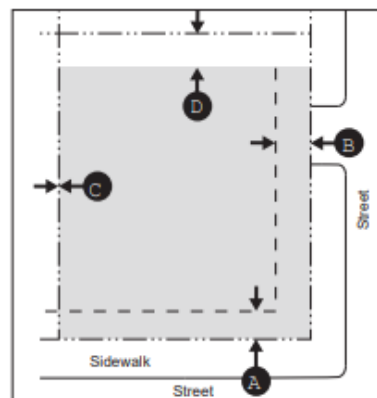
*(Performance zones relocated by 2012-001 and 2013-065)*

## Dundas Urban Core Zone

### 7.2 Dundas Urban Core (DUC) Zone Regulations

#### 7.2.1 Uses Permitted

- i. See Section 6.
- ii. *Retail Uses*
  - a. No single *retail use* shall exceed a maximum of 7,000 square metres of *leasable floor area*; and,
  - b. The maximum total *retail development* in the DUC Zone, including any DUC Performance Zone, shall not exceed 32,000 square metres *leasable floor area* of retail development, of which a maximum of 19,000 square metres of *leasable floor area* of retail development, which may include a *supermarket*, shall be located within 300 metres of the intersection of Dundas St. and Neyagawa Blvd.



The grey represents potential building area. The internal dashed line represents the maximum yard.

#### 7.2.2 Building Types Permitted and Related Standards

Building Type	Min. Floor Space Index	Max. Floor Space Index	Min. Front Yard and Flankage <b>A B</b>	Max. Front Yard and Flankage <b>A B</b>	Min. Interior Side Yard Setback <b>C</b>	Minimum Rear Yard Setback <b>D</b>
<ul style="list-style-type: none"> <li>Mixed use building;</li> <li>Apartment;</li> <li>Office building;</li> <li>Hotel;</li> <li>Institutional building;</li> <li>Parking garage;</li> <li>Commercial/residential building</li> </ul>	0.5	2.5	0.0 m	6 m	0.0 m <sup>1</sup>	1 m with rear lane access, unless there is a residential use at grade where the setback shall be 0.75 m 6 m without rear lane access, except abutting lands in the NHS, CE, or P Zones where the setback shall be a minimum of 3 m

<sup>1</sup> Minimum Interior Side Yard Setback – Notwithstanding this requirement, if the side yard abuts lands in the GU or S Zone and the building on the lot has a height of 4 storeys or less, the setback shall be 1.5 m; and if the building has a height of more than 4 storeys the setback shall be 7 m.

#### 7.2.3 Permitted Yard Encroachments

See Section 4.21



## 7.2.4 Height

- Minimum *height* for all *buildings*, other than *accessory buildings and structures* - 2 storeys

Maximum *height* - 8 storeys

## 7.2.5 Parking Standards

See Section 5

## 7.2.6 Accessory Buildings and Structures

See Sections 4.14 through 4.20 inclusive

## 7.2.7 Performance Zone Categories - Dundas Urban Core (DUC) Zone

### 7.2.7.1 Dundas Urban Core Performance (DUC-1) Zone 1

The permitted *uses, buildings* and regulations of the DUC Zone shall apply in the Dundas Urban Core Performance (DUC-1) Zone 1, however, in addition *commercial buildings* with a minimum *overall height* of 5 metres shall be permitted.

### 7.2.7.2 Dundas Urban Core Performance (DUC-2) Zone 2

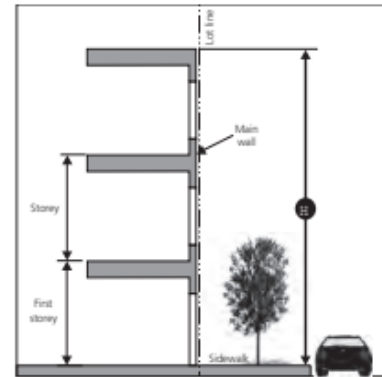
In addition to the permitted *uses* and regulations of the DUC Zone, in the Dundas Urban Core Performance (DUC-2) Zone 2, the *uses and buildings* in the GU Zone will also be permitted and the regulations of the GU Zone shall apply to those *uses and buildings*. Notwithstanding the regulations of the GU Zone, however, the *minimum rear yard* shall be 6 m.

### 7.2.7.3 Dundas Urban Core Performance (DUC-3) Zone 3

The permitted *uses, buildings* and regulations of the DUC Zone shall apply in the Dundas Urban Core Performance (DUC-3) Zone 3, however, the *minimum height* for all *buildings*, other than *accessory buildings and structure* shall be three storeys.

### 7.2.7.4 Dundas Urban Core Performance (DUC-4) Zone 4

The permitted *uses, buildings* and regulations of the DUC Zone shall apply in the Dundas Urban Core Performance (DUC-4) Zone 4, however, where a *lot* has *streets* on three or four sides, the *front and flankage yard* requirements shall apply to all *yards* abutting a *street*.



Height is measured to the tallest point of a building, excluding a number of rooftop features (see Section 4.28).



#### **7.2.7.5 Dundas Urban Core Performance (DUC-5) Zone 5**

The permitted *uses, buildings* and regulations of the DUC Zone shall apply in the Dundas Urban Core Performance (DUC-5) Zone 5, however, notwithstanding the provisions of Section 5.1.3, Location of *Parking Spaces*, the required *parking spaces* for any use may be located on the *lot* on which the *use* is located, and/or on one half of the *street* abutting the *lot* where the on-street parking is permitted.

#### **7.2.7.6 Dundas Urban Core Performance (DUC-6) Zone 6**

The permitted *uses, buildings* and regulations of the DUC Zone shall apply in the Dundas Urban Core Performance (DUC-6) Zone 6, however, notwithstanding the provisions of Sections 4.15.2 and 4.16.3, Maximum *Height* for *private garages*, the maximum *height*, if the *lot frontage* is 9 metres or more, for a roof, other than a flat roof, shall be 9 metres.

#### **7.2.7.7 Dundas Urban Core Performance (DUC-7) Zone 7**

In addition to the permitted uses and regulations of the DUC Zone, in the Dundas Urban Core Performance (DUC-7) Zone 7, the *uses* and *buildings* in the GU Zone will also be permitted and the regulations of the GU Zone shall apply to those *uses* and *buildings*. However, notwithstanding the permitted *building* types and related standards (Section 7.6.2) of the GU Zone, *single-detached* and *semi-detached building* types shall not be permitted.

#### **7.2.7.8 Dundas Urban Core Performance (DUC-8) Zone 8**

The permitted *uses, buildings* and regulations of the DUC Zone shall apply in the Dundas Urban Core Performance (DUC-8) Zone 8, however where a *lot* has *streets* on three or four sides, the *front* and *flankage yard* requirements shall apply to all *yards* abutting a *street*. In addition, the *height* for *commercial uses* shall be a minimum of 5 metres.

*(DUC-9 and higher relocated by 2013-065)*

### **4.21 Encroachments Permitted in Required Yards**

Every *yard* shall be open and unobstructed from the ground to the sky by any *structures*, except for:

- i. lawful fences, signs, and retaining, noise and ornamental walls;
- ii. *driveways* and walks; and,
- iii. the encroachments listed in Table 4.21 which are permitted in association with either the main *building*, or an *accessory buildings* or *structures*, subject to compliance with the regulations listed in the columns of the Table 4.21.

**Table 4.21 - Permitted Encroachments into Required Yards**

	<b>Structure</b>	<b>Yard in which Structure is Permitted</b>	<b>Maximum projection into required yard</b>	<b>Minimum Distance from Lot Line<sup>1</sup></b>
(a)	Sills, belt courses, cornices, coves, pilasters, eaves, gutters, awnings, canopies or other non-structural architectural features	All	0.6m	n/a
(b)	Fire Escapes	<i>Rear &amp; Side</i>	1.5m	0.6m from the <i>lot line</i>
(c)	Exterior stairways	<i>Rear &amp; Side</i>	1.5m	0.6m from the <i>lot line</i>
(d)	<i>Balconies</i>	<i>All</i>	1.5m	1.5m unless overtop a <i>porch</i> in which case the <i>setbacks</i> shall be the same <i>setbacks</i> as the <i>set-back</i> for a <i>porch</i> under sub-section (e)

**Table 4.21 - Permitted Encroachments into Required Yards**

	<b>Structure</b>	<b>Yard in which Structure is Permitted</b>	<b>Maximum projection into required yard</b>	<b>Minimum Distance from Lot Line<sup>1</sup></b>
(e)	<i>Uncovered platforms 0.6 m or greater in height measured from surrounding grade and porches, including a balcony on top of a porch including vertical supports and a roof above and landings. However, any exterior stair required to provide access to an uncovered platform or porch shall be permitted to encroach into any required yard.</i>	<i>Front</i>	1.5m	1m from the <i>front lot line</i> with exception of any exterior stair which may encroach to within 0.5m of the <i>front lot line</i> .
		<i>Rear</i>	2.5m	0.6m from the <i>rear lot line</i>
		<i>Interior Side</i>	n/a	0.6m from the <i>interior side lot line</i> except: i. where the platform is designed to allow drainage to occur underneath in which case the min. setback shall be 0m; and, ii. for <i>townhouse</i> and <i>semi detached</i> units in which case it may be 0m from an interior common wall
		<i>Flankage</i>	n/a	1m from the <i>flankage</i> with the exception of any exterior stair which may encroach to within 0.5m of the <i>flankage</i>
(f)	Chimneys and gas fireplace projections and chases (with a maximum width of 1.8m)	<i>Front &amp; Flankage</i>	0.6m	1m from the <i>front lot line</i> and <i>flankage</i>
		<i>All other yards</i>	0.6m	0.6m from the <i>lot line</i>
(g)	Bay, Box out and Bow Windows with or without foundations with a maximum width of 3m which may be a maximum of three storeys in height and which may include a door.	<i>Front &amp; Flankage</i>	0.6m	1m from the <i>front lot line</i> and <i>flankage</i>
		<i>All other yards</i>	0.6m	0.6m from the <i>lot line</i>
(h)	Uncovered stairs below grade accessing a main building	<i>Rear</i>	1.5m	0.6m from the <i>lot line</i>
		<i>Side</i>	0m	1.5m
(i)	<i>Uncovered platforms less than 0.6m in height measured from surrounding grade</i>	<i>Front</i>	1.5m	1m from the <i>front lot line</i> and <i>flankage</i>
		<i>Interior Side</i>	n/a	0.6m from the <i>interior side lot line</i> except: i. where the platform is designed to allow drainage to occur underneath in which case the minimum setback shall be 0m; and, ii. for <i>townhouse</i> and <i>semi detached</i> units in which case it may be over 0m from an interior common wall
		<i>All other yards</i>	n/a	0.6m from the <i>lot line</i>
(j)	Unenclosed barrier free ramps	All	n/a	0.6m from the <i>lot line</i> unless the ramp is designed to allow drainage to occur underneath in which case the minimum setback shall be 0m

Minimum distance from lot line does not apply where minimum building setback is 0 metres.

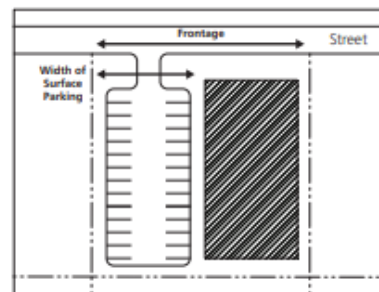
## Parking and Loading Regulations

### 5.1.6 Surface Parking Area Location

Where *surface parking areas* are permitted on lots which abut Trafalgar Road, the maximum *lot frontage* or *flankage* on Trafalgar Road occupied by *surface parking area* shall be:

<i>Office building</i>	50%
<i>Mixed use and apartment buildings</i>	20%
<i>Commercial buildings</i>	20%
<i>Parking garage</i>	5%
<i>Institutional building</i>	5%
<i>Hotel</i>	5%
<i>Commercial/Residential buildings</i>	5%

This shall be measured as the maximum percentage of *lot frontage* or *flankage* occupied by *surface parking area* where no *building edge* is located within 6 metres of the Trafalgar Road *lot frontage* or *flankage*. The only exception shall be where the By-law permits a greater *front yard* or *flankage setback*, in which case that setback shall apply.



Surface Parking Frontage.

### 5.1.7 Hardscape Surface Treatment (2022-007)

All *parking spaces*, *tandem parking spaces*, *parking pad*, *loading dock*, *aisles* and *driveways* in any Zone other than a Natural Heritage Zone shall be surface treated with asphalt, concrete, interlocking brick, similar hardscaped surface, or other material sufficient to provide stability, prevent erosion, be usable in all seasons, and allow infiltration of surface water.

**Table 5.1A - Parking Requirements For Residential Uses**

(4)	<i>Apartment - More than 4 storeys</i>	Up to 1.25 <i>parking spaces</i> per <i>dwelling unit</i> , plus 0.2 <i>parking spaces</i> per <i>dwelling unit</i> for visitors. Additional <i>parking spaces</i> shall not be permitted. In the Trafalgar Urban Core Zone, no <i>parking spaces</i> shall be permitted in a <i>surface parking area</i> , with the exception of visitor <i>parking spaces</i> which may be located underground, in a <i>parking garage</i> or in a <i>surface parking area</i> .
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**Table 5.1B - Parking Requirements For Non-Residential Uses**

(4)	<i>Café</i>	<p>No <i>parking spaces</i> shall be required and a maximum of 1 <i>parking space</i> per 20 square metres of <i>leasable floor area</i> shall be permitted.</p> <p>In the Trafalgar, Dundas, Neyagawa and Palermo Village North Urban Core Zones, no additional <i>parking spaces</i> above the minimum shall be permitted, unless such parking is located in a <i>parking garage</i> or <i>on-street</i>.</p>
(5)	<i>Club</i>	<p>1 <i>parking space</i> per 30 square metres of <i>leasable floor area</i> minimum; and,</p> <p>1 <i>parking space</i> per 20 square metres of <i>leasable floor area</i> maximum.</p> <p>In the Trafalgar Urban Core Zone, no additional <i>parking spaces</i> above the minimum shall be permitted, unless such parking is located in a <i>parking garage</i>.</p>
(6)	<i>Commercial fitness centre; or, Place of amusement</i>	<p>1 <i>parking space</i> per 30 square metres of <i>leasable floor area</i> minimum; and,</p> <p>1 <i>parking space</i> per 20 square metres of <i>leasable floor area</i> maximum.</p> <p>In the Trafalgar Urban Core Zone, no additional <i>parking spaces</i> above the minimum shall be permitted, unless such parking is located in a <i>parking garage</i>.</p>
(7)	<i>Commercial residential -combined (mixed use)</i>	The applicable regulations for <i>non-residential uses</i> apply to the non-residential <i>floor area</i> , while the applicable residential regulations apply to the <i>dwelling units</i> .
(8)	<i>Commercial residential -connected (live-work)</i>	<p>Residential Component: 1 <i>parking space</i> minimum and 2 <i>parking spaces</i> maximum per <i>dwelling unit</i> which may be provided in tandem; and,</p> <p>Commercial Component: i) up to 90 square metres - 2 <i>parking spaces</i> ii) over 90 square metres - the applicable regulations for <i>non-residential uses</i> apply to the non-residential <i>floor area</i></p>
(9)	<i>Commercial school</i>	<p>1 <i>parking space</i> per 30 square metres of <i>leasable floor area</i> minimum; and,</p> <p>1 <i>parking space</i> per 20 square metres of <i>leasable floor area</i> maximum.</p> <p>In the Trafalgar Urban Core Zone, no additional <i>parking spaces</i> above the minimum shall be permitted, unless such parking is located in a <i>parking garage</i>.</p>
(10)	<i>Commercial self storage</i>	1 <i>parking space</i> per 650 square metres of <i>leasable floor area</i> minimum.



**From:** Richard Hill  
**Sent:** Thursday, February 27, 2025 10:25 AM  
**To:** Jeff Knoll; Scott Xie  
**Cc:** Town Clerks; Catherine Buckerfield  
**Subject:** [EXTERNAL] Opposition to High-Density Development Proposal Neyagawa and Dundas

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff

could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

Richard Hill

Oakville Resident, River Oaks

**From:** Stephanie Laivenieks  
**Sent:** Wednesday, February 26, 2025 6:06 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Dear Town Clerk,

I am writing you to share my concerns regarding the proposed condo development at the corner of Neyagawa and Dundas Street. This plan cannot go forward as it will negatively impact the nearby neighbourhoods, homes and residents.

I have lived in the River Oaks community for 12 years. In that time, I have witnessed the land north of Dundas Street changed from farmland - there was a field with cows just across the street, to an unsightly and congested neighbourhood of condos and businesses that are turning North Oakville into a heartless wasteland of urban sprawl. We moved to the "Town" of Oakville to get out of the big city and give our kids a better life closer to parks and green space, North Oakville has not lived up to that promise.

The over development of the neighbourhood north of Dundas Street is negatively impacting our community and my family. We were very disappointed when Dundas Street was widened to a 6 lane highway with a speed limit of 70 km/hr. We left Toronto to get away from car emissions and pollution that have been proven to negatively impact health. Dundas Street is a road that my children need to cross to visit friends and go to parks, but it is extremely dangerous and has become a highway for commuters, who do not care about our children or elderly that are crossing this street. I understand the need to support improved infrastructure and traffic congestion, but this change has been very detrimental to the community of River Oaks.

I understand the need for more housing but I think it is misguided to do this by building high rise condos near a residential area that was originally planned as a single family community. Most families moving to Oakville do not want to live in condos, they need 3 bedrooms and a yard. From my understanding the condo market has stagnated, so why do we think we need more of this type of housing?

I have accepted the need for ongoing low rise multi family buildings north of Dundas, to support increasing population size and housing needs. What I do not accept is that high rises are a necessity and do not think they have a place in our neighborhood. I look out my window and see condos above the houses instead of the night sky and trees. We were previously under the impression development was focused on the Trafalgar and Dundas area or the North Oakville "core", what is the need to have this spill over to another neighbourhood? I use to pity the people who live in Mississauga among the massive condos and traffic congestion, now we are living in that nightmare.

There has been a constant negative impact on our school River Oaks PS (ROPS), caused by overdevelopment and poor planning. Children who move into homes north of Dundas never have enough schools in their areas due to oversights by the "Town" and the Province. These children end up at our school overflowing classrooms in our school leading to adding many portables that take over our playground. There is constant disruption in our school community as kids that my children grew up with are then taken away from our school when new schools are finally added and catchments are changed. There has been constant chaos and uncertainty for our kids at ROPS primarily caused by overdevelopment and poor infrastructure planning North of Dundas.

I would like to highlight the other infrastructure challenges related to this plan that we are very worried will negatively impact our neighbourhood:

- impact to the green space, animals and ecosystem surrounding Sixteen Mile Creek
- risk to overwhelming the drainage and sewage systems in our area, causing flooding and damage to our homes
- increase in the traffic which is already unmanageable on Neyagawa during rush hour
- a lack of transit to support the transportation needs of the community, "Town" buses cannot manage the influx of new residents planned on Dundas Street

Finally, I highly recommend the "Town" of Oakville change their name, you should no longer be allowed to use the term town. We lost the small town size and sensibilities long ago as development continues without any thought of the impact on the community. Please recognize we are in fact a city, that you are allowing to turn into a vast sprawling metropolis.

Please kindly share these concerns with the Town Council and the Mayor. A response and information decisions regarding this plan would be very appreciated.

Best regards,  
Stephanie Laivenieks

From: jordana rosen  
Sent: Tuesday, February 25, 2025 9:03 PM  
To: Catherine Buckerfield <catherine.buckerfield@oakville.ca>  
Subject: [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

This should not be happening. There will be too much traffic in the area.  
Sent from my iPhone



**From:** Grace Eunjung Yang  
**Sent:** Tuesday, February 25, 2025 11:00 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Attn: My neighbours are against 3056 Neygawa Boulevard Developmemt!!

Dear whom it may concern,

Hello. I am Grace Yang and writing to request an amendment of 3056 Neygawa Boulevard Developmemt!  
We are Grace Eungjung Yang, Sung Hoon Son and David Joonyoung Son.

Travertine dr, Oakville L6M5N6

We moved in Oakville a year ago as Oakville offered more peaceful less crowded, not too many high rising buildings compared to other cities.

So do my neighbours!!

Plz consider our opinions and read below.

Thank you!

Regards,  
Grace, Sung, David

**From:** Prashant Bharate

**Sent:** Sunday, February 23, 2025 3:48:36 PM

**To:** Marc Grant ; Jeff Knoll

**Subject:** [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns

for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

Your Neighbour,

Prashant Bharate.

**From:** iantoun  
**Sent:** Monday, February 24, 2025 12:35 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal at Dundas and Neyagawa

Good afternoon Catherine,

My name is Irene and I am a resident in the neighbouring area that is being considered for a high density development by Neatt (16 Mile Creek) Inc. My family and I have been fortunate to live in this area for 25 years and in Oakville for 35 years. We were drawn to the Town of Oakville, back then, because of its quaint size and small hometown appeal. It was a lovely community to raise a family and lay roots; comfortably away from the "big city". Needless and sad to say that the Oakville of old that we were drawn to is long gone.

Growth and expansion are inevitable and a number of the changes have enhanced our "town" over the years; the new OTMH, updated downtown, the beautiful Tannery park area, and the Smith-Triller bridge just to name a few.

In the last 5-10 years or so, however, the explosion of expansion has accelerated by leaps and bounds bringing along with it the obvious burdens of expansion, such as, crime, which were unheard of 5 years ago. The peace of mind that we once enjoyed seems to be something that will never be regained, even more so with an increase in population in an already dense area. Traffic has become consistently congested on all main arteries, especially on Dundas making those 10-15 minute drives closer to 45 minutes longer in high traffic times, but with an added frustration and anxiety. Something as simple as garbage collection which once occurred earlier in the day is now end of day, not that this in of itself matters, but it indicates the added load on the system and services. An increase of sirens heard daily, of emergency responders attending to these corresponding issues, is a small sampling of the changes experienced, all of which may seem insubstantial in themselves, but add up to a decline in home life quality.

We have worked to make our house a home over the years and the surrounding community supports that, when the community starts to drastically change from what you originally signed up for, it is disheartening.

I understand and empathize with the need for more housing, I don't understand why an already established high density area is being targeted and overburdened, when there are several open spaces that are not utilized.

I hope the impact on existing residents will be measured with greater consideration than those of a developer since we will have to live with the long term outcome.

Thank you for your time and the opportunity to communicate some of the concerns.

irene

**From:** ziyiing chen  
**Sent:** Sunday, February 23, 2025 12:49 AM  
**To:** Town Clerks; Catherine Buckerfield; Scott Xie; Nav Nanda  
**Subject:** [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

**Traffic Congestion and Infrastructure Strain**

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

1. **Overcrowding and Strain on Public Services**

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

2.

3. **Community Character and Livability**

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4.

5. **Environmental Impact**

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,  
Ziying Chen



**From:** Prashant Bharate  
**Sent:** Sunday, February 23, 2025 3:45 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

do understand towns long term need to have a growing younger population to balance maturing neighborhoods but adding almost half the size of total River oak community houses in single development is not the answer.

I'm open to well managed population density increase but strain on current infrastructure of town should be considered too. Already it's hard to get proper resources in towns libraries/day cares and summer camps which if you have children of school age will know gets full within a day or two. Our community centers are over loaded and River Oaks elementary school just getting out of holding school status will be overcrowded if there are no other new schools being built other than 2 in current planning. I as a resident oppose this huge number of units development.

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Your Neighbour,  
Prashant Bharate

**From:** Nancy JT  
**Sent:** Sunday, February 23, 2025 10:21 AM  
**To:** Town Clerks; Catherine Buckerfield  
**Subject:** [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West. The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

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2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an

important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

Nancy Jiang

**From:** Nancy JT  
**Sent:** Sunday, February 23, 2025 10:18 AM  
**To:** Town Clerks; Catherine Buckerfield  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Nancy Jiang

**From:** Joe Paquete  
**Sent:** Sunday, February 23, 2025 3:07 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

I have lived in Oakville for over 25 years.

I am NOW happy in the direction it is moving now.

What happened to to height restrictions.

I don't want our town to look like the city of Mississauga.

Stop this development now

Joe Paquete of River Oaks  
Sent from my iPhone

**From:** Farooq Akhter  
**Sent:** Sunday, February 23, 2025 1:14 PM  
**To:** Town Clerks; Catherine Buckerfield  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested, we are already seeing the effect of the new condo developments in my area (Neyagawa and Dundas). In morning rush hour and after work, its impossible to drive on Dundas East or West in order to reach 403 or Bronte intersection. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Thanks and regards  
Farooq Akhter, CPA



**From:** shahrzad davari  
**Sent:** Saturday, February 22, 2025 9:33 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Hello,

We are against building extra towers in Dundas & Neyagawa Blv.

Please stop bullying extra condos.

Regards!

**From:** Shahrzad Davari  
**Sent:** Saturday, February 22, 2025 9:30 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Hi,

Dundas street West and Boulevard, does not have capacity to build 7 towers.

You should not only think about benefit of developers, but also benefits of residents in this tiny location.

Stop building extra towers in Dundas.

Thanks!

**From:** Michael Stofberg  
**Sent:** Friday, February 21, 2025 10:15 AM  
**To:** Town Clerks; Catherine Buckerfield; Marc Grant  
**Subject:** [EXTERNAL] Objection to the development of 2056 Neyagawa Boulevard

Dear Town Council Members,

In reference to the document you sent for the development of 3056 Neyagawa Boulevard we oppose this extremely high density development on our door step.

We are not opposed to the building of additional housing but feel that the impact on the surrounding area will be detrimental to us to live in close vicinity. We are concerned about the traffic, if public services can cope, the local community centre and if the current construction will be under specced and the impact on our house value. We specifically bought close to 16 mile creek and also are concerned about the impact to that and cannot find any details on that analysis. We also believe that such high density living should be closer to the transport hub of Oakville or Bronte Go.

The development on the opposite corner is more in line with what we would consider acceptable and aligns to the other developments north of Dundas.

We believe that the council should consider the current residents and find alternative solutions to housing. Hence, they should reject this application.

Cheers

Michael and Rosanna Stofberg  
Valley Heights Crescent

**From:** ida bilajbegovic  
**Sent:** Friday, February 21, 2025 5:54 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Fwd: High Density Development Proposal at Neyagawa Blvd & Dundas St W

Begin forwarded message:

**From:** ida bilajbegovic  
**Subject:** High Density Development Proposal at Neyagawa Blvd & Dundas St W  
**Date:** February 21, 2025 at 5:45:52 PM EST  
**To:** "catherine.buckerfield@oakville.ca" , "jeff.knoll@oakville.ca" ,  
"marc.grant@oakville.ca" , "allan.elgar@oakville.ca" , "peter.longo@oakville.ca" ,  
"dave.gittings@oakville.ca" , "janet.haslett-theall@oakville.ca" ,  
"ray.chisholm@oakville.ca" , "cathy.duddeck@oakville.ca" ,  
"jonathan.mcneice@oakville.ca" , "sean.o'meara@oakville.ca" , "mayor@oakville.ca"

Dear Mayor Council and Development Planners

I have been an Oakville resident for the past 30 years, 20 of which have been in my current house at Neyagawa and Riverbank just across from the proposed development. As a neighbourhood we have lived through numerous developments including the construction of the Lyons Valley Dundas Bridge, 16 Mile Sports Complex, 16 Mile Community Centre and all the housing construction north of Dundas.

All this development was necessary for community growth, Oakville expansion, and keeping in target with Provincial housing needs.

This new development, however is not seen as such! I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St. W.

This project will worsen already heavy traffic congestion on Dundas! It will overburden public services and most definitely disrupt the character of our community which is the number one reason all of us in my neighbourhood have made Oakville our home.

Oakville's infrastructure is not equipped for 2,278 new residential units. This will strain roads, schools ....need I mention my son was moved three times from school to school during his kindergarten to Gr. 8 education, he is now in Gr. 9 at TA Blakelock HS. Additionally the environmental impact on our beautiful 16 Mile Creek has not been properly addressed. What is happening with keeping Oakville the "most livable and beautiful town"?

I believe this proposed development will lower the real estate value of our homes which is, as you all know the biggest investment any of us make.

I urge the Council to REJECT this proposal and prioritize sustainable growth which preserves Oakville's livability.

Sincerely,

Ida Bilajbegovic

**From:** Kent Potts  
**Sent:** Thursday, February 20, 2025 2:57 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

**I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W.** This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed. I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Regards,

Kent Potts

RiverBank  
Way Oakville,  
Ontario L6H 6X4



**From:** Rachel Phillips  
**Sent:** Thursday, February 20, 2025 2:21 AM  
**To:** Town Clerks; Catherine Buckerfield  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I have a question for you - do any of you live in North Oakville? I've been living here for 12 years. Over those 12 years, the area has slowly but surely deteriorated. The number of people and the traffic is ridiculous now. Who is approving the insane amount of condos being built!? Is this really all about money? I live in the Preserve and they built a condo RIGHT in front of my house on Kaitting Trail, with an entrance from my street right into the condo building. The street is completely unbearable with constant traffic, parking and Uber drivers parking in front of my driveway at all hours. They also built a condo right inside the neighbourhood amongst all detached houses. If you speak to anyone living in this neighbourhood now, they will all tell you the same thing - they despise living here now and are ready to leave. The sheer amount of people living here now is unbelievable. This is not a suburb - it's turning into a busy crap hole, excuse my language.

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

I assume my email means nothing as do the viewpoints of everyone living here, and I'm sure you don't care at all as it will not affect you but I'm voicing my opinion.

Sincerely,  
Rachel Azan  
Kaitting Trail Oakville, ON L6M0T6

**From:** Felix Xia  
**Sent:** Tuesday, February 18, 2025 5:04 PM  
**To:** Town Clerks; Catherine Buckerfield  
**Subject:** [EXTERNAL] Subject: Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Felix Xia

**From:** Essam Azar  
**Sent:** Monday, February 17, 2025 4:36 PM  
**To:** Catherine Buckerfield  
**Subject:** Niagawa / Dundas Condos

Dear Catherine

On behalf of my family and many of our friends who live near and around Niagawa and Dundas, we firmly believe that the massive condo proposed project not only does not add any value to our quite community but on the contrary, it hurts us in every possible way you can think of.

Please do not go ahead with any approvals for this project.

Thank you for keeping our community safe, quiet and livable.

Essam Azar  
Niagawa/Dundas resident

**From:** Danfer Yang  
**Sent:** Monday, February 17, 2025 10:20 PM  
**To:** Town Clerks; Marc Grant; Jeff Knoll; Scott Xie; Nav Nanda; Peter Longo; Allan Elgar  
**Subject:** [EXTERNAL] Opposition to Neyagawa/Dundas High Rise Condo Community

Dear Councillors of Ward 5, 7, and 4,

My name is Danfer, and I am a resident of the River Oaks community at Valley Heights Crescent, Oakville. I am writing to formally express my strong opposition to the proposed high-rise condo development at Neyagawa and Dundas. The details of this application can be found here:

[Development Proposal Link](#)

The proposed development would introduce seven high-rise buildings ranging from 15 to 28 storeys, totaling approximately 2,300 residential units and 1,600 square meters of retail space on just 3.75 hectares of land. I have several serious concerns about the negative impact this project will have on our community:

1. **Traffic Congestion:** Our neighborhood already faces severe traffic congestion, especially during peak hours. Adding thousands of new residents and their vehicles will significantly worsen gridlock, increase commute times, and create safety hazards for pedestrians and cyclists.
2. **Strain on Schools and Public Services:** The current infrastructure—including schools, healthcare facilities, and recreational spaces—is already overburdened. Local schools are at capacity, and families are struggling to access essential services. A development of this scale will only intensify these issues.
3. **Flooding and Drainage Risks:** The proposed development may overwhelm existing drainage systems, increasing the risk of flooding in nearby residential areas. Additionally, it could have long-term consequences for the surrounding ecosystem and conservation lands.
4. **Environmental and Heritage Impact:** The proposed site is adjacent to Sixteen Mile Creek and the historic Trafalgar Cemetery. A high-density development of this magnitude will disrupt the natural beauty and ecological balance of one of Oakville's most important conservation areas. Furthermore, it will compromise the tranquility of the cemetery, a site of historical and cultural significance.

Given these concerns, I urge the Town of Oakville to reject this proposal and prioritize responsible, sustainable development that aligns with our community's infrastructure capacity and environmental priorities.

I would appreciate the opportunity to discuss this matter further. Please feel free to contact me at 647-896-4696 or via email.

Thank you for your time and consideration.

**Best Regards**

Danfer

**From:** Rania Tannous  
**Sent:** Sunday, February 16, 2025 9:13 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Dear Members of the Oakville Town Council

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

**Traffic Congestion and Infrastructure Strain:** The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

**Overcrowding and Strain on Public Services:** A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

**Community Character and Livability:** Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

**Environmental Impact:** The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council considers the voices of concerned community members before making a final decision. In simple words: don't approve it, we don't want it.

Sincerely,

Rania Tannous  
Niagawa - Dundas resident

**From:** David Yin  
**Sent:** Sunday, February 16, 2025 1:10 PM  
**To:** Town Clerks; Catherine Buckerfield; Jeff Knoll  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West. The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

- **Traffic Congestion and Infrastructure Strain:** The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.
- **Overcrowding and Strain on Public Services:** A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.
- **Community Character and Livability:** Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.
- **Environmental Impact:** The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,  
David Yin



**From:** Song Yingying  
**Sent:** Saturday, February 15, 2025 9:41 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important

ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

Yingying Song

**From:** Sean Dent  
**Sent:** Saturday, February 15, 2025 1:53 PM  
**To:** Scott Xie  
**Subject:** [EXTERNAL] Proposed Development at Neyagawa and Dundas

Dear Scott,

I am a constituent in Ward 7 and voted for you in the last election and am very concerned and against a significant influx of residents and development intended to be crammed into the Dundas/Neyagawa area. I am hoping that as a resident yourself, you are against this proposal? I am also writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. **Traffic Congestion and Infrastructure Strain**  
The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.
2. **Overcrowding and Strain on Public Services**  
A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.
3. **Community Character and Livability**  
Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise

pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that you and the council consider the voices of concerned community members before making a final decision. If you have any suggestions that may aid in rejecting this development, I am eager to contribute.

Sincerely,  
Sean Dent

Hidden Trail Circle

**From:** mary gao  
**Sent:** Saturday, February 15, 2025 9:13 AM  
**To:** Town Clerks; Catherine Buckerfield  
**Subject:** [EXTERNAL] Subject: Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Mary Gao

**From:** Jennifer Fan  
**Sent:** Saturday, February 15, 2025 11:01 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to high-density development at Northwest corner of Neyagawa Blvd and Dundas St W

Dear Town Council Members,

For the recent proposed high-density development at Northwest corner of Neyagawa Blvd and Dundas St W. addressed to 3056 Neyagawa Boulevard, we the people living in the community strongly oppose the project for the following reasons:

7 buildings 2278 new residential units such a high density from the proposed high rises is definitely our primary concern

- This project will worsen traffic congestion, overburden public services
- disrupt the character of our community
- Oakville's infrastructure is not equipped for added pressure which will strain roads, schools, hospital, municipal and emergency services
- the environmental impact on 16 Mile Creek

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability, stability and prosperity.

Sincerely

Jennifer Fan  
Carding Mill Tr  
Oakville L6M 0Z5



**From:** Nan Liu  
**Sent:** Friday, February 14, 2025 8:36 AM  
**To:** Town Clerks; Scott Xie; Nav Nanda; Marc Grant; Jeff Knoll  
**Subject:** Re: [EXTERNAL] Strong Opposition to High-Density Development Proposal \_Neyagawa and Dundas

Thank you for your response!

I have another question. I recall that the original plan for that area was to build a new school but it was cancelled because there was a radio tower that could not be moved. However now, it seems the tower can be moved for the condo plan? I am a little bit confused. Please advise.

Nan Liu  
Daisy Way, Oaville, ON, L6M 1R1

On Fri, Feb 7, 2025 at 10:32 AM Town Clerks <[TownClerk@oakville.ca](mailto:TownClerk@oakville.ca)> wrote:

Good day,

Thank you for contacting the Town of Oakville.

Your correspondence has been forwarded to the appropriate parties for review.

Kind regards,

Laura Pennal

Clerk's Information Administrator

#### **Town Clerks**

Town of Oakville | 905-845-6601 | [www.oakville.ca](http://www.oakville.ca)

**Vision: A vibrant and livable community for all**

Please consider the environment before printing this email.

<http://www.oakville.ca/privacy.html>

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**From:** Nan Liu  
**Sent:** February 7, 2025 7:58 AM  
**To:** Scott Xie <[scott.xie@oakville.ca](mailto:scott.xie@oakville.ca)>; Nav Nanda <[nav.nanda@oakville.ca](mailto:nav.nanda@oakville.ca)>; Marc Grant <[marc.grant@oakville.ca](mailto:marc.grant@oakville.ca)>; Jeff Knoll <[jeff.knoll@oakville.ca](mailto:jeff.knoll@oakville.ca)>; Town Clerks <[TownClerk@oakville.ca](mailto:TownClerk@oakville.ca)>  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal \_Neyagawa and Dundas

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Our kids go to portable classrooms everyday. Traffic on dundas is getting worse and worse. I worked by airport Pearson, I spend the same amount of time on duands as I do on the highway since Dundas traffic is always heavy.

Everyone is expecting the new community center but now there will be 7 condos built around it, I do not think it is safe. I do not think Oakville's infrastructure is equipped for this.

Also, the environmental impact on 16 Mile Creek has not been properly addressed. I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,

Nan Liu

**From:** mingliming2  
**Sent:** Thursday, February 13, 2025 2:16 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

## Appeal email Template

### Subject: Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,

[Your Name]

[Your Contact Information]

Subject: Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate

traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

[Your Name]

[Optional : Your Contact Information]

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,

Ming Li

## **Appeal email Template**

**Subject: Strong Opposition to High-Density Development Proposal**

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,

[Your Name]

[Your Contact Information]

**Subject: Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W**

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

## 2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

## 3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

## 4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

[Your Name]

[Optional : Your Contact Information]

Sent from my Galaxy



**From:** Sylvia Moreira  
**Sent:** Thursday, February 13, 2025 8:05 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. **Traffic Congestion and Infrastructure Strain**  
The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.
2. **Overcrowding and Strain on Public Services**  
A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.
3. **Community Character and Livability**  
Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.
4. **Environmental Impact**  
The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,  
Sylvia Szczepanik-Moreira

**Franca Piazza**

---

**From:** william zhao  
**Sent:** Thursday, February 13, 2025 8:53 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Dear Town Clerk,

As an Oakville citizen, I strongly oppose to the high density apartment plan at 3056 Neyagawa Blvd.

We live in the area and already experience traffic jam during peak hours especially when making a left turn from Dundas to Neyagawa. There's no sufficient infrastructure to support high density population in the area. The plan will make Oakville a less livable place and is a major damage to the master plan of Oakville.

I call for immediate action to review and stop the development plan.

Yours,

William Zhao

**From:** Carol Dirks  
**Sent:** Thursday, February 13, 2025 12:04 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to high-density development at Dundas and Neyagawa

We are writing to strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. The addition of 7 condo towers with 2,278 residential units at the northwest corner of Dundas St West and Neyagawa on a small parcel of land located between Sixteen Mile Sports Complex and Dundas will be a traffic nightmare and a strain on the current infrastructure. Roads, schools, hospitals etc are already overburdened with the extremely dense housing north of Dundas St and could not handle several thousand more people in such a tiny location. In addition, a full environmental study on the impact to 16 Mile Creek will need to be undertaken due to the proximity of the proposed site.

We strongly urge the Town of Oakville to reject the proposed development plan and pursue sustainable growth that preserves the community character and livability for its existing residents.

Sincerely,

Carol & Jurgen Dirks  
Valley Forest Way  
Oakville, ON L6H 6W9

**From:** Daljeet Nagi  
**Sent:** Thursday, February 13, 2025 12:39 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Opposition to development

Dear town council members,

I live on Sixteen Mile Drive. I have become aware of a proposal for significant development at the corner of Neyagawa and Dundas.

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed. I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,

Daljeet and Anterdhyan Nagi

**From:** Essam Azar  
**Sent:** Thursday, February 13, 2025 9:41 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

2.

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

3.

4. Overcrowding and Strain on Public Services

5.

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

6.

7. Community Character and Livability

8.

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

9.

10. Environmental Impact

11.

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council considers the voices of concerned community members before making a final decision. In simple words: don't approve it, we don't want it.

Sincerely,

Essam Azar

Niagawa - Dundas resident



**From:** Grace Eunjung Yang  
**Sent:** Thursday, February 13, 2025 10:06 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Attn: urgent! Our neighbors are against a plan 3056 Neyagawa Boulevard

Dear whom it may concern,

Hello. We are residents in Oakville and writing to have an option against the plan 3055 Neyagawa Boulevard. Majority of my neighbours are against the plan for high-rise community. The new plan is in our community at the northwest corner of Dundas St West and Neyagawa Boulevard. 7 towers from 15 to 28 storeys 2,278 residential units!

That means more than 2,500 additional vehicles and approximately 10,000 new residents to our community in this tiny location.

We are living in Oakville as Oakville is different from other cities in which have high-rise buildings with heavy traffic jams. This current plan will create huge congestion on Dundas, security problem and environmental problems etc.

We are not opposed to developing high-rise buildings for newcomers. However, any new plans must take into account the current situation and needs of the community, rather than solely focusing on the benefits for the developer.

Thank you

Regards,

Grace, Sung, David

나의 iPad에서 보냄

**From:** jenny li  
**Sent:** Thursday, February 13, 2025 11:32 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] 3056 Neyagawa Blvd - OPA1321.02, Z.1321.02, 24T-24006/1321

Dear Oakville Town Officers,

I am writing to express my concerns regarding the development plans for the area to the northwest of Dundas and Neyagawa. When we purchased our home in 2010, the North Oakville development plan indicated that a secondary school would be built in this area. At that time, my daughter was 4 years old, and we were looking forward to her attending this new school. Now, as she is nearing graduation, we have learned that the land previously designated for the school may instead be developed for seven condos.

This change has been very disappointing, as it goes against the expectations we had when moving to this area. I would like to know if the condo development plan has been finalized, or if there are other options being considered for this land. Additionally, I would appreciate it if you could inform me of any public hearings or consultations regarding this development, as I would like to better understand the process and explore opportunities for public input.

Thank you for your time and attention to this matter. I look forward to your response.

Sincerely,

Jenny Li

**From:** John Moreira  
**Sent:** Thursday, February 13, 2025 8:04 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
John Moreira

**From:** liene manor  
**Sent:** Thursday, February 13, 2025 8:20 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Hi there,

I would like to write with my concerns about the proposed high rise tower. I do not support having this added to our community.

I am particularly concerned about the added traffic congestion and overly crowding north Dundas. Builders need to put community needs over profits. It feels like we are filling every square inch of land to the max instead of preserving the beauty of oakville.

Thanks for considering the voices of local residents.

Liene Daley

**From:** wang elaine  
**Sent:** Wednesday, February 12, 2025 11:50 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

- 1. Traffic Congestion and Infrastructure Strain**The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.
- 2. Overcrowding and Strain on Public Services**A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.
- 3. Community Character and Livability**Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.
- 4. Environmental Impact**The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,  
Elaine Wang



**From:** yibin liang  
**Sent:** Wednesday, February 12, 2025 1:52 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Yibin Liang

**From:** g l  
**Sent:** Tuesday, February 11, 2025 3:13 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

Brad Gao

**From:**  
**Sent:** Tuesday, February 11, 2025 4:42 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, medical and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Please continue to focus on making Oakville the most livable city – this proposal will destroy our neighbourhood and city.

Sincerely,  
Jonathan Dodds.

**From:** Sang Yu  
**Sent:** Tuesday, February 11, 2025 8:32 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact

assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

Yu Sang

Address: Sarah Cline Dr, ON L6M 0V7

**From:** Gmail  
**Sent:** Tuesday, February 11, 2025 6:15 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development Proposed 3056 Neyagawa Blvd

**Subject: Strong Opposition to High-Density Development Proposal**

Dear Town Council Members,

As a long time River Oaks citizen, I strongly oppose the proposed high-density development proposed at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. Having experienced the construction noise and sheer volume of traffic with the development on the east side of Neyagawa I have great concerns for local residents for a project of this size and density. Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has still to be properly addressed. I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's residential communities.

Sincerely,  
Sean McBride

Valley Heights Cres  
Oakville, Ont



**From:** S A  
**Sent:** Tuesday, February 11, 2025 4:58 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Hi  
I'm owner and resident of Daniel Clarke Way Oakville and like to cast my strong opposition to this new proposed high-density development at 3056 Neyagawa Blvd

Sohail Ahmed

**From:** Ashish Sharma  
**Sent:** Sunday, February 9, 2025 10:54 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal (Dundas/Neyagawa)

Dear Town of Oakville Clerk and Senior Planner,

I am writing to express my strong opposition to the proposed high-density development at the intersection of Dundas Street West and Neyagawa Boulevard in North Oakville.

While I understand the need for growth, this project raises serious concerns about its impact on our community.

The proposal for seven residential towers ranging from 15 to 28 storeys, with 2,278 residential units, will significantly strain our already burdened infrastructure.

Our roads are already congested, and this development will only exacerbate the problem, despite the planned Dundas BRT.

Furthermore, such a large influx of residents raises security concerns for our neighborhood. The sudden population increase may also overwhelm our community services, which are not equipped to handle such rapid growth.

Additionally, the environmental impact of this development cannot be overlooked. The transition from farmland to high-density urban areas will undoubtedly affect local ecosystems.

While we welcome newcomers and understand the need for housing, any new development must consider the current situation and needs of our community, not just the developer's interests. We urge you to reconsider this proposal and work towards a more balanced approach that aligns with the original vision of accommodating 55,000 people in North Oakville.

Thank you for your attention to this matter. We look forward to a thoughtful reconsideration of this development plan.

Sincerely,

--

Regards,  
Ashish Sharma

Resident of: Dundas ST W Oakville, L6M 5P9

**From:** diane seymour  
**Sent:** Sunday, February 9, 2025 5:54 PM  
**To:** catherine.buckerfield@akville.ca  
**Cc:** Town Clerks  
**Subject:** [EXTERNAL] High density development of 7 buildings Neyagawa Blvd. and Dundas St

I am extremely opposed to this development.

This project would worsen traffic congestion, overburden public services and disrupt the character of the community.

I urge the council to reject this proposal. We have a large investment in our property and this development of extreme high rises is not in keeping with the area.

Diane Seymour  
Resident of the Preserve.

**From:** Faran Ali  
**Sent:** Sunday, February 9, 2025 9:52 PM  
**To:** Town Clerks; Catherine Buckerfield  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Dear Ms Buckerfield & Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. **Traffic Congestion and Infrastructure Strain**  
The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.
2. **Overcrowding and Strain on Public Services**  
A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.
3. **Community Character and Livability**  
Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.
4. **Environmental Impact**  
The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize

responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

Faran Ali

**From:** Frank Gao  
**Sent:** Sunday, February 9, 2025 12:38 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] File No: OPA 1321.02, Z.1321.02 and 24T-24006/1321

Dear Sir or Madam:

As a more than 20 years resident of Oakville. I would like to say NO and STOP:

3056 Neyagawa Boulevard, 7 BUILDINGS PLAN

Thank you,

Frank Gao

**From:**  
**Sent:** Sunday, February 9, 2025 11:48 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed. The residents had to suffer through 3+ years of disruption, congestion and construction during the project to widen Dundas street. The work was done completely backwards and makes me wonder about the engineering and scheduling depts for the City of Oakville. The road widening work should have been completed first to put the infrastructure in place to support the introduction of the residential work. I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Grant Lawes



**From:**  
**Sent:** Sunday, February 9, 2025 11:51 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain  
The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.
2. Overcrowding and Strain on Public Services  
A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.
3. Community Character and Livability  
Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.
4. Environmental Impact  
The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,  
Grant Lawes

**From:** Jamie Parker  
**Sent:** Sunday, February 9, 2025 2:26 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] 3056 Neyagawa Blvd Application to Amend zoning Bylaw

Hello, I am vehemently against the change of this zoning bylaw and permit any high density development subsequently destroying the environment/Lions Valley Park.

Please do not change the zoning bylaw.

Thank you,

Jamie Parker  
Oakville,ON

**From:** Jason Zhou  
**Sent:** Sunday, February 9, 2025 11:41 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough

environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Best Regards

Jason Zhou

**From:** Jenny Zhong  
**Sent:** Sunday, February 9, 2025 6:12 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Jenny Zhong

**From:** LIN Z.  
**Sent:** Sunday, February 9, 2025 8:28 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Members of the Oakville Town Council,

I am writing to express my opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

While I understand the need for development and housing expansion, I strongly believe that this project will lead to severe overcrowding and create significant challenges for our community.

1. This large-scale development will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Our public services are already stretched, My son was not able to go to the local school due to over capacity same as our neighbours. There is no clear plan to support the rapid influx of new residents. The parks were already crowded.
2. Impact on Community Character and Livability. High-density projects like this risk increasing congestion, noise pollution, and reducing green space. There is a large scale 2 stories homes in the area and existing condo build such as District Trailside, Condo at east end of the corner of Dundas and Neyagawa.
3. Environmental Impact: The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

Thank you

Lin Zhang

**From:** Mary McNeil  
**Sent:** Sunday, February 9, 2025 11:37 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Zoning By-law Amendment 3056 Neyagawa Boulevard Proposal

Re: Proposed Official Plan Amendment 7 Plan of Subdivision  
3056 Neyagawa Boulevard  
Neat916 Mile Creek ) Inc.  
OPA1321.02 Z.1321.02 24T-24006/1321, Ward 7

I live 1 block SW of Dundas and Neyagawa.

As a concerned citizen regarding the future development of the NW corner of Dundas & Neyagawa. I would like to say it would be disastrous to change the bylaws in order for this gigantic development to go through. These are the reasons for my disapproval of this project.

1. **Traffic Congestion:** Our neighborhood already experiences significant traffic issues during peak hours. Adding 7 new buildings will undoubtedly increase the number of vehicles on the road, leading to further gridlock, longer commute times, and potential safety hazards for pedestrians and cyclists. Without substantial improvements to the existing infrastructure, this proposal risks making the area nearly impassable.

2. **Insufficient Schools and Facilities:** The current infrastructure, including schools, healthcare facilities, and recreational spaces, is already stretched thin. The influx of new residents from 7 additional buildings will place an even greater strain on these resources. Local schools are already overcrowded, and families are struggling to access essential services. This proposal does not seem to include plans to expand or improve these facilities to accommodate the increased population.

3. **Quality of Life:** The charm and livability of our neighborhood are at risk if this proposal moves forward without addressing these critical issues. Overdevelopment without corresponding infrastructure upgrades will lead to a decline in the quality of life for current and future residents.

I urge the planning committee to reconsider this proposal and prioritize the following:

- Conduct a thorough traffic impact study and implement measures to alleviate congestion before approving any new developments.
- Ensure that schools, healthcare facilities, and recreational spaces are expanded or upgraded to meet the needs of the growing population.
- Engage with the community to develop a balanced approach to growth that preserves the neighborhood's character and livability.

I hope the committee will take a thoughtful and comprehensive approach to ensure that any development benefits all residents and maintains the integrity of our community.

**Regards**

**Mary**



**From:** Neveen Hanna  
**Sent:** Sunday, February 9, 2025 11:13 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

I am writing to express my strong opposition to the proposed high-rise development in Dundas and Neyagawa.

New plans must take into account the needs of the community rather than focusing on the benefits for the developer .. we strongly oppose this idea.

Here are some of the Draw backs

Environmental Impact: Increased density would harm green spaces and worsen pollution

Property Values: The project could reduce the desirability and value of existing homes.

This project would drastically alter the character of our community and negatively impact residents.

Overcrowding: High-rises would strain infrastructure, public services, and traffic systems.

Loss of Character: The development is incompatible with our neighborhood e.g., quiet, family-friendly,

I urge you to reconsider this proposal and prioritize solutions that align with the community's needs and capacity. Please keep me informed of updates or opportunities for public input.

Neveen Hanna

**From:** Steph Girouard  
**Sent:** Sunday, February 9, 2025 4:18 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Stephanie Girouard

**From:** Swapna Sangayya  
**Sent:** Sunday, February 9, 2025 6:58 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision. Sincerely,  
Swapna Sangayya

**From:** Swapna Sangayya  
**Sent:** Sunday, February 9, 2025 7:00 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Swapna Sangayya

**From:** william yu  
**Sent:** Sunday, February 9, 2025 6:08 PM  
**To:** Town Clerks; Catherine Buckerfield  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

**Dear Town Council Members,**

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
William Yu

**From:** Xianghua Qi  
**Sent:** Sunday, February 9, 2025 6:14 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Xianghua Qi



**From:** Amjad Al Bochi  
**Sent:** Saturday, February 8, 2025 7:27 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Subject: Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Amjad Al Bochi

**From:** areen swedan  
**Sent:** Saturday, February 8, 2025 3:40 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents. I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,  
Aree sweidan

**From:** Christopher Wicks  
**Sent:** Saturday, February 8, 2025 11:14 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Subject: Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

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Dear Town Council Members,

We strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. Oakville's infrastructure is not equipped for 2,278 new residential units in such a small area, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed. The density in Oakville north of Dundas is getting to the point where there will be more population than in the rest of Oakville!

We strongly urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Christopher and Gina Wicks  
valley forest way  
Oakville ON L6H 6W8

**From:** Frank Mogus  
**Sent:** Saturday, February 8, 2025 1:13 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important

ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

Frank Mogus

Resident of Oakville

**From:** Glenn  
**Sent:** Saturday, February 8, 2025 3:39 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Re: Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Hi

I recently acknowledged the proposed project of 7 high-density buildings in our neighborhood.

I am writing to let you know my strong opposition to this development. This development threatens the unique character of our community and poses several concerns that I believe must be addressed before moving forward with such a project.

Firstly, the increased population density that comes with high-rise condos would put an undue strain on our already limited infrastructure, including roads, utilities, and public services. Traffic congestion is a growing problem, and further development would only exacerbate this issue, making it even more difficult for residents to commute safely and efficiently.

Additionally, the construction of high-rise buildings would negatively impact the aesthetic appeal of our neighborhood. The current low-rise character of our area is one of the features that makes it an attractive place to live, and introducing large, dense buildings would drastically alter the visual landscape and sense of community.

Environmental concerns are also paramount. Increased construction and population density could lead to a loss of green space, negatively affecting local wildlife and reducing opportunities for recreation. Furthermore, the development's environmental footprint, both during construction and in the long term, could contribute to greater pollution and reduced quality of life for residents.

Finally, we must consider the social impact. High-density developments tend to increase the cost of living in the surrounding area, displacing long-time residents and altering the social fabric of the community. Our neighborhood should remain a place where people of all backgrounds and income levels can live comfortably, not one that becomes unaffordable to many due to rapid, profit-driven development.

I urge you to reconsider this proposal and engage in more community-driven discussions about sustainable and responsible development that better aligns with the values and needs of current residents. Our neighborhood deserves thoughtful, balanced planning that respects its history and ensures a high quality of life for all who live here.

Thank you for your time and attention to this matter. I hope to see a more suitable, community-oriented development proposal in the future.

Sincerely,  
Glenn Liao



**From:** Helen Zhao  
**Sent:** Saturday, February 8, 2025 10:30 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] My Concerns Regarding the Proposal to Add 7 Buildings on a small intersection of Dundas and Neyagawa

Dear neighbours and members at the town of Oakville

I am writing to express my concerns regarding the proposed addition of 7 new buildings. While development and growth are important, I believe this proposal does not adequately address the existing challenges in our area, particularly in terms of traffic congestion, school capacity, and access to essential facilities.

1. **Traffic Congestion:** Our neighborhood already experiences significant traffic issues during peak hours. Adding 7 new buildings will undoubtedly increase the number of vehicles on the road, leading to further gridlock, longer commute times, and potential safety hazards for pedestrians and cyclists. Without substantial improvements to the existing infrastructure, this proposal risks making the area nearly impassable.
2. **Insufficient Schools and Facilities:** The current infrastructure, including schools, healthcare facilities, and recreational spaces, is already stretched thin. The influx of new residents from 7 additional buildings will place an even greater strain on these resources. Local schools are already overcrowded, and families are struggling to access essential services. This proposal does not seem to include plans to expand or improve these facilities to accommodate the increased population.
3. **Quality of Life:** The charm and livability of our neighborhood are at risk if this proposal moves forward without addressing these critical issues. Overdevelopment without corresponding infrastructure upgrades will lead to a decline in the quality of life for current and future residents.

I urge the planning committee to reconsider this proposal and prioritize the following:

- Conduct a thorough traffic impact study and implement measures to alleviate congestion before approving any new developments.
- Ensure that schools, healthcare facilities, and recreational spaces are expanded or upgraded to meet the needs of the growing population.
- Engage with the community to develop a balanced approach to growth that preserves the neighborhood's character and livability.

I hope the committee will take a thoughtful and comprehensive approach to ensure that any development benefits all residents and maintains the integrity of our community.

Sincerely,

Hui Zhao

Sent from my iPhone

**From:** Nicole <  
**Sent:** Saturday, February 8, 2025 2:01 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Regards ,  
Nicole

**From:** Nishant Taneja  
**Sent:** Saturday, February 8, 2025 12:34 AM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield; Preeti  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal (Dundas/Neyagawa)

Dear Town of Oakville Clerk and Senior Planner,

I am writing to express my strong opposition to the proposed high-density development at the intersection of Dundas Street West and Neyagawa Boulevard in North Oakville.

While I understand the need for growth, this project raises serious concerns about its impact on our community.

The proposal for seven residential towers ranging from 15 to 28 storeys, with 2,278 residential units, will significantly strain our already burdened infrastructure.

Our roads are already congested, and this development will only exacerbate the problem, despite the planned Dundas BRT.

Furthermore, such a large influx of residents raises security concerns for our neighborhood. The sudden population increase may also overwhelm our community services, which are not equipped to handle such rapid growth.

Additionally, the environmental impact of this development cannot be overlooked. The transition from farmland to high-density urban area will undoubtedly affect local ecosystems.

While we welcome newcomers and understand the need for housing, any new development must consider the current situation and needs of our community, not just the developer's interests. We urge you to reconsider this proposal and work towards a more balanced approach that aligns with the original vision of accommodating 55,000 people in North Oakville.

Thank you for your attention to this matter. We look forward to a thoughtful reconsideration of this development plan.

Sincerely,  
Nishant Taneja & Preeti Wadhawan  
Resident of: Robert Brown Blvd  
Oakville L6M 0P2

**From:** Danielle Grenier  
**Sent:** Friday, February 7, 2025 10:52 AM  
**To:** Town Clerks  
**Subject:** Re: [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Thank you. I would like to add that it is becoming very challenging to park in the Fortinos plaza. I can't imagine another 2000 families using that grocery store. There is not enough infrastructure to accommodate such a large amount of new residents. This is very disappointing from Oakville.

On Feb 7, 2025, at 10:03, Town Clerks wrote:

Good day,  
Thank you for contacting the Town of Oakville.  
Your correspondence has been forwarded to the appropriate parties for review.  
Kind regards,  
Laura Pennal  
Clerk's Information Administrator

### **Town Clerks**

Town of Oakville | 905-845-6601 | [www.oakville.ca](http://www.oakville.ca)

**Vision: A vibrant and livable community for all**

Please consider the environment before printing this email.

<http://www.oakville.ca/privacy.html>

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**From:** Danielle Grenier  
**Sent:** February 6, 2025 7:19 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

- 1. Traffic Congestion and Infrastructure Strain**

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. It can take 2-3 lights in order to be able to turn. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

- 2. Overcrowding and Strain on Public Services**

A development of this magnitude will put immense pressure on local schools, healthcare

facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents. It is already difficult to access a Family doctor, go to a walk-in clinic and get treated at the hospital emergency. This project shows a complete disregard of the people and community of Oakville.

3. **Community Character and Livability**

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents. Years ago the area was peaceful and it is not longer that way. Sixteen Mile Creek Conservation area is already overcrowded. If I recall properly I am paying an extra tax for this Park which has been taken over by large crowds. Adding 2300 units won't help that for sure. Oakville is becoming like Markham where everyone is stuck on the roads in back to back traffic.

4. **Environmental Impact**

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

Danielle Grenier

Valley Stream Place, Oakville

**From:** Dominic Wang  
**Sent:** Friday, February 7, 2025 11:12 PM  
**To:** Town Clerks; Mayor Rob Burton; Scott Xie  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

The left turn time at the intersection of Neyagawa and Dundas to Neyagawa north costs 7-9 minutes every evening at the rush hours for now. It is easy to understand that after this proposal, the traffic will get even worse.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

Oakville town should build more community centres with libraries, multi-used gyms and swimming pools in the northern area according to the residents' numbers, but not more high-density condos. The density for the whole northern area is already very high. Oakville town should not make the density rate for the northern area much more higher.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Dominic Wang

**From:** Gord Davies  
**Sent:** Friday, February 7, 2025 12:51 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

My name is Gord Davies. I live in this area. I am on Valley Forest Way. I oppose this development because of the high volume of people that will live here.

We have major traffic problems now as well as congestion. It's a mess in the morning and afternoon.

Gord Davies



**From:** Helen He  
**Sent:** Friday, February 7, 2025 8:39 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL]

**Subject: Strong Opposition to High-Density Development Proposal**

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Helen H

**Subject: Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W**

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools,

healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

Helen H

## **Appeal email Template**

**Subject: Strong Opposition to High-Density Development Proposal**

## **Appeal email Template**

**Subject: Strong Opposition to High-Density Development Proposal**

**From:** Joyce Feng  
**Sent:** Friday, February 7, 2025 6:59 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Oppose the high density development

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed. I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Joyce

**From:** Julio Amorim  
**Sent:** Friday, February 7, 2025 1:36 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Kind Regards

Julio Amorim

**From:** Le Xing  
**Sent:** Friday, February 7, 2025 10:46 PM  
**To:** Nav Nanda; Marc Grant; Jeff Knoll; Town Clerks; Scott Xie  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal \_Neyagawa and Dundas

Dear Town Council Members,

I am writing to express my strong opposition to the proposed high-density development at Neyagawa Blvd and Dundas St W. I believe this project will significantly increase traffic congestion, place undue pressure on public services, and alter the unique character of our community.

The local school cannot handle the demand at all. My son has not been in a proper classroom in his 4 years at school. He has always studied in one of the portables. Is this really an ideal environment for a child growing up? The lack of space and resources is concerning, and it's unfair to our children.

There are also many other concerns, as everyone is aware, such as traffic, safety, and the environment- there has been too much construction around 16 Mile Creek in recent years. I strongly encourage the council to reject this proposal and prioritize growth that ensures Oakville remains a livable and thriving community.

Thank you,

Adam Xing

**From:** Margaret Gupta  
**Sent:** Friday, February 7, 2025 1:21 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Living at the intersection of Robert Brown. There is already congestion in this area. To make a left turn onto Neyagawa Blvd. is already a problem for morning traffic. Are you aware of when the Sports complex is in operation what the problems will be then?

As my home is on corner of Sixteen Mile and Robert Brown Blvd. The Traffic fumes are dreadful with cars idling while waiting to turn left at the lights. Please be considerate of the locals. I also get cars parked by people going to Fortinos to shop. Must also let you know that garbage is left by these car occupants. That being plastic bottles and cans. A suitable sign like Do Not Litter may work.

No more High Rise Please, Let these builders go further North.

Enjoy your Day.

Margaret Gupta



**From:** Mary Brozovic  
**Sent:** Friday, February 7, 2025 1:12 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

This is absolutely not acceptable, this is not Oakville, let's preserve what is still here Sent from my iPhone

**From:** Nan Liu  
**Sent:** Friday, February 7, 2025 7:58 AM  
**To:** Scott Xie; Nav Nanda; Marc Grant; Jeff Knoll; Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal \_Neyagawa and Dundas

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Our kids go to portable classrooms everyday. Traffic on dundas is getting worse and worse. I worked by airport Pearson, I spend the same amount of time on duands as I do on the highway since Dundas traffic is always heavy.

Everyone is expecting the new community center but now there will be 7 condos built around it, I do not think it is safe. I do not think Oakville's infrastructure is equipped for this.

Also, the environmental impact on 16 Mile Creek has not been properly addressed. I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,

Nan Liu

**From:** QING YE  
**Sent:** Friday, February 7, 2025 2:55 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Subject: Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

qing ye

**From:** Abed Fayyad  
**Sent:** Thursday, February 6, 2025 5:41 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield; Nina Demurcheva  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Dear Members of the Oakville Town Council,

I am writing to formally express my **strong** opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

**Traffic Congestion and Infrastructure Strain**

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

**Overcrowding and Strain on Public Services**

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

**Community Character and Livability**

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

**Environmental Impact**

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

Abed Fayyad

Neyagawa Blvd

**From:** Alina Rashid  
**Sent:** Thursday, February 6, 2025 7:08 PM  
**To:** Town Clerks; Catherine Buckerfield  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed. I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Alina Rashid

**From:** ch amanda  
**Sent:** Thursday, February 6, 2025 11:23 AM  
**To:** scott.xie@aokville.ca; Nav Nanda; Marc Grant; jeff.knoff@oakville.ca; Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. The left turn time at the intersection of Neyagawa and Dundas to Neyagawa north costs 7-9 minutes every evening at the rush hours for now. It is easy to understand that after this proposal, the traffic will get even worse.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

Oakville town should build more community centres with libraries, multi-used gyms and swimming pools in the northern area according to the residents' numbers, but not more high-density condos. The density for the whole northern area is already very high. Oakville town should not make the density rate for the northern area much more higher.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Best Regards,

Amanda Chen  
Resident of Oakville for 18 years

**From:** Anthony Sestanovic  
**Sent:** Thursday, February 6, 2025 5:44 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Hello,

I am a resident of Valley Heights Crescent, and after hearing about the proposed development at the corner of Dundas and Neyagawa Boulevard, I wanted to express my concerns. I am deeply disappointed by the potential impact this project could have on our community. Some of the key issues I have are:

1. **Traffic and Infrastructure:** The intersection at Dundas and Neyagawa is already congested, and the current infrastructure is not equipped to handle a significant increase in traffic or accommodate the many new residents this development would bring to the area.
2. **Preservation of Oakville's Character:** As a lifelong resident of Oakville, I am strongly opposed to the addition of high-rise developments in our city. Oakville has traditionally been known for its suburban charm, featuring detached homes and spacious properties. The shift towards high-density living is fundamentally at odds with the character that makes Oakville unique and desirable.
3. **Safety Concerns:** Given the rising security concerns in the area, I am uneasy about the impact of such a dense development. Adding a large number of new residents could exacerbate safety issues and further strain our local resources.

In conclusion, I believe this proposed development is far too dense for the area and would place undue strain on our infrastructure and community. As a member of the Valley View neighborhood for my entire life, I am strongly opposed to this project and do not believe it should move forward.

Thank you for your time and consideration.

Sincerely,  
Anthony Sestanovic



**From:** ava\_kidman  
**Sent:** Thursday, February 6, 2025 11:41 AM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Ava Duan

**From:** Ben Li <  
**Sent:** Thursday, February 6, 2025 11:08 PM  
**To:** Jeff Knoll; Marc Grant; Mayor Rob Burton  
**Cc:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Mayor, Councilor Knoll and Councilor Grant,

I am Oakville resident in Ward 5 near Neyagawa & Dundas. I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

**1. Traffic Congestion and Infrastructure Strain**

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

**2. Overcrowding and Strain on Public Services**

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

**3. Community Character and Livability**

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

**4. Environmental Impact**

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

Ben Li

**From:** Brian Gibson  
**Sent:** Thursday, February 6, 2025 4:23 PM  
**To:** Town Clerks  
**Cc:** Deborah A. Gibson  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Dear Town Council Members,

We strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. Oakville's infrastructure is not equipped for 2,278 new residential units in such a small area, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed. The density in Oakville north of Dundas is getting to the point where there will be more population than in the rest of Oakville!

We strongly urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Brian and Deborah Gibson  
Riverbank Way

Oakville ON L6H 7P2

**From:** Cindy Chen  
**Sent:** Thursday, February 6, 2025 7:52 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  Steven Song

**From:** Danielle Grenier  
**Sent:** Thursday, February 6, 2025 7:19 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

**1. Traffic Congestion and Infrastructure Strain**

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. It can take 2-3 lights in order to be able to turn. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

**2. Overcrowding and Strain on Public Services**

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents. It is already difficult to access a Family doctor, go to a walk-in clinic and get treated at the hospital emergency. This project shows a complete disregard of the people and community of Oakville.

**3. Community Character and Livability**

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents. Years ago the area was peaceful and it is not longer that way. Sixteen Mile Creek Conservation area is already overcrowded. If I recall properly I am paying an extra tax for this Park which has been taken over by large crowds. Adding 2300 units won't help that for sure. Oakville is becoming like Markham where everyone is stuck on the roads in back to back traffic.

**4. Environmental Impact**

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

Danielle Grenier

Valley Stream Place, Oakville

**From:** Eden Wang  
**Sent:** Thursday, February 6, 2025 12:49 PM  
**To:** Town Clerks; Catherine Buckerfield  
**Cc:** Marc Grant; Jeff Knoll  
**Subject:** [EXTERNAL] Concerns Regarding High-Density Development Proposal in 3056 Neyagawa Boulevard

Dear Ward5 Councillors and Town Planner,

As a resident of Ward 5, I am writing to express my concerns regarding the proposed high-density residential development at Neyagawa Blvd and Dundas St W. While I appreciate the need for thoughtful urban planning, I strongly believe that this project, as currently proposed, is not in the best interest of our community.

The addition of 2,278 residential units will significantly strain existing infrastructure, exacerbate traffic congestion, and put immense pressure on schools, healthcare, and emergency services. Our community is already experiencing these challenges, and such a large-scale development will only intensify them.

Furthermore, I urge the council to consider alternative uses for this land that would better serve the needs of Oakville residents. Expanding green spaces, parks, and public service facilities such as community centers and libraries would greatly enhance the quality of life in our neighbourhood. Preserving natural areas and promoting sustainable development should be key priorities.

Additionally, I kindly request a clear update on the current status of this project, including the next steps in the approval process and a detailed timeline. Specifically, I would like to know when public hearings or consultations will take place so that residents have an opportunity to voice their concerns.

I kindly ask for your support in advocating for a more balanced and environmentally responsible plan that prioritizes green spaces and public services over high-density residential expansion. Thank you for your time and dedication to representing the interests of Ward 5 residents. I look forward to your response and a detailed update on this matter.

Sincerely,  
Eden Wang  
A resident in Ward5 Oakville

**From:**  
**Sent:** Thursday, February 6, 2025 1:43 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strongly oppose the proposed

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,

Haifanh Jin



**From:** Jamie Parker  
**Sent:** Thursday, February 6, 2025 8:22 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

ps. There is an accident on that intersection monthly at least - poor planning.

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed. I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,

Jamie Parker

**From:** Kelly Xu  
**Sent:** Thursday, February 6, 2025 11:30 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] opposition to high rise development

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

The left turn time at the intersection of Neyagawa and Dundas to Neyagawa north costs 7-9 minutes every evening at the rush hours for now. It is easy to understand that after this proposal, the traffic will get even worse.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

Oakville town should build more community centres with libraries, multi-used gyms and swimming pools in the northern area according to the residents' numbers, but not more high-density condos. The density for the whole northern area is already very high. Oakville town should not make the density rate for the northern area much more higher.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Kelly  
Sent from my iPhone

**From:** Max McInnis  
**Sent:** Thursday, February 6, 2025 9:27 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

The planned high-density construction near Neyagawa Blvd and Dundas St W is something I passionately oppose. Public services will be overworked, traffic congestion will intensify, and our community's character will be disrupted by this project. Roads, schools, and emergency services will be strained by the 2,278 more residential units that Oakville's infrastructure cannot handle. Furthermore, the effects on 16 Mile Creek's ecosystem have not been adequately addressed. In order to maintain Oakville's livability, I urge the council to reject this proposal and give sustainable growth top priority.

Sincerely,  
Maxwell McInnis

**From:** May He  
**Sent:** Thursday, February 6, 2025 3:00 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Opposition to Development Plans

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. The left turn time at the intersection of Neyagawa and Dundas to Neyagawa north costs 7-9 minutes every evening at the rush hours for now. It is easy to understand that after this proposal, the traffic will get even worse.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed. Oakville town should build more community centres with libraries, multi-used gyms and swimming pools in the northern area according to the residents' numbers, but not more high-density condos. The density for the whole northern area is already very high. Oakville town should not make the density rate for the northern area much more higher.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Mei

**From:** Monica Bourque  
**Sent:** Thursday, February 6, 2025 3:35 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal - Development Application 3056 Neyagawa Boulevard File No: OPA 1321.02, Z.1321.02 and 24T-24006/1321

The Town Clerk at the Town of Oakville,  
Clerk's department,  
1225 Trafalgar Road,  
Oakville, ON L6H 0H3

Dear Town Council Members,

**Subject: Strong Opposition to High-Density Development Proposal regarding to**

*Proposed Official Plan Amendment, Zoning By-law Amendment and Draft Plan of Subdivision for a high-density mixed-use development comprising of 7 buildings in 3 development blocks. The proposal is for 2,278 residential units as well 1,551 square metres of retail space, in addition to new public street conveyances.*

We strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. Currently, Dundas Street West is under heavy traffic. Safety is also a concern when more people move from different areas and the crime rate may increase. Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability. We have been living in Oakville for close to 30-years. We don't want to see Oakville follow Mississauga's footsteps.

Sincerely yours,

**Monica & Gary Bourque**

Address: Sixteen Mile Drive, Oakville, ON L6MoP8

Email:

**From:** Nina Demurcheva  
**Sent:** Thursday, February 6, 2025 10:40 AM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Opposition to High-Density Apartment Development in 3056 Neyagawa Blvd

Dear Major, the Members of the Oakville Town Council, city planners and other relevant people

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,  
Nina Demurcheva  
Neyagawa Blvd

**From:** richard han  
**Sent:** Thursday, February 6, 2025 2:05 PM  
**To:** Town Clerks  
**Cc:** Catherine Buckerfield  
**Subject:** [EXTERNAL] Strong Opposition to the High-Density condo proposal

Dear Town Council Members,

I am an Oakville resident living in the River Oaks Community. I notice that this is a proposed high-density condo development project at the cross of Neyagawa Blvd and Dundas St W. This project will have 2300 units within a very limited area.

I am writing this letter to express my strong opposition to this project which will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Our community's infrastructure will not be able to accommodate additional 2,278 new residential units. The project will strain roads, schools, and emergency services.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Xinlin Han

**From:** Talia Valentine  
**Sent:** Thursday, February 6, 2025 1:05 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely,  
Talia Valentine



**From:** Talia Valentine  
**Sent:** Thursday, February 6, 2025 1:06 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W

Dear Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental

impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,

Talia Valentine

**From:** Tom Zhang  
**Sent:** Thursday, February 6, 2025 9:06 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I am writing to express my strong opposition to the proposed high-density development at Neyagawa Blvd and Dundas St W. This project poses serious concerns regarding traffic congestion, public services, and the overall character of our community.

Oakville's infrastructure is not equipped to handle 2,278 new residential units. Our roads are already congested, and this development will significantly increase traffic delays. Furthermore, the potential environmental impact on 16 Mile Creek has not been adequately addressed, raising concerns about long-term sustainability.

I urge the council to reject this proposal and instead focus on responsible, sustainable growth that aligns with Oakville's commitment to livability and smart urban planning. Our community deserves thoughtful development that enhances, rather than diminishes, our quality of life.

Thank you for your time and consideration.

Sincerely,

Li Zhong Zhang  
Valley Heights Crescent, Oakville, ON L6H 4X2

**From:** Wendy Ruan  
**Sent:** Thursday, February 6, 2025 9:09 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

Dear Town Council Members,

I am writing to voice my strong opposition to the proposed high-density development at Neyagawa Blvd and Dundas St W. This project raises serious concerns about traffic congestion, overburdened public services, and the long-term impact on our community's character and environment.

Oakville's infrastructure is already under strain and is not equipped to support an additional 2,278 residential units. Increased traffic will further clog our roads, leading to longer commute times and heightened safety risks. Schools and emergency services, which are already operating at capacity, will face even greater pressure. Moreover, the environmental impact on 16 Mile Creek has not been thoroughly assessed, posing potential risks to local ecosystems and long-term sustainability.

I urge the council to reject this proposal and instead advocate for responsible, well-planned development that aligns with Oakville's vision for smart growth and livability. Our community deserves thoughtful planning that enhances—not compromises—our quality of life.

Thank you for your time and consideration. I trust you will make a decision that prioritizes the well-being of Oakville's residents and future generations.

Sincerely,

Wen Ruan  
Valley Heights Crescent, Oakville, ON L6H 6X2

**From:** wendy weng  
**Sent:** Thursday, February 6, 2025 7:08 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Appeal regarding the high-rise condo

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

The left turn time at the intersection of Neyagawa and Dundas to Neyagawa north costs 7-9 minutes every evening at the rush hours for now. It is easy to understand that after this proposal, the traffic will get even worse.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

Oakville town should build more community centres with libraries, multi-used gyms and swimming pools in the northern area according to the residents' numbers, but not more high-density condos. The density for the whole northern area is already very high. Oakville town should not make the density rate for the northern area much more higher.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability. Thank you!

Best Rgds!  
Wendy

**From:** yan bin  
**Sent:** Thursday, February 6, 2025 11:52 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W.

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

The left turn time at the intersection of Neyagawa and Dundas to Neyagawa north costs 7-9 minutes every evening at the rush hours for now. It is easy to understand that after this proposal, the traffic will get even worse.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

Oakville town should build more community centres with libraries, multi-used gyms and swimming pools in the northern area according to the residents' numbers, but not more high-density condos. The density for the whole northern area is already very high. Oakville town should not make the density rate for the northern area much more higher.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

**From:** Asaf Maruf  
**Sent:** Wednesday, February 5, 2025 7:16 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Subject: Strong Opposition to High-Density Development Proposal

Dear Town Council Members,  
I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed. I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.  
Sincerely,  
Asaf Maruf

**Subject: Objection to High-Density Development Proposal at Neyagawa Blvd & Dundas St W**

Dear Members of the Oakville Town Council,  
I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West. The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate

traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject or significantly revise the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council consider the voices of concerned community members before making a final decision.

Sincerely,  
Asaf Maruf



**From:** Fern Jacob (CA)  
**Sent:** Wednesday, February 5, 2025 8:38 AM  
**To:** Town Clerks  
**Cc:**  
**Subject:** [EXTERNAL] Resident Concerns on the Proposed Plan Amendment - 3056 Neyagawa Boulevard

**Attention City of Oakville**  
**Regarding: Proposed Plan Amendment - 3056 Neyagawa Boulevard**

**Resident Concerns:**

To whom it may concern,

As a resident who lives on Riverbank Way, which is a mature neighborhood with beautiful green space and the sixteen-mile creek, I am extremely concerned about the proposed development to build another high-rise building and commercial space in the northwest corner of Neyagawa & Dundas for the following reasons:

- Dundas Street is already congested, and further widening is costly and challenging.
- Dundas and Neyagawa pedestrian safety concerns with increased traffic and more cars on the road at this intersection.
- With a high population in one concentrated area, there are security concerns for our communities.
- Strain on the infrastructure in an already highly concentrated area, which already has several high-rise development buildings that have gone up in the last couple of years.
- Environmental concerns, city views, and congestion/pollution concerns.
- Pressure on community services.

I am not opposed to developing homes for newcomers to our communities. What I am opposed to is high-rise buildings of increased size and magnitude in suburban residential spaces for the above reasons. The new planning should take account of the needs of the community and its residents and not solely focus on the needs and financial benefits that this brings to developers.

I believe it is important to preserve the character of Oakville & Dundas. I would welcome a discussion on alternative solutions that address both development and community concerns. I request that you take my views and other community member views into consideration.

Kind regards,

Fern Jacob  
Resident of Riverbank Way

**From:** Ivy W  
**Sent:** Wednesday, February 5, 2025 7:48 PM  
**To:** Town Clerks; Scott Xie  
**Cc:** seniorplanner@oakville.ca  
**Subject:** [EXTERNAL] Strong Opposition to Proposed High-Density Condo Development

Good evening,

I recently acknowledged the proposed project of 7 high-density buildings in our neighborhood.

I am writing to let you know my strong opposition to this development. This development threatens the unique character of our community and poses several concerns that I believe must be addressed before moving forward with such a project.

Firstly, the increased population density that comes with high-rise condos would put an undue strain on our already limited infrastructure, including roads, utilities, and public services. Traffic congestion is a growing problem, and further development would only exacerbate this issue, making it even more difficult for residents to commute safely and efficiently.

Additionally, the construction of high-rise buildings would negatively impact the aesthetic appeal of our neighborhood. The current low-rise character of our area is one of the features that makes it an attractive place to live, and introducing large, dense buildings would drastically alter the visual landscape and sense of community.

Environmental concerns are also paramount. Increased construction and population density could lead to a loss of green space, negatively affecting local wildlife and reducing opportunities for recreation. Furthermore, the development's environmental footprint, both during construction and in the long term, could contribute to greater pollution and reduced quality of life for residents.

Finally, we must consider the social impact. High-density developments tend to increase the cost of living in the surrounding area, displacing long-time residents and altering the social fabric of the community. Our neighborhood should remain a place where people of all backgrounds and income levels can live comfortably, not one that becomes unaffordable to many due to rapid, profit-driven development.

I urge you to reconsider this proposal and engage in more community-driven discussions about sustainable and responsible development that better aligns with the values and needs of current residents. Our neighborhood deserves thoughtful, balanced planning that respects its history and ensures a high quality of life for all who live here.

Thank you for your time and attention to this matter. I hope to see a more suitable, community-oriented development proposal in the future.

Sincerely,

**From:** Kai Deng  
**Sent:** Wednesday, February 5, 2025 6:02 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density

Development Proposal Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed. I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

Sincerely, Kai

**From:** Mustafa Zaidi  
**Sent:** Wednesday, February 5, 2025 6:37 PM  
**To:** Town Clerks; Catherine Buckerfield  
**Subject:** [EXTERNAL] Strong Vehement Opposition to Density Development Proposal by a family in Oakville already feeling crowded and underserved!!

Dear Town Council Members,

I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community.

Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed.

I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

We have been refined three times for schooling, face enormous wait times in hospitals, and are scared out of our wits with the level of car thefts and crime that has come into our community! We can not handle any more development here!

Sincerely,

Mustafa Zaidi  
North Oakville Resident, near Neyagwa/Dundas



Project Name: 3056 Neyagawa Boulevard  
DOCA Project Number: 2024-0146  
Proponent: Neatt (16 Mile Creek) Inc.  
Project Location: 3056 Neyagawa Boulevard  
Approval Authority: Town of Oakville

Dear Franca Piazza,

This letter is to confirm receipt of the project-related correspondence sent by Town of Oakville, on January 31, 2025, regarding 3056 Neyagawa Boulevard.

The Mississaugas of the Credit First Nation (MCFN) are the Treaty Holders of the land on which the project will take place – specifically, the Head of the Lake Treaty No. 14. The MCFN holds Indigenous and Treaty Rights specific to the project location and its environs, which may be adversely impacted by it. The Department of Consultation and Accommodation (DOCA) is designated by the MCFN to handle consultation matters on its behalf.

The DOCA consultation team has filed the project-related correspondence identified above. Upon our review, we have determined that **the project should be proceeded by, at minimum, a Stage 1 archaeological assessment** to determine the location's archaeological potential. DOCA must be notified of, invited to participate in, and provided the opportunity to review this archaeological assessment, as well as any environmental assessments. At its discretion, DOCA may request capacity funding from the proponent for its consultation and engagement activities relating to the project.

DOCA expects to be notified of any and all future project updates and/or changes.

If you have any questions for the DOCA consultation team, please feel free to contact us.

Thank you,  
Payton Mitchell  
Consultation Assistant  
Department of Consultation and Accommodation  
Phone: 905-768-4260  
Email: [payton.mitchell@mncfn.ca](mailto:payton.mitchell@mncfn.ca)  
CC Megan DeVries, Manager of Consultations, [megan.devries@mncfn.ca](mailto:megan.devries@mncfn.ca)



Mississaugas of the Credit First Nation  
2789 Mississauga Road, Hagersville, Ontario N0A 1H0



Phone: (905) 768-1133  
Fax: (905) 768-1225



**From:** Sean Dent  
**Sent:** Wednesday, February 5, 2025 7:56 PM  
**To:** Town Clerks; seniorplanner@oakville.ca  
**Subject:** [EXTERNAL] Seriously? Town Council - Strong objection to Development Proposal at Neyagawa and Dundas

Dear our ELECTED Members of the Oakville Town Council,

I am writing to formally express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the development of a high-density mixed-use project at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposal, which includes the construction of seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, raises significant concerns for our community. The following issues must be addressed before any approval is granted:

1. Traffic Congestion and Infrastructure Strain

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested. The addition of over 2,000 residential units will further exacerbate traffic issues, leading to increased commute times, safety hazards, and environmental impact. Existing road infrastructure is not designed to handle this volume of additional residents and commercial activity.

2. Overcrowding and Strain on Public Services

A development of this magnitude will put immense pressure on local schools, healthcare facilities, emergency services, and public utilities. Current public services are already stretched, and the town has not presented adequate plans to accommodate this rapid influx of new residents.

3. Community Character and Livability

Oakville is known for its balanced urban planning, green spaces, and suburban charm. High-density developments of this scale risk undermining the town's character by introducing congestion, noise pollution, and reduced green space. The proposal lacks sufficient open spaces, parks, and recreational areas necessary to support the well-being of future and existing residents.

#### 4. Environmental Impact

The development is near Sixteen Mile Creek Conservation area, an important ecological area. Increased construction, pollution, and stormwater runoff could negatively impact local wildlife and water quality. A thorough environmental impact assessment must be conducted and reviewed by independent experts before proceeding.

In light of these concerns, I strongly urge the Town of Oakville to reject the proposed development plan. Sustainable, well-planned growth should prioritize responsible infrastructure expansion, environmental conservation, and maintaining the quality of life for current residents.

I appreciate your attention to this matter and request that the council absolutely consider the voices of concerned community members before making a final decision.

Sincerely,

Sean and Heidi Dent

Hidden Trail Circle

Oakville, ON

**From:** 马天舒  
**Sent:** Wednesday, February 5, 2025 8:14 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development at Neyagawa Blvd & Dundas St W

Dear Members of the Oakville Town Council,

I am writing to express my strong opposition to the proposed official plan amendment, zoning by-law amendment, and plan of subdivision submitted by Neat (16 Mile Creek) Inc. for the high-density mixed-use development at the northwest corner of Neyagawa Boulevard and Dundas Street West.

The proposed development, which includes seven buildings comprising 2,278 residential units and 1,551 square meters of retail space, poses significant concerns that must be addressed before any approval is considered.

### **1. Traffic Congestion and Infrastructure Strain**

The intersection of Neyagawa Blvd and Dundas St W is already heavily congested, with frequent delays and safety hazards for drivers and pedestrians. Adding over 2,000 residential units will drastically worsen traffic conditions, leading to gridlock and increased risks for all road users. The existing road infrastructure is not designed to handle such a dramatic increase in vehicle volume, and without substantial upgrades to roadways and public transit, this development will create long-term transportation challenges.

### **2. Overburdened Public Services**

Oakville's schools, healthcare facilities, and emergency services are already operating at or near capacity. A development of this scale will place an unsustainable burden on these critical resources, compromising the quality of education, healthcare, and emergency response times. Without a clear and well-funded plan to expand these services in alignment with the population increase, the well-being of current and future residents will be at risk.

### **3. Incompatibility with Surrounding Neighborhoods**

The proposed high-density development is inconsistent with the character of the surrounding neighborhoods, which primarily consist of low- and medium-density residential areas. The dramatic increase in population density will alter the suburban character of the community and could negatively impact property values. New developments should be planned in a way that harmonizes with the existing scale and aesthetic of the area.

### **4. Environmental and Green Space Concerns**



This project raises serious environmental concerns, including increased pollution, loss of green space, and potential strain on local ecosystems. Oakville has long been committed to sustainable and responsible urban planning, and any new development must prioritize environmental preservation, green spaces, and adequate recreational areas for residents.

## **5. Lack of Meaningful Community Consultation**

Many residents feel that there has been insufficient consultation and transparency regarding this proposal. A development of this size requires thorough engagement with the community to ensure that residents' voices are heard and their concerns are addressed. Major zoning and density changes should not be approved without significant input from those who will be most affected.

## **Conclusion: Prioritize Responsible Growth**

I urge the Oakville Town Council to reject this proposal in its current form and instead pursue a development plan that aligns with the town's long-term vision, infrastructure capacity, and the needs of existing residents. Sustainable, thoughtful growth is essential to preserving Oakville's high quality of life.

Thank you for your time and consideration. I appreciate your dedication to responsible urban planning and look forward to your response.

**Sincerely,**

Tianshu(Tony) Ma

Hidden Trail, Oakville,

L6M 0N4

**From:** Yalin Guo  
**Sent:** Wednesday, February 5, 2025 6:04 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Strong Opposition to High-Density Development Proposal

**Follow Up Flag:** Follow up  
**Flag Status:** Completed

Development Proposal Dear Town Council Members,  
I strongly oppose the proposed high-density development at Neyagawa Blvd and Dundas St W. This project will worsen traffic congestion, overburden public services, and disrupt the character of our community. Oakville's infrastructure is not equipped for 2,278 new residential units, which will strain roads, schools, and emergency services. Additionally, the environmental impact on 16 Mile Creek has not been properly addressed. I urge the council to reject this proposal and prioritize sustainable growth that preserves Oakville's livability.

sincerely, Yolanda

**From:** Rebecca L.  
**Sent:** Monday, February 3, 2025 11:32 PM  
**To:** Franca Piazza  
**Cc:** Catherine Buckerfield; Town Clerks  
**Subject:** [EXTERNAL] File No: OPA 1321.02, Z.1321.02 and 24T-24006/1321, Ward 7, Neatt (16 Mile Creek) Inc. - 3056 Neyagawa Boulevard  
**Attachments:** NCA\_OPA1321.02 Z.1321.02 24T-24006-1321\_20250131.pdf

Hello Franca,

I am reaching out as a concerned resident living near 3056 Neyagawa Boulevard regarding the development application (OPA1321.02, Z.1321.02, 24T-24006/1321, Ward 7).

I have reviewed the attached application confirmation letter, which mentions the public meeting several times. However, I would appreciate more information regarding the timeline of this application, particularly when the Town of Oakville plans to host the public meeting for this project. Additionally, could you please provide insights into the general timeline for OP and Zoning applications of this nature to receive a decision?

Given the proposed massing, which seems significantly out of scale, I am eager to stay informed and participate in the public consultation process.

Thank you for your time and assistance.

Best regards,

Rebecca

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**From:** lu vincent  
**Sent:** January 28, 2025 5:35 PM  
**To:** Town Clerks <[TownClerk@oakville.ca](mailto:TownClerk@oakville.ca)>  
**Subject:** [EXTERNAL] Concern about development application: 3056 Neyagawa Boulevard

Hi Oakville Clerk:

I am an Oakville resident/ Canadian, living near the intersection of Dundas/Neyagawa. I recently noticed there's a development application posted on your website at [3056 Neyagawa Boulevards](#)

My neighbors in this community and nearby residents are very upset and worried about this proposal.

Oakville is not a public transportation oriented city like Mississauga or Toronto. Majority of the residents living in this city depend on own cars to send kids to classes and buying groceries. Even for people like me, who worked in Downtown Toronto, we still need to drive to Oakville Go to catch a train. I can't imagine we have 30-story highrises to be built in this area, standing awkwardly from nowhere and surrounded by townhouses and houses. It could be a devastating situation for nearby residents to keep our current lifestyle with 2300+ units thrown into this piece of tiny land (5000+new residents expected) This is not a high density community and never should be. It's not even close to downtown Oakville nor Go Train station.

This area is so close to the Sixteen Miles Creek and Lions Valley Park, which are enjoyed by all local Oakville residents or even tourists from GTA as natural and scenic sites for weekends leisure time. Imagine you built many high rises at the lakeshore in Oakville. How devastating it could be to the attractiveness of Oakville.

Secondly, it's actually already crowded enough if you check the traffic at the Neyagawa/ Dundas crossing. Also the area on the north of Dundas already built so many new homes with smaller lot compared to Oakville other areas, which caused a shortage of schools and amenities. Many students in this area have to study in portables, not classroom, even after many of them are already allocated to the school on the south side.

My point is there's no valid reasons we need high rises in this area or in Oakville to accommodate new residents.

There're still many vacant or undeveloped lands in Oakville to build the types of residence a family with kids really want. We normally don't raise kids in condos here. More importantly, this is not City of Toronto, where people live there for work. We live in Oakville to enjoy the life and spend valuable time with our family. We love Oakville and settle down here only because it's a nice, peaceful, family oriented town. **Please save Oakville and keep what it is.**

Last but not least, I request to be notified for this development application process, attend public meetings. Please add me in, and notify me of the public hearing meeting date. I will attend to raise my concerns about these developments.

Please reply to my email.

My cell phone number:

Best Regards

Vincent

**From:** Danfer Yang  
**Sent:** Friday, January 24, 2025 6:07 PM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] Request to be notified for development application: 3056 Neyagawa Boulevard

Hi, Oakville Clerk:

I am an Oakville resident living near the intersection of Dundas/Neyagawa.

I am aware there is a development application at 3056 Neyagawa Boulevard. The following is the link.

[https://www.oakville.ca/business-development/planning-development/active-development-applications/ward-7-development-and-site-plan-applications/neatt-\(16-mile-creek\)-inc-3056-neyagawa-boulevard-opa-1321-02-z-1321-02-and-24t-24006-1321/](https://www.oakville.ca/business-development/planning-development/active-development-applications/ward-7-development-and-site-plan-applications/neatt-(16-mile-creek)-inc-3056-neyagawa-boulevard-opa-1321-02-z-1321-02-and-24t-24006-1321/)

Development Application  
3056 Neyagawa Boulevard  
File No: OPA 1321.02, Z.1321.02 and 24T-24006/1321

I have many concerns about this extremely high density development, which will bring many problems to our community.

I request to be notified for this development application process, attend public meetings. Please add me in, and notify me of the public hearing meeting date. I will attend to raise my concerns about these developments.

Please reply to my email.  
My cell phone number:

Best Regards  
Danfer

-----Original Message-----

From: Grace Eunjung Yang

Sent: February 13, 2025 10:06 PM

To: Town Clerks <TownClerk@oakville.ca>

Subject: [EXTERNAL] Attn: urgent! Our neighbors are against a plan 3056 Neyagawa Boulevard

Dear whom it may concern,

Hello. We are residents in Oakville and writing to have an option against the plan 3055 Neyagawa Boulevard.

Majority of my neighbours are against the plan for high-rise community.

The new plan is in our community at the northwest corner of Dundas St West and Neyagawa Boulevard. 7 towers from 15 to 28 storeys 2,278 residential units!

That means more than 2,500 additional vehicles and approximately 10,000 new residents to our community in this tiny location.

We are living in Oakville as Oakville is different from other cities in which have high-rise buildings with heavy traffic jams. This current plan will create huge congestion on Dundas, security problem and environmental problems etc.

We are not opposed to developing high-rise buildings for newcomers. However, any new plans must take into account the current situation and needs of the community, rather than solely focusing on the benefits for the developer.

Thank you

Regards,

Grace, Sung, David

나의 iPad에서 보냄

## REPORT

### Planning and Development Council

Meeting Date: March 17, 2025

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**FROM:** Planning and Development Department

**DATE:** March 4, 2025

**SUBJECT:** Public Meeting and Recommendation Report for Zoning By-law Amendment Application – 65, 71, 77, 83 & 89 Loyalist Trail, Tafia Development Corporation, File No.: Z.1215.04

**LOCATION:** 65, 71, 77, 83 & 89 Loyalist Trail

**WARD:** Ward 7 Page 1

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### RECOMMENDATION

1. That the Zoning By-law Amendment application submitted by Tafia Development Corporation, File No.: Z.1215.04, be refused as proposed;
2. That the notice of Council's decision reflect that Council has fully considered all of the written and oral submissions relating to these matters and that those comments have been appropriately addressed; and,
3. That, in accordance with Section 34(17) of the *Planning Act*, no further notice is determined to be necessary.

### KEY FACTS

The following are key points for consideration with respect to this report:

- **Location:** The subject property is located on the north side of Loyalist Trail, east of Sixth Line and east of Loyalist Woods Park (opposite Channing Crescent and Eternity Way), and is municipally known as 65, 71, 77, 83 and 89 Loyalist Trail.
- **Policy Context:** The subject property is designated 'Urban Area' with an 'Employment Area' overlay in the Region of Halton Official Plan, identified as 'Employment Area' in the Urban Structure of the Livable Oakville Official



Plan, and designated '*Employment Area*' within the North Oakville East Secondary Plan.

- **Zoning:** The subject property is zoned '*Light Employment (LE)*' which permits light employment uses and some commercial uses. The existing permitted commercial uses are intended to support and complement the primary employment uses without undermining the overall planned function of the employment area.
- **Previous Applications:** The site was the subject of a Plan of Subdivision and Zoning By-law Amendment (24T-13002/1215 and Z.1215.01) that was approved by the former Ontario Municipal Board (OMB). The subject lands contain five (5) large buildings that were designed and constructed, for Light Employment uses, along with associated parking and landscaping, in accordance with the final approved plans under Site Plan File No. SP.1215.003/01 (issued June 21, 2022, updated December 20, 2023).
- **Current Application:** The applicant has applied for a Zoning By-law Amendment to add a *Commercial Fitness Centre* and remove *Nightclub* and *Hotel* from the list of permitted uses on the subject property. The applicant's site-specific request is to permit Commercial Fitness Centre uses for up to 50% of the gross floor area (GFA) of all buildings, along with a combined parking rate for all uses on the site (i.e., 1 space per 50 square metres of leasable floor area). Staff note that the request for 50% GFA would not include any other uses permitted on the site.
- **Public Consultation:** An applicant-initiated virtual Public Information Meeting ("PIM") was held on August 15, 2024, which was attended by 29 members of the public, as well as the Ward Councillors and town staff. A Public Meeting was held on November 25, 2024, and no members of the public attended. Five (5) written submissions have been received from the public at the time of the writing of this report and are included within Appendix 'A'. Two (2) written submissions are from business owners interested in establishing *Commercial Fitness Centres* (i.e., racket sports and basketball) on the site. The other two (2) written submissions requested notification of Council's decision on the matter. A combined Statutory Public Meeting and Recommendation Meeting will be held on March 17, 2025.
- **Timing:** The subject application was deemed complete on September 27, 2024. Pursuant to the *Planning Act*, Council had until December 30, 2024 to make a decision on the application.
- **Staff Recommendation:** The applicant's request to allocate 50% of the existing Gross Floor Area (GFA) to a Commercial Fitness Centre, combined

with the fact that the existing by-law permits other commercial uses ancillary to employment uses, creates the potential for commercial uses to exceed 50% of the site. As a result, the lands would no longer be predominantly designated for employment. Therefore, the application is not consistent with the Provincial Planning Statement 2024, and does not conform to the Region of Halton Official Plan, the Livable Oakville Official Plan or the North Oakville East Secondary Plan, and therefore, should be refused.

## **BACKGROUND**

The purpose of this report is to provide a recommendation on the proposed Zoning By-law Amendment application for 65, 71, 77, 83 and 89 Loyalist Trail. A Public Meeting was held on November 25, 2024; however, the meeting did not meet all of the statutory requirements, thereby requiring a combined Statutory Public Meeting and Recommendation Meeting to be held on March 17, 2025.

The subject lands are comprised of Block 152 of the Subdivision known as Star Oak North Phase 1 (20M-1221), which was draft approved by the OMB in 2017 together with a Zoning By-law Amendment for residential, employment and natural heritage system uses that implemented the North Oakville East Secondary Plan (NOESP).

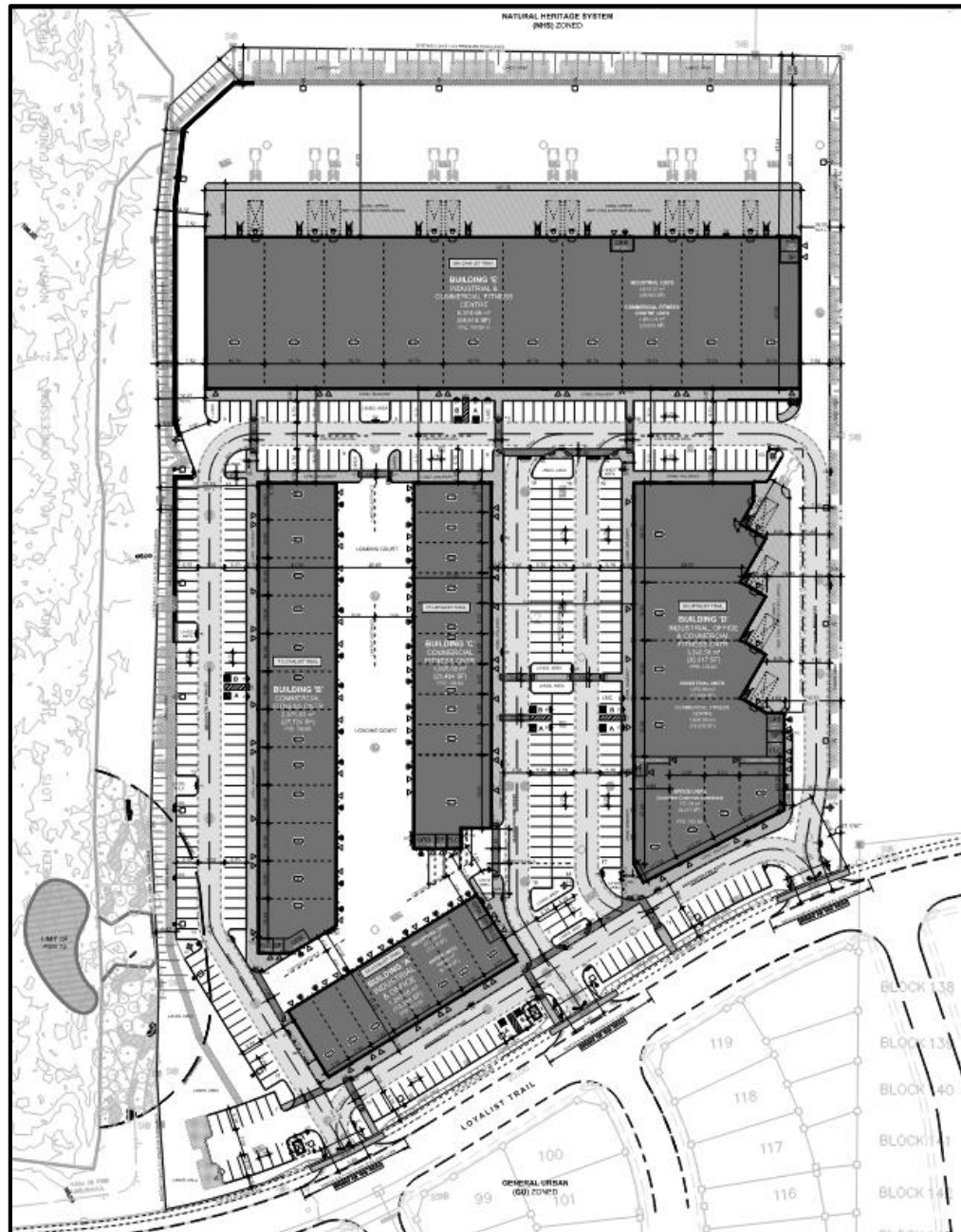
Recently, the new Provincial Planning Statement (PPS) 2024 came into effect, along with changes to the *Planning Act*, and introduced a new policy framework for lands within and adjacent to an “Area of Employment”. In response, Council adopted OPA 332 (as well as OPA 68 and 333) to ensure the town’s employment areas align with the new definition for “Area of Employment”. Staff will be undertaking a comprehensive evaluation of the town’s employment areas to ensure alignment with the current provincial policy regime. At this time, it is not known whether the subject lands are required for the adequate provision of employment uses on a community basis. Further discussion about the OPAs and pending employment area study is provided in this report.

## **Proposal**

The applicant has submitted a Zoning By-law Amendment application to modify the existing Special Provision 45, by:

- Removing *Nightclub* and *Hotel* from the list of permitted uses;
- Adding *Commercial Fitness Centre* to the list of permitted uses;
- Restricting *Commercial Fitness Centre* to a maximum Gross Floor Area (GFA) limit of 50% for all buildings; and,
- Establishing a single minimum parking requirement for all permitted uses to one (1) parking space per 50 square metres of *leasable floor area*.

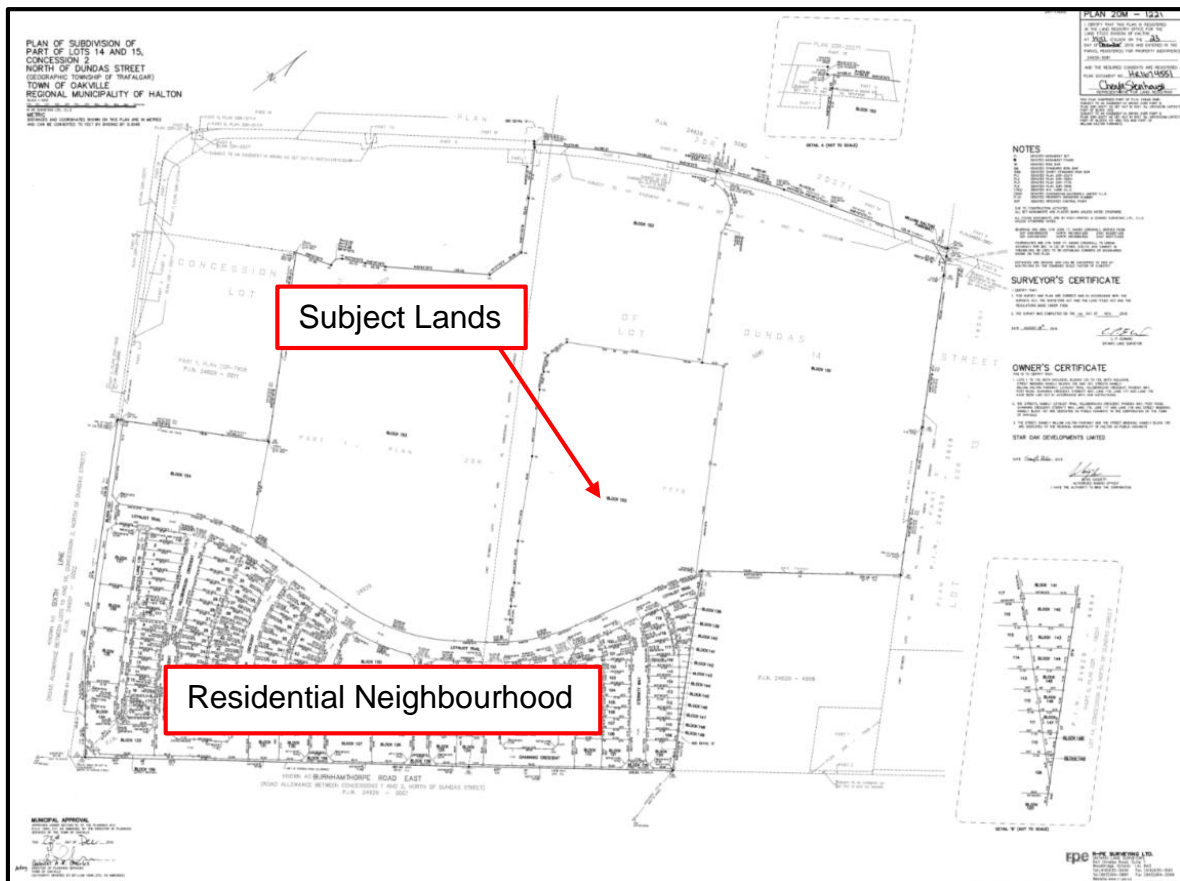
An excerpt of the applicant's as-constructed Site Plan with the identification of the proposed *Commercial Fitness Centre* uses is provided in Figure 1.



*Figure 1: Approved Site Plan*

## Location & Site Description

The subject property is the first property to be developed within the Employment District of the North Oakville East Secondary Plan Area. It is located on the north side of Loyalist Trail, east of Sixth Line and east of Loyalist Woods Park (opposite Channing Crescent and Eternity Way). The lands were identified as Block 152 of Registered Plan 20M-1221 – Star Oak North (Phase 1) Subdivision, as shown in Figure 2, and are now municipally known as 65, 71, 77, 83 and 89 Loyalist Trail.



*Figure 2: Registered Plan 20M-1221 – Star Oak (Phase 1) Subdivision*

The subject lands contain five (5) large buildings that were designed and constructed, for Light Employment uses, along with associated parking and landscaping, (SP.1215.003/01). The Site Plan process paid particular attention to locating the largest buildings, as well as the orientation of loading bays, away from Loyalist Trail to mitigate any impacts on adjacent residential uses to the south (see Figure 1, above).

## Surrounding Land Uses

The land uses surrounding the subject lands include the following:

- North – Loyalist Woods Park (Natural Heritage System)
- East – Vacant (Future Development – Employment Area)
- South – Low density residential uses
- West – Loyalist Woods Park (Natural Heritage System)

To provide additional context to the surrounding land uses, Figure 3 shows the subject lands relative to existing and future development. The lands on the south side of Loyalist Trail were developed for residential uses, in accordance with the Zoning approved by the OMB under the 'Transitional Area' designation, which permits a range of commercial and residential uses to act as a buffer to the employment area to the north, subject to an Area Design Plan.

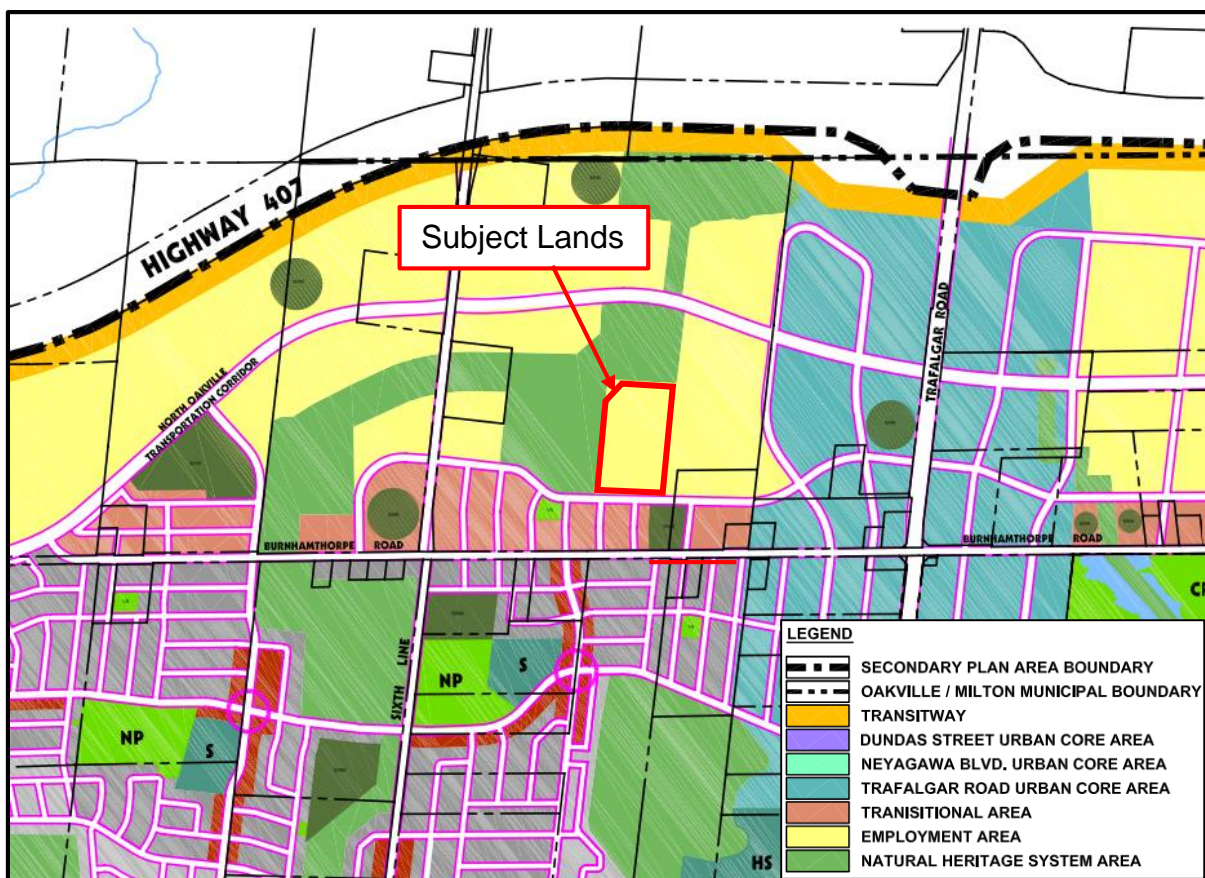


Figure 3: Excerpt of Appendix 7.3 Town of Oakville Master Plan

## PLANNING POLICY & ANALYSIS

The property is subject to the following policy and regulatory framework:

- The *Planning Act*
- Provincial Planning Statement (2024)
- Halton Region Official Plan (implemented by the town)
- North Oakville East Secondary Plan (NOESP)
- Zoning By-law 2009-189

### Planning Act, R.S.O. 1990 (*Planning Act*)

As of October 20, 2024, the effective date of recent amendments to the *Planning Act*, the new definition for “Area of Employment” means:

*“an area of land designated in an official plan for clusters of business and economic uses, those being uses that meet the following criteria:*

1. *The uses consist of business and economic uses, other than uses referred to in paragraph 2, including any of the following:*
  - i. *Manufacturing uses.*
  - ii. *Uses related to research and development in connection with manufacturing anything.*
  - iii. *Warehousing uses, including uses related to the movement of goods.*
  - iv. *Retail uses and office uses that are associated with uses mentioned in subparagraphs i to iii.*
  - v. *Facilities that are ancillary to the uses mentioned in subparagraphs i to iv.*
  - vi. *Any other prescribed business and economic uses.*
2. *The uses are not any of the following uses:*
  - i. *Institutional uses.*
  - ii. *Commercial uses, including retail and office uses not referred to in subparagraph 1 iv.”*

The new definition differs from the previous definition in that it explicitly excludes institutional and commercial uses from an “Area of Employment”. Previously, those uses were not excluded and could be located within an “Area of Employment” without risking non-conformity with the provincial definition and loss of status as a protected employment area. The change to the definition under the *Planning Act* does not change the Official Plan designations applicable to those areas, but does determine whether applications for Official Plan amendments or Zoning By-law amendments to remove the lands from the area of employment are subject to appeal.



Further, the transition provision for the change in definition for “Area of Employment” under subsections 1 (1.1) and (1.2) of the *Planning Act* allows municipalities to have policies in their official plans that allow employment areas to continue as an “Area of Employment”. This applies even if lands are occupied by uses excluded from the new definition if the use was “lawfully established” before the effective date and if official plan policies do not authorize uses that are excluded from the new definition on or after the effective date within those employment areas.

On October 15, 2024, Town Council adopted OPAs 68 (Livable Oakville), 332 (NOESP), and 333 (North Oakville West Secondary Plan) to allow the town’s employment areas to continue as protected “Area(s) of Employment” and provide time for staff to align the town’s employment policy framework with the provincial changes. Town Council excluded the subject lands from being within an “Area of Employment, as defined by the *Planning Act*”. As a result, applications for Official Plan amendments or Zoning By-law amendments to remove the subject lands from an area of employment would be subject to appeal. OPAs 332, 68 and 333 are all currently under appeal to the OLT. If approved, the OPAs will be deemed to take effect as of, the date on which the appeals are resolved.

Further, under Section 2 of the *Planning Act*, decision makers shall have regard for matters of Provincial Interest such as:

- “(k) *the adequate provision of employment opportunities;*
- “(l) *the protection of the financial and economic well-being of the Province and its municipalities;*
- “(m) *the co-ordination of planning activities of public bodies;*
- “(n) *the resolution of planning conflicts involving public and private interests;*
- “(p) *the appropriate location of growth and development;”*

The town is engaging in a comprehensive Employment Area Review to determine (but not limited to):

- The town’s long term employment needs;
- Where lands to support those long term needs should be located;
- How to address lands deemed not suitable for employment (per the new definition), but also not suitable for sensitive land uses, given proximity to employment areas; and,
- Where lands should be located for complementary commercial and institutional uses.

The subject lands have been developed to provide opportunities for employment uses and, subject to the Review, these lands are still necessary for the adequate provision of employment uses on a community basis.

The proposed removal of *Nightclub* and *Hotel* from the list of permitted uses on the subject site does not interfere with the employment uses on the site. It is staff's opinion that the proposal to permit an additional commercial use (*Commercial Fitness Centre*) that would occupy up to 50% GFA of the existing buildings on the site does not have regard for matters of Provincial Interest under Section 2 of the *Planning Act*, which could impact the ability to protect the broader "Area of Employment" as set out by the *Planning Act*. Furthermore, the full impact of the proposed parking rate for all uses on the site may negatively impact light industrial uses on the site, as such uses have a reduced parking rate than other uses permitted within the LE (Light Employment) Zone.

It is staff's opinion that the proposal does not have sufficient regard for the matters set out in section 2 of the *Planning Act*.

### **Provincial Planning Statement**

The new Provincial Planning Statement (2024) (PPS) is intended to promote a policy-led system, which recognizes that there are complex relationships among environmental, economic, and social factors in land use planning. The PPS encourages the wise management of land to achieve efficient development and land use patterns by directing growth to settlement areas and by promoting a compact development form. All planning decisions must be consistent with the PPS.

The new PPS definition for employment areas is the same as the *Planning Act* "Area of Employment" definition and recognizes the transition provision under 1(1.1) and (1.2) in the *Planning Act*. The subject lands were previously identified to be within a Provincially Significant Employment Zone, which has since been removed in favour of the new PPS policies and the changes to the *Planning Act* noted above. The PPS continues to provide that municipalities should plan, protect, and preserve employment areas for current and future needs.

Policies within Section 2.8.1 Supporting a Modern Economy state:

- "1. *Planning authorities shall promote economic development and competitiveness by:*
  - a) *providing for an appropriate mix and range of employment, institutional, and broader mixed uses to meet long-term needs;*
  - b) *providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;*



- c) *identifying strategic sites for investment, monitoring the availability and suitability of employment sites, including market-ready sites, and seeking to address potential barriers to investment;*
  - d) *encouraging intensification of employment uses and compatible, compact, mixed-use development to support the achievement of complete communities; and,*
  - e) *addressing land use compatibility adjacent to employment areas by providing an appropriate transition to sensitive land uses.*
2. *Industrial, manufacturing and small-scale warehousing uses that could be located adjacent to sensitive land uses without adverse effects are encouraged in strategic growth areas and other mixed-use areas where frequent transit service is available, outside of employment areas.*
3. *In addition to policy 3.5, on lands within 300 metres of employment areas, development shall avoid, or where avoidance is not possible, minimize and mitigate potential impacts on the long-term economic viability of employment uses within existing or planned employment areas, in accordance with provincial guidelines.”*

The PPS, 2024 provides the following definition:

*“Employment area: means those areas designated in an official plan for clusters of business and economic activities including manufacturing, research and development in connection with manufacturing, warehousing, goods movement, associated retail and office, and ancillary facilities. An employment area also includes areas of land described by subsection 1(1.1) of the Planning Act. Uses that are excluded from employment areas are institutional and commercial, including retail and office not associated with the primary employment use listed above.”*

As previously mentioned, the subject lands are designated ‘*Employment District*’ within the NOESP and have been fully developed to support the town’s existing and future needs for employment and employment-related uses to support the function of the surrounding employment area. The proposed removal of *Nightclub* and *Hotel* from the list of permitted uses on the subject site does not interfere with the employment uses on the site. However, the introduction of an additional commercial use that would occupy up to 50% GFA of the existing buildings on the site that has the potential to undermine the intended function of the employment area that will negatively impact the effective operation of employment uses both on and off the site.

Policies under Section 2.8.2 Employment Areas include the following:

- “1. Planning authorities shall plan for, protect and preserve employment areas for current and future uses, and ensure that the necessary infrastructure is provided to support current and projected needs.*
- 2. Planning authorities shall protect employment areas that are located in proximity to major goods movement facilities and corridors, including facilities and corridors identified in provincial transportation plans, for the employment area uses that require those locations.*
- 3. Planning authorities shall designate, protect and plan for all employment areas in settlement areas by:*
  - a) planning for employment area uses over the long-term that require those locations including manufacturing, research and development in connection with manufacturing, warehousing and goods movement, and associated retail and office uses and ancillary facilities;*
  - b) prohibiting residential uses, commercial uses, public service facilities and other institutional uses;*
  - c) prohibiting retail and office uses that are not associated with the primary employment use;*
  - d) prohibiting other sensitive land uses that are not ancillary to uses permitted in the employment area; and,*
  - e) including an appropriate transition to adjacent non-employment areas to ensure land use compatibility and economic viability.”*

The above policies further affirm the need to plan, protect, and preserve employment areas for current and future needs. The proposed Zoning by-law amendment, as proposed, will undermine the town’s ability to protect, maintain, or preserve the viability of employment areas and the long term economic goals of the town.

It is staff’s opinion that the proposed rezoning is not consistent with the PPS.

### **Halton Region Official Plan**

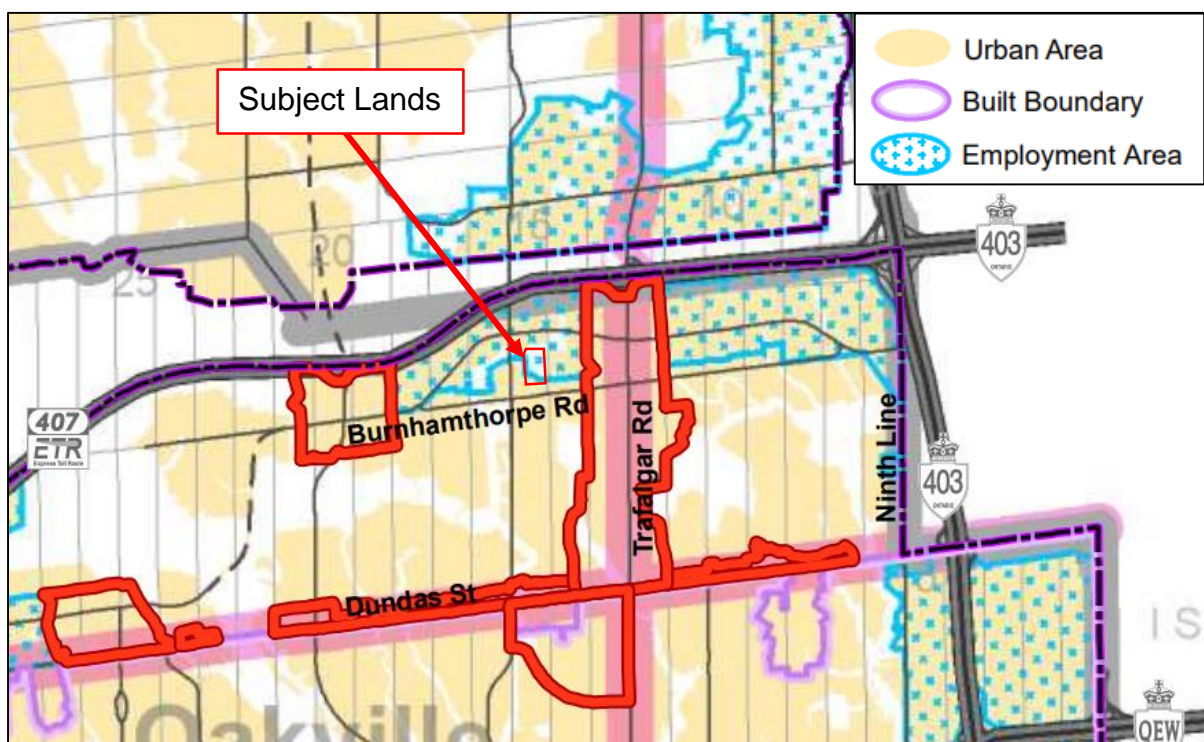
As of July 1, 2024 (Bill 185), the responsibility of the Halton Region Official Plan (“Regional Official Plan”) rests with the Town of Oakville.

The subject lands are designated as ‘Urban Area’ and Policy 75 of the Regional Official Plan states:

- “75. The Urban Area is planned to accommodate the distribution of population and employment for the Region and the four Local Municipalities...”*

The Regional Urban Structure implements Halton's planning vision and growth management strategy to ensure efficient use of land and the long-term protection of lands for employment uses. One of the objectives of the Regional Urban Structure is to identify and protect 'Regional Employment Areas', which includes providing for an appropriate range and balance of employment uses (e.g., industrial, office, retail and institutional uses) to meet long-term needs and to protect areas designated for employment uses.

Figure 4, below, identifies the lands as being within the Employment Area.



*Figure 4: Excerpt of Map 1H – Regional Urban Structure*

As provided in Subsection 83.2(1) c), Employment Areas are intended to permit a range of employment uses, including but not limited to industrial, manufacturing, warehousing and office uses. In addition to employment uses, Subsection 83.2(1) d) allows local municipalities to also permit a range of ancillary uses that are associated with an employment use or are supportive to the overall Employment Area, in accordance with Section 83.2(6) b) of the Regional Official Plan. Section 83.2(2) affirms that Employment Areas are to be planned for, protected, and preserved for, current and future use. Section 83.2(6) b) requires Local Municipalities to develop policies that support forecasted employment growth and

identify a range of employment uses appropriate for the planned function of Employment Areas.

Staff is of the opinion that the proposed addition of another commercial use (i.e., *Commercial Fitness Centre*) that could occupy up to 50% of the GFA of the existing buildings will negatively impact the planned function of the site for employment uses, and that the proposed commercial use (i.e., *Commercial Fitness Centre*) does not support the surrounding 'Regional Employment Area', which would undermine the function of these employment areas, as set out in Policy 83.2.

On this basis, it is staff's opinion that the proposed rezoning does not conform to the Region of Halton Official Plan.

### **North Oakville East Secondary Plan**

The North Oakville area consists of land located between Dundas Street to the south and Highway 407 to the north, from Ninth Line in the east to Tremaine Road in the west. The vision of North Oakville East includes the establishment of a business park along Highway 407 to provide a range of employment opportunities to residents of Oakville that will contribute to the accommodation of employment targets for the town and help to create a community where people can live and work. As shown on Figure NOE2 Land Use Plan (shown in Figure 5, below), the subject lands are designated as 'Employment District', which form part of this larger business park.

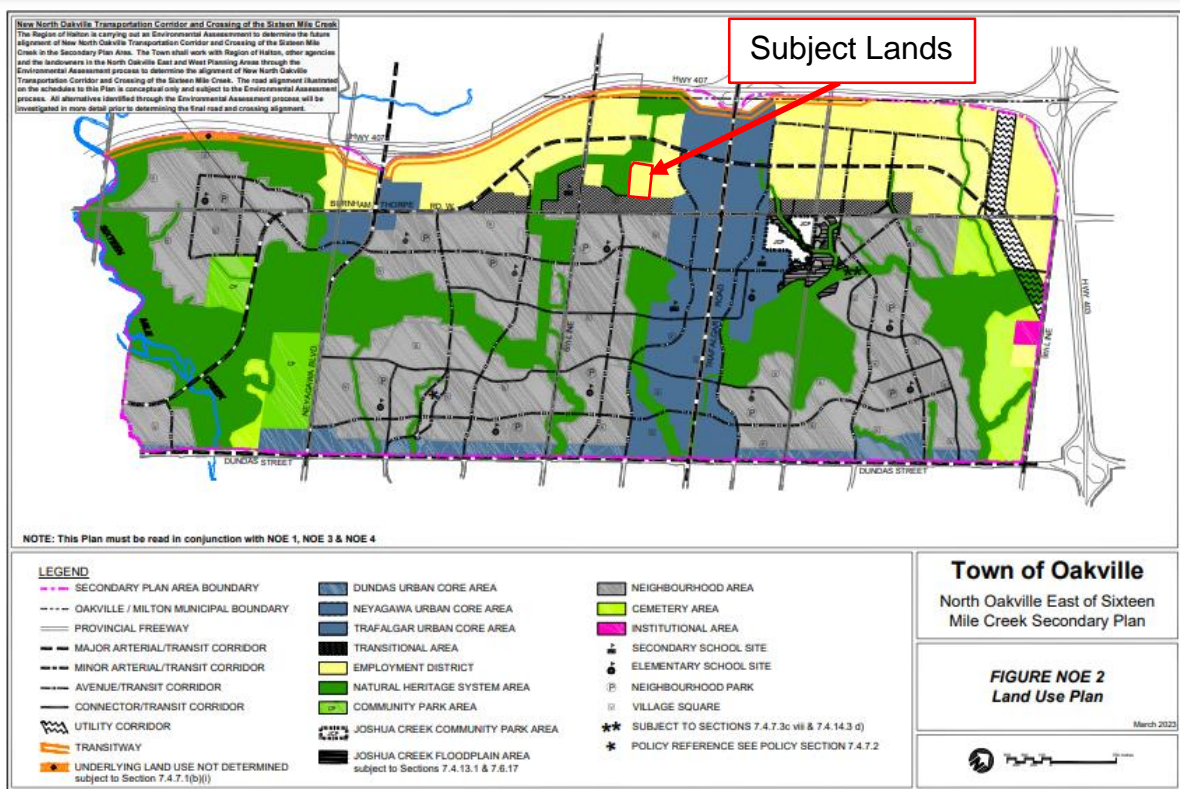


Figure 5: Figure NOE 1 Community Structure, NOESP

Within the NOESP Section 7.3 – Community Structure, Policy 7.3.4 states:

*“Employment Districts refer to land designed to accommodate development of predominantly employment generating uses including a wide range of industrial and office development. Limited retail and service commercial uses designed to serve the businesses and employees will also be permitted within the Employment Districts.”*

Furthermore, Policy 7.5.17 states:

*“The Employment District will permit a full range of employment uses. In addition, limited retail and service commercial uses serving the employment area shall be clustered at the intersections with Arterial, Avenue and Connector roads. These sites and uses will be zoned in a separate zone in the implementing Zoning By-law.*

*The Urban Design and Open Space Guidelines and Zoning By-law will also apply standards designed to enhance the compatibility of permitted employment uses with residential and other sensitive development located*

*within nearby Transitional, Residential or Institutional designations including prohibition of outdoor storage adjacent to such uses.”*

Within Section 7.6 – Land Use Strategy, Section 7.6.8 concerns the Employment District and Policy 7.6.8.1 affirms that the primary focus of the designation is to protect for, and provide a range of employment generating industrial, office and service employment uses that are sensitive to, and compatible with, the adjacent residential neighbourhoods. In terms of permitted uses, Policy 7.6.8.2 provides:

*“7.6.8.2 Permitted Uses, Buildings and Structures*

*Permitted uses may include:*

- a) light industrial operations, including light manufacturing, assembling, processing, fabricating, repairing, warehousing, distribution and wholesaling;*
- b) business and professional office uses and medical clinics;*
- c) service establishments such as print shops, equipment rental establishments, restaurants, hotels, banquet halls, financial institutions, and service establishments which primarily provide services at the customer’s location such as electricians and plumbers and limited retail commercial development such as business supply and industrial supply establishments subject to the requirements of Section 7.6.8.3 and 7.6.8.4 d);*
- d) public uses, institutional uses including places of worship, vocational schools;*
- e) sport and recreation, and place of amusement uses;*
- f) automobile related uses, including gas stations; and,*
- g) ancillary retail sales of products produced, assembled and/or repaired on the premises,*
- h) as part of a distribution use, the ancillary retail sale of the products distributed from an ancillary showroom;*
- i) research and development;*
- j) information processing, call centres and similar uses; and,*

*k) computer based services including design studios.”*

To provide further clarification on the permitted uses within the Employment District, Policy 7.6.8.4 a) states:

*“a) It is not intended that the full range of employment uses will be permitted in all locations designated “Employment District”. The precise range of uses and density of development shall be stipulated in the zoning by-law...”*

The subject lands were developed in accordance with the policies of the NOESP that would permit a range of employment uses as stipulated in the zoning by-law. The proposal of sport and recreational uses (i.e., *Commercial Fitness Centre*) occupying up to 50% GFA of the existing buildings has the potential to impact the ability of future employment uses of this site and future employment sites along Loyalist Trail to operate their businesses efficiently, in terms of employee/client parking and the movement of goods. Similarly, the increased amount of personal vehicular traffic associated with the proposed *Commercial Fitness Centre* is likely to interfere with the shipping and receiving of goods to the *Light Industrial* uses anticipated to operate on-site and the larger employment area beyond.

#### Official Plan Amendment 332 – Under Appeal

Official Plan Amendment (OPA) 332, as it applies to the North Oakville East Secondary Plan, was adopted by Council in response to the new provincial policy framework established by the introduction of “Area of Employment” defined in the *Planning Act* and the Provincial Planning Statement (2024). OPA 322 is subject to appeal and is not in force.

OPA 332 introduced text changes, including new policies 7.6.8.2. l), m) and n), as follows:

- “l) Notwithstanding sections 7.3.4, 7.5.17, 7.6.2.2, and 7.6.11.3 and policies under section 7.6.8, after October 19, 2024, new institutional and commercial uses, including retail and office uses, not referred to in paragraph 1 of subsection 1 (1) of the Planning Act shall not be permitted within the Employment District.*
- m) Notwithstanding 7.6.8.2 l), parcels of land within the Employment District that were used for uses excluded from the definition “area of employment” in paragraph 2 of subsection 1 (1) of the Planning Act, that were lawfully established on or before October 19, 2024, may continue to be used for such purposes pursuant to subsections 1 (1.1) and (1.2) of the Planning Act.*



- n) *For the purposes of 7.6.8.2 l) and m), the property municipally addressed as 65, 71, 77, 83, 89 Loyalist Trail is not considered to be within an “area of employment” as defined in subsection 1 (1) of the Planning Act.”*

Policy 7.6.8.2 l) applies to lands designated Employment District and prohibits the establishment of new institutional and commercial uses, including retail and office uses, not referred to in subsection 1(1) of the *Planning Act*. Policy 7.6.8.2 n) excludes the subject lands from being considered an “Area of Employment”, but the subject lands remain designated Employment District in the NOESP.

As indicated earlier, a town-initiated comprehensive Employment Area Review is underway. The anticipated outcome includes recommendations on policy changes to the town’s policy framework to fully accommodate the town’s employment growth projections to 2051 and provide consistency and conformity with the provincial planning framework, amongst other goals. Without the completion of the comprehensive review of the town’s Employment Area Review, the amount of land that may be eligible for removal from ‘*Employment District*’ is unknown. The applicant’s proposed amendments to the zoning by-law have the potential to erode and undermine the planned function and integrity of the larger employment area.

As indicated above, the subject property is the first to be developed within the ‘*Employment District*’ of the North Oakville East Secondary Plan Area, and the proposal allowing 50% GFA to *Commercial Fitness Centre* undermines the intended function of the surrounding employment area. Furthermore, until the town’s Employment Area Review has been completed, it remains unknown what amount of land, if any, may be released from employment uses.

As a result, it is staff’s opinion that the proposal does not conform to the NOESP.

### **Zoning By-law 2009-189**

The North Oakville Zoning By-law was approved to implement the policies of the NOESP and the NOWSP. The zones and associated regulations have been prepared to reflect the growth and employment opportunities planned for North Oakville.

As indicated in Figure 6, below, the subject lands are zoned LE (Light Employment) to permit light industrial uses, such as light manufacturing, fabricating, processing, assembly, repair, servicing, packaging, industrial warehousing and wholesaling of products or materials not accessible to the general public. Additional permitted uses include, but are not limited to, *General Office, Hotel, Place of Amusement, Club, Nightclub, Commercial School, Private Career College, and Day Care centre*. The existing, in-effect zoning implements the NOESP.



Supportive commercial uses are permitted within employment areas to enhance and facilitate the primary employment functions; however, maintaining the planned function of these lands as dedicated employment areas remains a priority. While certain commercial activities, such as an office or day care, may be allowed to serve the needs of businesses and employees within the area, commercial uses must not overshadow or detract from the primary objective of preserving land for employment uses. The intent is to ensure that employment areas remain viable areas for manufacturing, fabricating and warehousing for example, rather than evolving into a commercial area. As a result, commercial uses should be carefully regulated to prevent them from undermining the long-term economic and functional objectives of employment lands.

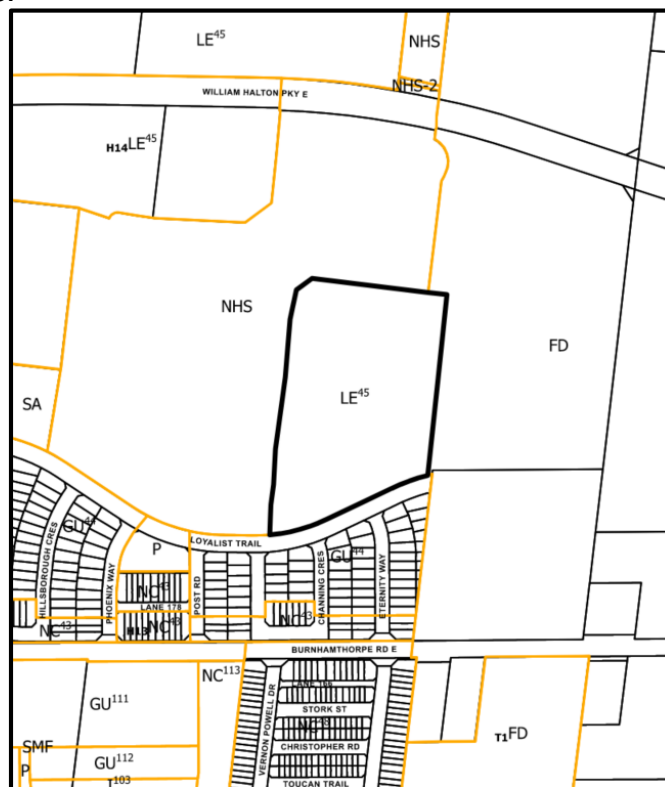


Figure 6: Excerpt of Zoning By-law 2009-189

The applicant proposes an amendment to Zoning By-law 2009-189 to modify the existing Special Provision 45, as follows:

- Removing *Nightclub* and *Hotel* from the list of permitted uses;
- Adding *Commercial Fitness Centre* to the list of permitted uses;
- Restricting *Commercial Fitness Centre* to a maximum Gross Floor Area (GFA) limit of 50% for all buildings;

- Establishing a single minimum parking requirement for all permitted uses to one (1) parking space per 50 square metres of *leasable floor area*.

Zoning By-law 2009-189 provides the following definitions:

*“commercial fitness centre” means premises in which facilities are provided for the purpose of profit for recreational or athletic activities such as body-building and exercise classes and may include associated facilities such as a sauna, swimming pool, solarium, cafeteria and accessory uses as well as ancillary retail, but does not include a stadium or arena.*

*“hotel” means a building, or group of buildings, each containing sleeping accommodation, catering primarily to the traveling public, for rent or hire for temporary lodging. Hotel may also include restaurant, public hall and ancillary retail uses which are incidental and subordinate to the primary hotel function and oriented to serve the hotel patrons.*

*“nightclub” means an establishment or part thereof having a capacity to accommodate no more than 1000 persons at a time, whose principal function is the provision of music and where food or beverages may be served.*

Section 5 of Zoning By-law 2009-189 provides parking and loading regulations for North Oakville and, in accordance with Table 5.1B – Parking Requirements for Non-Residential Uses, the proposal of a *Commercial Fitness Centre* requires 1 parking space per 30 square metres of leasable floor area. Table 1, below, provides a list of some uses permitted within the LE (Light Employment) Zone and their respective minimum parking requirements, beginning with the most restrictive uses.

<b>Land Use</b>	<b>Minimum Parking Requirements</b> (Based on <i>leasable floor area</i> , unless otherwise noted)
Arena, Stadium, Theatre	1 <i>parking space</i> per 6 seats
<i>Nightclub, Public Hall</i>	1 <i>parking space</i> per 10 sq.m.
<i>Hotel</i>	0.65 <i>parking spaces</i> per suite plus 1 <i>parking space</i> for every 10 sq.m. of <i>leasable floor area</i> devoted to <i>public hall</i> uses; and, 1 <i>parking space</i> per suite plus 1 <i>parking space</i> for every 10 sq.m. of <i>leasable floor area</i> devoted to <i>public hall</i> uses
<i>Medical Office</i>	1 <i>parking space</i> per 20 sq.m.

<b>Land Use</b>	<b>Minimum Parking Requirements</b> (Based on <i>leasable floor area</i> , unless otherwise noted)
Trade and Convention Centre, <i>Club, Place of Amusement, Commercial School, Service Establishment, Private Career College</i>	1 <i>parking space</i> per 30 sq.m.
<i>General Office Use, Financial Institution, Call Centre, Research and Development, Information Processing</i>	1 <i>parking space</i> per 37 sq.m.
<i>Day Care Centre</i>	1 <i>parking space</i> per 40 sq.m.; and, 1 <i>queued parking space</i> maximum per every five pupil capacity
<i>Light Industrial</i> (each premises 7,500 sq.m. or less)	1 <i>parking space</i> per 100 sq.m.
<i>Light Industrial</i> (each premises 7,500 sq.m. or greater)	1 <i>parking space</i> per 200 sq.m.
<i>Place of Worship</i>	No minimum

*Table 1: Minimum Parking Requirements for Uses Permitted in the LE (Light Employment) Zone, Zoning By-law 2009-189*

As referenced above, *Nightclub* and *Hotel* are currently permitted uses intended to support the function of the Employment District; whereas the proposal of a *Commercial Fitness Centre* has the potential to cater more to local residents. The nature of said use (i.e., hours of operation, frequency and number of customers) is anticipated to impact the ability of the future employment uses to operate their businesses efficiently on this site, and future employment sites along Loyalist Trail, in terms of employee/client parking and the movement of goods. On this basis, staff is not supportive of proposed amendments to the Zoning By-law as they do not implement the NOESP.

An amendment to the Zoning By-law to permit 50% GFA for an additional commercial use within the LE Zone does not conform to the NOESP.

## **TECHNICAL & PUBLIC COMMENTS**

The development application was circulated to internal departments and external agencies for a full assessment of the proposal.

## Issues Under Review / Matters to be Considered

The changes to provincial policy has challenges with respect to how employment areas are currently laid out throughout the province, as well as any land use compatibility matters resulting from the change in definition of “Area of Employment”. On this basis, staff continue to rely on the existing town and regional policies to help inform the land use planning matters related to the protection of lands that have been designated for employment uses.

The following is an overview of the matters identified by Staff and Council at the November 25, 2024, Public Meeting.

### 1. Consistency with the Provincial Planning Statement regarding employment areas

The proposal has been assessed and has been determined to be inconsistent with the Provincial Planning Statement, 2024, as explained in the Planning Policy Analysis section, above.

### 2. Conformity to the North Oakville East Secondary Plan

The proposal has been assessed and has been determined to not be in conformity with the North Oakville East Official Plan, as the proposal of allowing 50% GFA for Commercial Fitness Centre does not support the planned function of the ‘*Employment District*’.

### 3. Proposed land use and contribution to total gross floor area

The subject property contains five (5) buildings that have been constructed for light employment uses and the approved site plan locates the largest buildings, as well as the orientation of loading bays, away from Loyalist Trail to mitigate any impacts on adjacent residential uses. Table 2, below, provides a breakdown of the size and description of the existing buildings.

	Number of Units	Gross Floor Area	Loading Dock Typology	Intended Uses
Building ‘A’ (65 Loyalist Trail)	8	1,240.08 sq.m.	9 loading docks designed for up to Light Single Unit Truck (LSU, TAC-2017)	50% Industrial & 50% Office
Building ‘B’ (71 Loyalist Trail)	12	2,575.63 sq.m.	10 loading docks designed for Light Single Unit Truck (LSU, TAC-2017)	50% Industrial & 50% Office

Building 'C' (77 Loyalist Trail)	10	1,995.88 sq.m.	10 loading docks designed for up to Light Single Unit Truck (LSU, TAC-2017)	50% Industrial & 50% Office
Building 'D' (83 Loyalist Trail)	8	3,392.56 sq.m.	10 loading docks designed for up to Semi-Tractor Trailer (WB20, TAC-2017)	4 Industrial Units & 4 Office Units
Building 'E' (89 Loyalist Trail)	10	6,374.88 sq.m.	10 loading docks designed for up to Semi-Tractor Trailer (WB20, TAC-2017)	100% Industrial
Total	48	15,578.83 sq.m.		Industrial & Office

*Table 2: Building Statistics, Loading, and Intended Uses, as per approved Site Plan (SP.1215.003/01)*

The applicant proposes that up to 50% of the buildings' GFA on the subject lands, totaling 7,789.41 sq.m, may be used for a *Commercial Fitness Centre*. Allowing this use, along with the potential for other permitted commercial uses, would significantly reduce the space available for industrial uses, undermining the planned function of the Employment District designation.

#### **4. Transportation implications (i.e., traffic volumes)**

The Transportation Impact Study (TIS) Addendum, prepared by Nextrans and dated January 24, 2025, analyzed the impact of dedicating 50% of the gross floor area to the proposed *Commercial Fitness Centre*. The study projects that this use will generate 194 two-way personal vehicle trips (129 in, 65 out) during the AM peak hour and 373 trips (181 in, 192 out) during the PM peak hour, along with one inbound heavy truck trip during both peak periods.

Regardless of whether the *Commercial Fitness Centre* is introduced, traffic movements generated by the site warrant signalization at the intersection of Sixth Line and Loyalist Trail. While the study concludes that the proposed use will not introduce unacceptable traffic impacts within the study area, Transportation Engineering staff have identified gaps in the analysis, including the lack of clarification on why passenger vehicles using the intersection of Eternity Way and Burnhamthorpe Road were not considered. Further analysis is required.

#### **5. Parking requirements**

The parking justification component of the Transportation Impact Study (TIS) Addendum only considers Industrial, Office and *Commercial Fitness Centre* uses.

This is because the existing buildings were designed for Industrial and Office uses, and the proposal is to add *Commercial Fitness Centre* to the permitted uses.

However, the TIS Addendum does not provide sufficient justification for the proposed parking rate. Specifically, it does not account for other as-of-right uses within the LE Zone that may have more restrictive parking requirements than the proposed rate of 1 per 50 square metres. Staff recommend a comprehensive calculation of all potential uses based on the available parking. Additionally, the applicant should demonstrate how parking constraints would be managed, either by regulating certain uses or restricting some as-of-right permissions if adequate parking cannot be provided.

## **6. Integration / impact on adjacent properties**

Loyalist Trail is classified as a connector road intended to accommodate vehicles that serve the existing and planned '*Employment District*' and '*Transitional Area*' uses. Loyalist Trail is intended to extend easterly to Trafalgar Road to further service additional employment lands as well as uses resulting from the Trafalgar Urban Core (TUC) developments on Trafalgar Road. The concerns of area residents include the mixture of passenger vehicles with truck traffic, which staff anticipate will be reduced once Loyalist is extended to Trafalgar Road.

While fewer trucks are anticipated to access the site with the introduction of *Commercial Fitness Centre*, the TIS Addendum noted a significant increase in the number of personal vehicle trips throughout the day, particularly during the PM peak hours due to the size of the proposed *Commercial Fitness Centre*.

Currently, there is no on-street parking signage provided on Loyalist Trail, but it is anticipated that a limited number of street parking spaces will be available abutting the Village Square (Loyalist Parkette) on the south side of the street and the Loyalist Woods Park Trail through the NHS on the north side of the street to encourage usage of these neighbourhood amenities. On-street parking is intended to supplement the on-site parking and is not to be relied upon for daily parking needs. As the applicant has not provided a fulsome analysis of the parking requirements and transportation demand measures to support alternative modes of transportation, staff are concerned that insufficient parking may be provided on-site for the proposed *Commercial Fitness Centre* uses and may spill onto the abutting streets.

## **7. Utility company requirements**

Initially, Enbridge staff raised concerns about the proposal; however, these concerns have been addressed, and they no longer have any objections.

## 8. Public Comments

At the time of the writing of this report, five members of the public have provided written comments regarding the subject rezoning application. Three members of the public represent prospective leaseholders interested in establishing a *Commercial Fitness Centre* within the existing buildings; one seeking to open a racket sport facility (e.g., padel tennis, pickleball, etc.) and the other two looking to create a membership-based basketball facility. The remaining two individuals have submitted written requests to be notified of Council's decision on the application.

## 9. Should any other use(s) currently permitted, be removed from the list of permitted uses for the subject lands (e.g., hotel, club, etc.).

Although staff recommend that the town's Employment Area Review inform any changes in zoning, should Council approve the addition of *Commercial Fitness Centre* on the subject lands, staff recommend that a new Special Provision incorporating the previous permissions under Special Provision 45 be applied to the subject lands to restrict the maximum leasable floor area of permitted uses based on the parking that is available on-site. Staff advise that Special Provision 45 applies to additional properties within the Star Oak North Phase 1 (20M-1221) Subdivision, so amending Special Provision 45 would have unintended consequences within the Employment District beyond the subject lands.

## CONCLUSION

Staff has provided a thorough analysis of the policy framework and an assessment of the impacts of the proposed rezoning to permit a *Commercial Fitness Centre* on the subject lands. Fundamentally, staff do not support the application from a land use planning perspective. Maintaining the existing provisions of the LE zone will provide the opportunity for appropriate small-scale employment uses with complimentary commercial uses that serve the employment uses and are in the public interest. The following points summarize staff's opinion:

- While some supportive commercial uses are currently allowed on this site, the primary focus should remain on employment uses. Limited commercial activities, such as offices or daycares, may be permitted to serve businesses and workers, but they cannot take priority over manufacturing, fabricating, and warehousing. To protect the planned function of these lands, staff are recommending denial of the applicant's request to permit Commercial Fitness Centre uses for up to 50% of the gross floor area (GFA) of all buildings, as it would compromise the long-term economic and employment objectives of the area.
- This application is not in the public interest at this time.

- The rezoning of the subject lands for a *Commercial Fitness Centre* is not consistent with the PPS 2024 and does not conform to the Region of Halton Official Plan, and the North Oakville East Secondary Plan on the basis that the introduction of a *Commercial Fitness Centre* use does not protect, maintain, or preserve the viability of employment areas and the long term economic goals of the town.

## CONSIDERATIONS

### (A) PUBLIC

The applicant held a Public Information Meeting on August 15, 2024, which was attended by 29 members of the public, Ward 7 Councillors, and staff. A Public Meeting was held on November 25, 2024 and no members of the public attended. Unfortunately, the meeting did not meet the statutory requirements of 20-days notice. As a result, the combined Statutory Public and Recommendation Meeting to be held on March 17, 2025 will meet the statutory requirements. Public notice was mailed and the sign updated on February 24, 2025 (i.e., 21 days prior to the meeting).

Notice of this meeting was distributed to property owners within 240m of the Subject Property in accordance with the town's current notice requirements and *Planning Act*. Staff have received four letters of correspondence for the subject application, included in Appendix 'A'. The matters concerned prospective leaseholders and requests for notification of Council's decision, as referenced in the report above.

### (B) FINANCIAL

None.

### (C) IMPACT ON OTHER DEPARTMENTS & USERS

The application was circulated to internal and external departments and agencies for review.

### (D) COUNCIL STRATEGIC PRIORITIES

This report addresses Council's strategic priority/priorities: Growth Management, Community Belonging, Environmental Sustainability and Accountable Government.

### (E) CLIMATE CHANGE/ACTION

Any future development on this site will be reviewed to ensure compliance with the Town's sustainability objectives of the NOESP.



## **APPENDICES**

### Appendix “A”: Written Correspondence

Prepared by:

Delia McPhail, MCIP, RPP  
Planner – Current Planning

Recommended by:

Brandon Hassan, MCIP, RPP  
Acting Manager of Current Planning – East

Submitted by:

Gabriel A.R. Charles, MCIP, RPP  
Director, Planning & Development

**Delia McPhail**

---

**From:** Delia McPhail  
**Sent:** Friday, September 27, 2024 4:01 PM  
**To:** 'Ahmad Ali'  
**Cc:** Franca Piazza  
**Subject:** RE: 89 Loyalist Drive - Permitted Use and Zoning Inquiry

Hello Ahmed,

I am the planner processing the Site Specific Zoning By-law Amendment Application (Z.1215.04) that has been submitted for 65-89 Loyalist Trail proposing the addition of *commercial fitness centre* to the list of permitted uses on the site, restricting the size of said use to 50% of the Total Gross Floor Area (GFA), and amending the parking requirement for said use.

The *Planning Act* requires municipalities to make decisions on privately-initiated Zoning By-law Amendments Applications within 90 days of the application being deemed complete; otherwise the applicant may appeal the matter to the Ontario Land Tribunal for a non-decision. In this case, a decision would need to be made by December 26, 2024. I think it would be fair to assume that a decision would be made in early December to avoid the situation of a non-decision.

There are public notice requirements for this application and, because you have expressed an interest in being kept up-to-date, I am cc'ing Franca Piazza, Legislative Coordinator, who takes care of public notices, to include you on the notification list.

I will advise you that the policy framework is such that the planned function of these employment lands should be maintained (i.e., protecting lands for industrial uses), so I'm unsure whether I will be able to support the proposal of additional non-employment uses on this site, as the zoning already permits a range of commercial uses as-of-right. However, my professional opinion is a recommendation and the decision whether to approve or deny the amendment is ultimately up to Town Council. If Council's decision is to deny the amendment, the applicant may appeal Council's decision to the Ontario Land Tribunal.

In terms of next steps, the application will be circulated to staff and the public for their comments shortly. I would encourage you to submit formal comments on the application in the manner to be prescribed in the Notice of Complete Application, which will be issued within the next few weeks.

Feel free to follow up with me in about 4 weeks' time, and I will be able to provide you with an update on the outcome of the review at that point.

Please let me know any other questions.

Thank you,  
 Delia

**Delia McPhail, MCIP, RPP**  
**Planner**  
**Planning & Development**

Town of Oakville | 905-845-6601, ext. 3785 | f: 905-338-4414 | [www.oakville.ca](http://www.oakville.ca)

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**From:** Planning Apps <[planningapps@oakville.ca](mailto:planningapps@oakville.ca)>  
**Sent:** Wednesday, September 25, 2024 9:24 AM  
**To:** Delia McPhail <[delia.mcphail@oakville.ca](mailto:delia.mcphail@oakville.ca)>  
**Subject:** 89 Loyalist Drive - Permitted Use and Zoning Inquiry

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**From:** Ahmad Ali [REDACTED]  
**Sent:** Tuesday, September 24, 2024 11:46 AM  
**To:** Planning Apps <[planningapps@oakville.ca](mailto:planningapps@oakville.ca)>  
**Subject:** Fw: [EXTERNAL] 89 Loyalist Drive - Permitted Use and Zoning Inquiry

You don't often get email from [REDACTED] [Learn why this is important](#)

Hello,

Would you kindly be able to share any information available regarding the Zoning By-Law amendment application to change the zoning for 89 Loyalist Dr?

Do you have an expected date for a decision?  
Any idea of how likely is it to proceed?  
How can I follow / stay up to date on this application?

Any information would be helpful

Best,  
Ahmad

---

**From:** Zoning Requests <[zoningrequests@oakville.ca](mailto:zoningrequests@oakville.ca)>  
**Sent:** September 24, 2024 11:00 AM  
**To:** 'Ahmad Ali' [REDACTED]  
**Subject:** RE: [EXTERNAL] 89 Loyalist Drive - Permitted Use and Zoning Inquiry

Hi Ahmad,

The property at **89 Loyalist** is zoned **LE sp:45**. The permitted uses in the LE zone can be found in [Section 6 – Permitted Uses](#) under the LE column. Currently a commercial fitness centre is not permitted in the LE zone. However there is an application through the planning department for a Zoning By-Law amendment to include this use in the future.

Should you want information in regards to this application for a Zoning By-Law amendment you will need to contact planning at [planningapps@oakville.ca](mailto:planningapps@oakville.ca)

Regards,

**Gonzalo Marco**  
**Zoning Plans Examiner**  
**Building Services**  
Town of Oakville | 905-845-6601, ext.3177 | [www.oakville.ca](http://www.oakville.ca)

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Although effort has been made to ensure that the information contained herein is correct, the Town of Oakville does not accept responsibility for any errors, omissions or inaccuracies. Individuals are encouraged to review the Town's zoning by-law themselves at [www.oakville.ca/zoning](http://www.oakville.ca/zoning). Formal confirmation of zoning compliance is only provided through Zoning Certificate of Occupancy or Building Permit issuance, where required.

---

**From:** Ahmad Ali [REDACTED]  
**Sent:** September 24, 2024 10:23 AM  
**To:** Zoning Requests <[zoningrequests@oakville.ca](mailto:zoningrequests@oakville.ca)>  
**Subject:** [EXTERNAL] 89 Loyalist Drive - Permitted Use and Zoning Inquiry

You don't often get email from [REDACTED]. [Learn why this is important](#)

Hello,

I am currently interested in leasing a space at 89 Loyalist Drive and would appreciate your help understanding the permitted uses for the property I'm considering.

I am looking to establish a racket sport facility (padel tennis, pickleball, etc) at the location. Would you kindly confirm:

- The required zoning for such use?
- If this use would be permitted at this location?
- If not, are there any exemption or adjustment process that I can follow to allow for such use?

The location is classed for Light Employment (LE sp:45) and is ideal for a sports facility. Unfortunately not a lot of spaces offer the same space, ceiling and column clearances, and proximity to the core.

Looking forward to hearing from you. Happy to discuss over the phone if easier.

Best,

Ahmad  
[REDACTED]

## Delia McPhail

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**From:** Darko Stojic [REDACTED]  
**Sent:** Wednesday, October 23, 2024 12:32 PM  
**To:** Town Clerks  
**Cc:** Davide Baldassarra  
**Subject:** [EXTERNAL] Application for a Proposed Zoning By-law Amendment 65-89 Loyalist Trail  
**Attachments:** 20241022170455.pdf

**Follow Up Flag:** Follow up  
**Flag Status:** Completed

Good afternoon,

We are in receipt of Application Note for Proposed zoning by-law amendment / 65-89 Loyalist Trail Z.1215.04 (attached file).

Please be aware that we would like to be notified of decision on this matter.

Best regards,

Darko Stojic | [REDACTED]

*Junior Project Management*

*Properties Division*

MELROSE INVESTMENTS INC.

145 REYNOLDS STREET, SUITE 400 | OAKVILLE, ONTARIO | L6J 0A7 | [REDACTED]

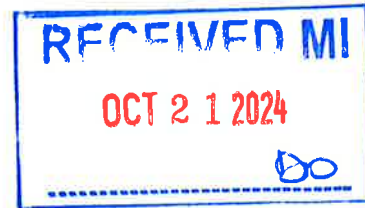


[www.melroseinvestments.com](http://www.melroseinvestments.com) [REDACTED]

You may withdraw your consent to continue receiving emails at any time by replying to [REDACTED]

**Complete Application for a Proposed Zoning By-law Amendment**  
**65-89 Loyalist Trail**  
**Tafia Development Corporation**  
**Z.1215.04, Ward 7**

ROWHEDGE CONSTRUCTION LIMITED  
145 REYNOLDS ST SUITE 400  
OAKVILLE ON L6J 0A7



☒ RB  
☐ AC  
☐ sg  
☐ AM  
☐ DS

The Town of Oakville has received a complete application by Tafia Development Corporation for a proposed zoning by-law amendment.

The purpose of the zoning by-law amendment is to add Commercial Fitness Centre to the list of permitted uses on the property, to occupy a maximum Gross Floor Area (GFA) of 50% of all buildings and reduce the parking requirements for the site.

The effect of the application would allow for Commercial Fitness Centre uses such as children's indoor playgrounds, gymnastics clubs, and indoor sports areas (e.g., soccer, badminton, pickleball, basketball) within the existing buildings.

The subject land is located on the north side of Loyalist Trail and east of Loyalist Woods Park (opposite Channing Crescent and Eternity Way).

At this time there are no other applications, under the *Planning Act*, R.S.O. 1990, c. P.13, as amended, pertaining to the subject lands.

If a person or public body would otherwise have an ability to appeal the decision of the Town of Oakville to the Ontario Land Tribunal but the person or public body does not make oral submissions at a public meeting or make written submissions to Council c/o the Town Clerk at the Town of Oakville, Clerk's department, 1225 Trafalgar Road, Oakville, ON L6H 0H3 (Dropbox is located in front of Town Hall) or at [TownClerk@oakville.ca](mailto:TownClerk@oakville.ca) before the by-law is passed, the person or public body is not entitled to appeal the decision.

If a person or public body does not make oral submissions at a public meeting, or make written submissions to Council c/o the Town Clerk at the Town of Oakville, Clerk's department, before the by-law is passed, the person or public body may not be added as a party to the hearing of an appeal before the Ontario Land Tribunal unless, in the opinion of the Tribunal, there are reasonable grounds to do so.

Town of Oakville | 1225 Trafalgar Road, Oakville L6H 0H3 | 905-845-6601 | oakville.ca

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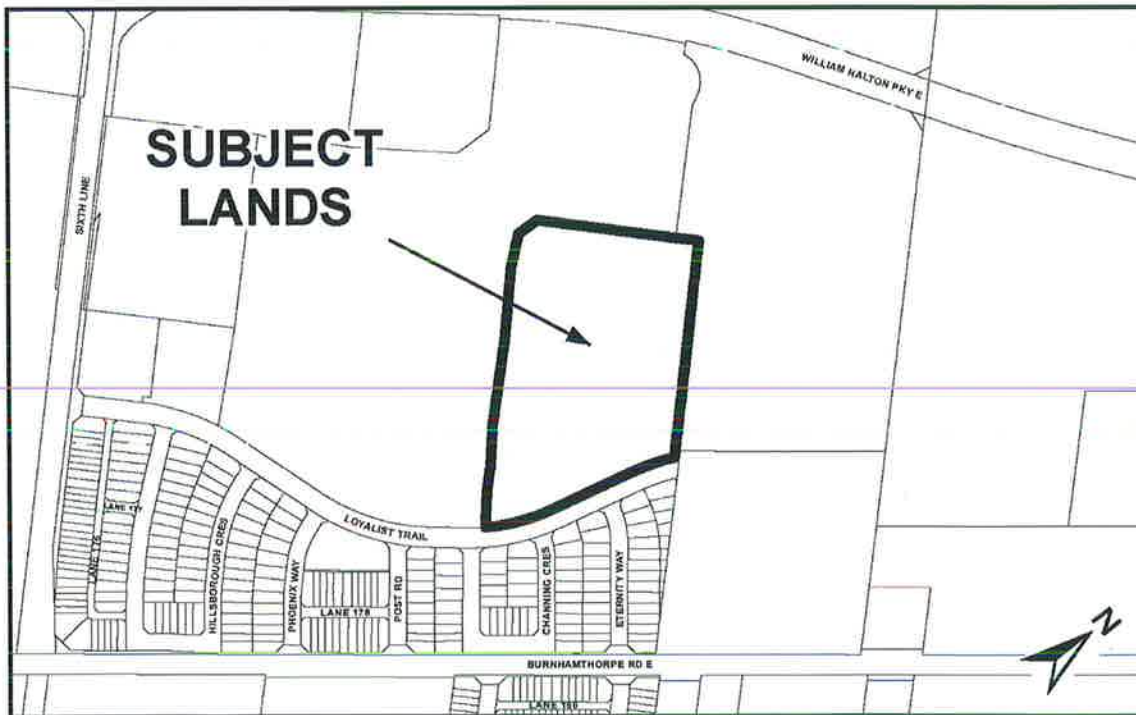


If you wish to be notified of the decision of the Town of Oakville on this matter, you must make a written request to the Town Clerk at the Town of Oakville, Clerk's department, 1225 Trafalgar Road, Oakville, ON L6H 0H3 or at [TownClerk@oakville.ca](mailto:TownClerk@oakville.ca).

For more information about this matter, including information about preserving your appeal rights, visit [www.oakville.ca](http://www.oakville.ca) and search "Z.1215.04", or contact Delia McPhail, Planner, Planning and Development department at 905-845-6601, ext. 3785 (TTY 905-338-4200) or at [delia.mcphail@oakville.ca](mailto:delia.mcphail@oakville.ca).

The personal information accompanying your submission is being collected under the authority of the *Planning Act*, R.S.O. 1990, c. P.13, as amended, and may form part of the public record which may be released to the public.

Dated at the Town of Oakville October 16, 2024



## Delia McPhail

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**From:** Majed Bar. [REDACTED]  
**Sent:** Thursday, November 14, 2024 1:56 PM  
**To:** Town Clerks  
**Cc:** Delia McPhail; Majed Abukhater  
**Subject:** [EXTERNAL] Zoning By-law Amendment Application - 65-89 Loyalist Trail (Z.1215.04)

M1 Basketball Ltd  
1185 Westdale Rd,  
Oakville, ON L6L 6P5



November 14, 2024

Town Clerk  
Town Clerk's Department  
1225 Trafalgar Road  
Oakville, Ontario L6H 0H3  
Email: [townclerk@oakville.ca](mailto:townclerk@oakville.ca)

*Re: Zoning By-law Amendment Application - 65-89 Loyalist Trail (Z.1215.04)*

**Dear Members of the Oakville Town Council,**

We are writing to you regarding the Zoning By-law Amendment application submitted by Tafia Development Corp. for 65, 71, 77, 83, and 89 Loyalist Trail (Z.1215.04). I would like to express my strong support for this application, particularly in relation to the proposed establishment of a Shoot 360 franchise at this location. I hope that staff will bring these comments forward at the public meeting and include them in the report to the Planning & Development Council on November 25, 2024, followed by a subsequent Recommendation Report.

The proposed Shoot 360 facility would be the first of its kind in Ontario and the second in Canada, offering advanced basketball training technology in a membership-based environment to the Oakville community. This project is not just about introducing a unique sports and fitness experience; it is also about fostering local economic growth and creating meaningful employment opportunities for Oakville residents.

### **Community and Employment Benefits:**

- **Job Creation for Youth:** We are looking at hiring around 20-25 individuals, including both part-time and full-time roles. These positions will be ideal for high school students, university students, and recent graduates. They will have the opportunity to work as coaches, trainers, and facility management staff, gaining valuable experience while contributing positively to the community.
- **Supporting Education and Athletic Development:** The technology used in Shoot 360 is utilized by NBA players to enhance their shooting and training techniques. For the first time, this cutting-edge technology will be available to the public, offering local athletes a chance to experience the same



high-level training. Additionally, we plan on inviting NBA players to visit the facility to run camps and clinics, providing young athletes with a unique opportunity to learn from professionals. This will make Oakville a focal point for athletic excellence and innovation.

- **Economic Contribution:** We anticipate individuals and teams from across the Greater Toronto Area (GTA) will use the facility. Attracting people will undoubtedly support economic activity for local businesses such as restaurants, shops, and other services in the area. The steady influx of visitors is expected to enhance the vibrancy and economic vitality of Oakville, providing a boost to local businesses.

### **A Personal Investment in Oakville's Future:**

My business partner and I are two entrepreneurs investing our savings into this venture because we truly believe in its potential. We see Shoot 360 as more than just a business; it's an opportunity to create something that our friends, family, and the Oakville community can be proud of. Our goal is to build this facility into a place that embodies our vision for growth, excellence, and community engagement, making a positive and lasting impact in Oakville.

I strongly believe that the introduction of Shoot 360 in Oakville aligns with the town's vision of creating a thriving and inclusive community. It offers a unique opportunity to support youth employment, provide cutting-edge training facilities, and boost the local economy. I am confident that this facility will become a valued asset in Oakville, enhancing our community's reputation as a place that supports innovation and development.

Thank you for considering my comments. I look forward to the positive progress of this project and am happy to provide any further information or support as needed.

Kind regards,

Majed Barhoush & Majed Abukhater  
Owners

## Delia McPhail

---

**From:** Matt Cappuccitti [REDACTED]  
**Sent:** Tuesday, February 4, 2025 10:09 AM  
**To:** Town Clerks  
**Subject:** [EXTERNAL] File Number Z.1215.04

**Follow Up Flag:** Follow up  
**Flag Status:** Completed

Good morning,

I hope this email finds you well. Is it possible to be notified regarding the official decision on the above noted file?

Thank you in advance,

Matt

**Matthew Cappuccitti**  
Commercial Real Estate Broker

Email: [REDACTED]  
Mobile CAD: [REDACTED]  
Mobile USA: [REDACTED]

4711 Yonge Street, 2nd Floor, Toronto, ON M2N 6K8  
Website: [www.mattcappuccitti.exprealty.com](http://www.mattcappuccitti.exprealty.com)

## REPORT

### Planning and Development Council

Meeting Date: March 17, 2025

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**FROM:** Planning and Development Department

**DATE:** March 4, 2025

**SUBJECT:** Old Oakville Heritage Conservation District By-law and Plan and Guidelines; By-law 2025-054 and By-law 2025-063 – March 17, 2025

**LOCATION:** Old Oakville

**WARD:** Ward 3

Page 1

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### RECOMMENDATION

1. That By-law 2025-054, a by-law to amend By-law 1981-144, a by-law to designate an area of the Town as a Heritage Conservation District under Section 41(1) of the Ontario Heritage Act, to include Part IV properties at 8 Navy Street, 110-114 King Street and a Portion of 144 Front Street, be passed; and
2. That By-law 2025-063, a by-law to adopt the Old Oakville Heritage Conservation District Plan and Guidelines dated March 2025 under subsection 41.1 (2) of the Ontario Heritage Act, be passed.

### KEY FACTS

The following are key points for consideration with respect to this report:

- The Old Oakville Heritage Conservation District has been in place since it received Council approval in 1981 and OMB approval in 1982.
- The Old Oakville Heritage Conservation District Plan and Guidelines that serves to manage change in Old Oakville has not been updated since that time.
- The planning process to update the District Plan and Guidelines was started in 2021 and consists of two major components – the District Study and a new District Plan and Guidelines.
- In February 2024, Council approved the District Study component that describes and evaluates the cultural heritage value of the Old Oakville Heritage Conservation District.

- On February 3, 2025, the statutory public meeting for the Old Oakville Heritage Conservation District Plan and Guidelines was held at Planning and Development Council.
- The Heritage Oakville Advisory Committee was consulted on the District Plan and Guidelines at its meeting on February 25, 2025. Feedback received at the statutory public meeting, the Heritage Oakville Advisory Committee and provided by stakeholders has been considered for the final draft of the District Plan and Guidelines in Schedule 'A' of By-law 2025-063.
- Highlights of the draft Old Oakville Heritage Conservation District Plan and Guidelines are presented in this report along with how feedback has been addressed.
- Two by-laws have been attached for Council's consideration: Appendix A: By-law 2025-054 to amend the Old Oakville Heritage Conservation District to include the Part IV designated properties that were previously excluded from the District; and Appendix B: By-law 2025-063 to adopt the Old Oakville Heritage Conservation District Plan and Guidelines.

## **BACKGROUND**

Established in 1981, the Old Oakville Heritage Conservation District (the District) is Oakville's first designated district, and one of the first in Ontario. This historically significant area extends south of Robinson Street to the lakefront from the Sixteen Mile Creek in the west to Allan Street in the east. The intimate atmosphere of the District provides two picturesque waterfront parks, Dingle Park and Lakeside Park, as well as Oakville Museum at Erchless Estate.

The District features early vernacular homes, nineteenth-century lakeside cottages, turn-of-the-century luxury houses and churches. Architectural styles are diverse and include 19th-century Georgian, Neo-Classical, Victorian and Classical Revival, many of which are a vernacular interpretation of the style. "Vernacular" architecture is a style of regional or local building that uses conventional supplies and resources from the region in which the building is situated.

The Old Oakville Heritage Conservation District Plan (District Plan) has served the community well for 40 years and has been instrumental in protecting the character of the area. With changes to the *Ontario Heritage Act* (the Act) in 2005 and 2021, the District Plan requires updating to meet the requirements of current legislation. There are two components required for a heritage conservation district in Ontario.

The first component is a study, as defined under section 40 of the Act. The Old Oakville Heritage Conservation District Study was completed and approved by Council at the [Planning and Development Council meeting of February 5, 2024](#). The Study recommended that the boundary of the Old Oakville Heritage Conservation District remain the same, with the addition within the boundary of the Part IV

properties that were previously excluded from the District because they were individually designated under Part IV and could not then be included within the District. Now s.41 (2) of the *Act* permits them to be included. These properties are 8 Navy Street, 110-114 King Street which form the Erchless Estate and a portion of 144 Front Street that contains the Merrick Thomas House and the Historic Post Office.

At that same meeting, Council also directed staff to complete the District Plan Update and to create a revised District Plan and Guidelines. Work on the new Plan and Guidelines has been on-going since early 2024.

The statutory public meeting for a new District Plan as required by subsection 41.1 (6) b of the *Act* was held on [February 3, 2025 at Planning and Development Council](#). The full list of statutory requirements for heritage conservation district plans and processes can be found within that staff report. Comments provided to staff in advance and at that meeting have been considered for the revisions to the District Plan and Guidelines that is attached as Schedule 'A' of By-law 2025-063.

At the Heritage Oakville Advisory Committee meeting on February 25, 2025, additional feedback was provided, which has been included in this final draft of the District Plan and Guidelines that is attached as Schedule 'A' of By-law 2025-063.

## **COMMENTS**

### **Highlights of the Draft District Plan and Guidelines**

*Section 5 – Guidelines for Managing Change* will be the most utilized part of the document. The formulation of this section has benefitted from a high degree of engagement, review and recommendations by key stakeholders and staff.

Section 5 features five Character Areas that were identified in the Heritage Conservation District Study. These areas provide a framework that acknowledges distinct features through the historic, streetscape and landscape analysis of Old Oakville's evolving urban fabric. Specific guidance is provided for each Area:

- Area 1 - Waterfront Open Space
- Area 2 - Old Oakville Settlement Pattern
- Area 3 - Gully Landscape
- Area 4 - Mixed Residential Development Pattern
- Area 5 - St. Andrew's Traditional

The balance of Section 5 provides direction for managing change based on a classification of properties within the District as contributing or non-contributing.

Within the District, 90 properties (70% of all properties) have been identified as contributing properties.

Contributing properties exhibit physical heritage attributes which directly contribute to the cultural heritage value or interest of the District. They support the identified cultural heritage values from the Statement of Cultural Heritage Value or Interest, and they have met two or more criteria as identified as per the *Ontario Heritage Act* O. Reg. 9/06.

Non-contributing properties do not exhibit that threshold amount of cultural heritage value as per the criteria in O. Reg. 9/06. Non-contributing properties are included in the guidelines to provide appropriate consideration of proposed changes to the entire district, especially surrounding properties.

Section 5 continues with guidelines regarding property level changes as well as direction for classes of alterations, use of specific materials and additions to buildings and properties and landscape guidelines.

### **Personnel, Participants and Public Engagement for the District Plan Update**

For a complete description of all engagement for the update to the Old Oakville Heritage Conservation District, please refer to the staff report for the Statutory Public Meeting that took place on [February 3, 2025 at Planning and Development Council](#).

At the February 3, 2025, Statutory Public Meeting, Council identified areas that they would like additional information in advance of making a decision.

#### **Summary of additional information requested by Council:**

- Consideration should be given to the feedback from the Oakville Lakeside Residents' Association regarding height, scale and massing
- Staff should be given more credit for writing the Plan and Guidelines

These items for consideration have been addressed in the draft of the District Plan and Guidelines attached to this report. Staff has engaged with the Oakville Lakeside Residents' Association regarding the changes.

### **CONCLUSION**

As the boundaries of the Old Oakville Heritage Conservation District are only changing to include the previously excluded Part IV designated properties, which is now permitted through section 41 (2) of the Act, staff recommends that the original heritage designation By-law 1981-144 be amended to adjust the mapping to show those properties now included within the Old Oakville Heritage Conservation District through By-law 2025-054.

Staff also recommends the passing of By-law 2025-063 to adopt the Old Oakville Heritage Conservation District Plan and Guidelines under subsection 41.1 (2) of the Act. The District Plan and Guidelines meet all the requirements of Part V of the Act and staff are confident that it is a defensible and reasonable plan for the management of heritage resources in the Old Oakville Heritage Conservation District.

**CONSIDERATIONS:**

**(A) PUBLIC**

The public and stakeholders for the Old Oakville Heritage Conservation District will be notified of the passage of the proposed by-laws in accordance with the *Ontario Heritage Act* and town policy.

**(B) FINANCIAL**

There are no financial considerations associated with this item.

**(C) IMPACT ON OTHER DEPARTMENTS & USERS**

The District Plan and Guidelines may impact the work of Building Services, Parks and Open Space, Forestry Services, Roads and Works, Transportation and Engineering, and Legal. Feedback from these departments has been provided and incorporated into the District Plan and Guidelines.

**(D) COUNCIL STRATEGIC PRIORITIES**

This report addresses Council's strategic priorities of Accountable Government, Community Belonging and Environmental Sustainability.

**(E) CLIMATE CHANGE/ACTION**

Town Council declared a Climate Emergency in June 2019 for the purpose of strengthening Oakville's community commitment to reduce carbon footprints. This update to the Old Oakville Heritage Conservation District Plan and Guidelines supports the conservation of Oakville's cultural heritage resources, including built heritage. The greenest and most sustainable building is the one that already exists.

**APPENDICES**

Appendix A- By-Law 2025-054

Appendix B- By-Law 2025-063 with Schedule 'A' - Old Oakville Heritage Conservation District Plan and Guidelines dated March 2025

Prepared by:

Susan Schappert, CAHP, RPP, MCIP  
Heritage Planner

Recommended by:

Kirk Biggar, RPP, MCIP  
Manager, Policy Planning and Heritage

Submitted by:

Gabe Charles, RPP, MCIP  
Director, Planning and Development



THE CORPORATION OF THE TOWN OF OAKVILLE

BY-LAW NUMBER 2025-054

A by-law to amend By-law 1981-144, a by-law to designate an area of the Town as a Heritage Conservation District under Section 41(1) of the Ontario Heritage Act, to include Part IV properties at 8 Navy Street, 110-114 King Street and a Portion of 144 Front Street.

**WHEREAS** the Town of Oakville passed By-law 1981-144 on October 20, 1981 to designate an area of the Town as a Heritage Conservation District under section 41 (1) of the *Ontario Heritage Act*, RSO 1980, c.337, "the *Act*" which By-law was amended by By-law 1982-44 on March 1, 1982 and by By-law 1987-266 on September 1, 1987 all of which by-laws were approved as required by the Ontario Municipal Board;

**WHEREAS** the Official Plan for the Town of Oakville contains policies relating to the establishment of heritage conservation districts;

**WHEREAS** section 41 (2) of the *Act* states that a property that is designated under Part IV may subsequently be included in an area designated as a heritage conservation district;

**WHEREAS** it is desirable that certain properties which were excluded from the heritage conservation district in 1981 until now as they were already designated under Part IV of the *Act*, now be included in the heritage conservation district being 8 Navy Street, 100-114 King Street and a portion of 144 Front Street as permitted by the *Act*;

**COUNCIL ENACTS AS FOLLOWS:**

1. That Schedule "A" to By-law 1981-144 (as amended), be further amended to include 8 Navy Street, 100-114 King Street and a portion of 144 Front Street as shown in yellow in Schedule "A" hereto.
2. That the Old Oakville Heritage Conservation District therefore be as set out in Schedule "B" hereto.
3. This Amending By-law shall come into force in accordance with section 41 of the *Act*, either on the day following the last day of the prescribed appeal period, or as otherwise provided by sections 41 (5) and (10) of the *Act*.

PASSED this 17th day of March, 2025

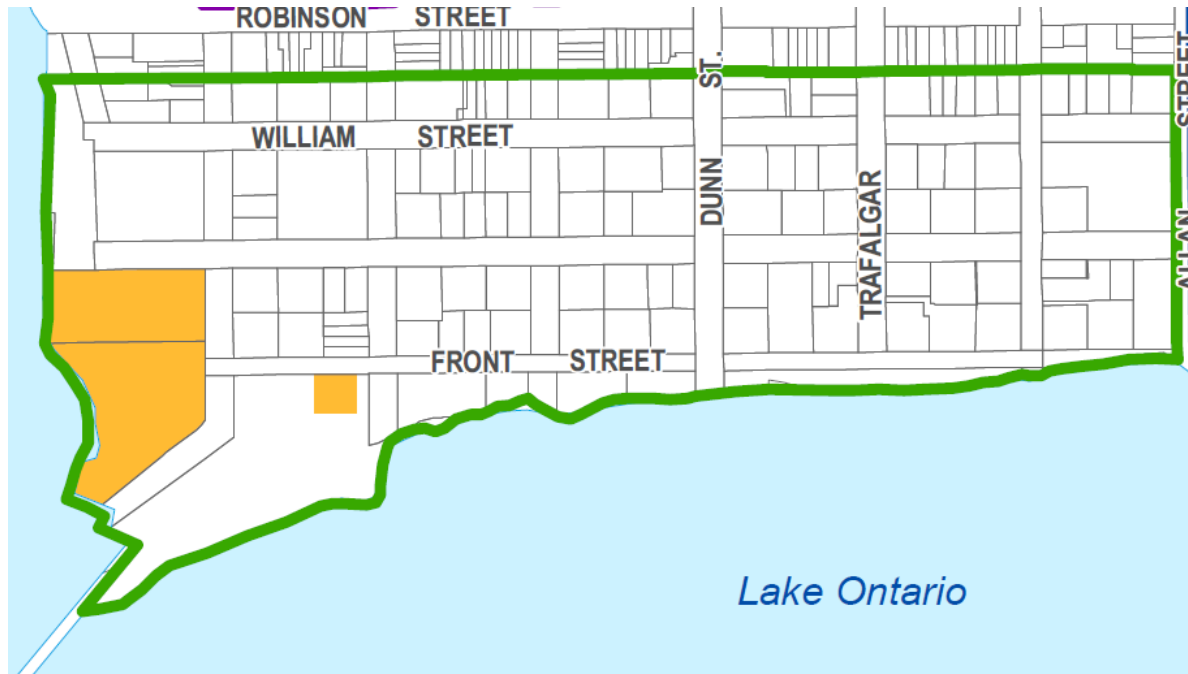
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MAYOR

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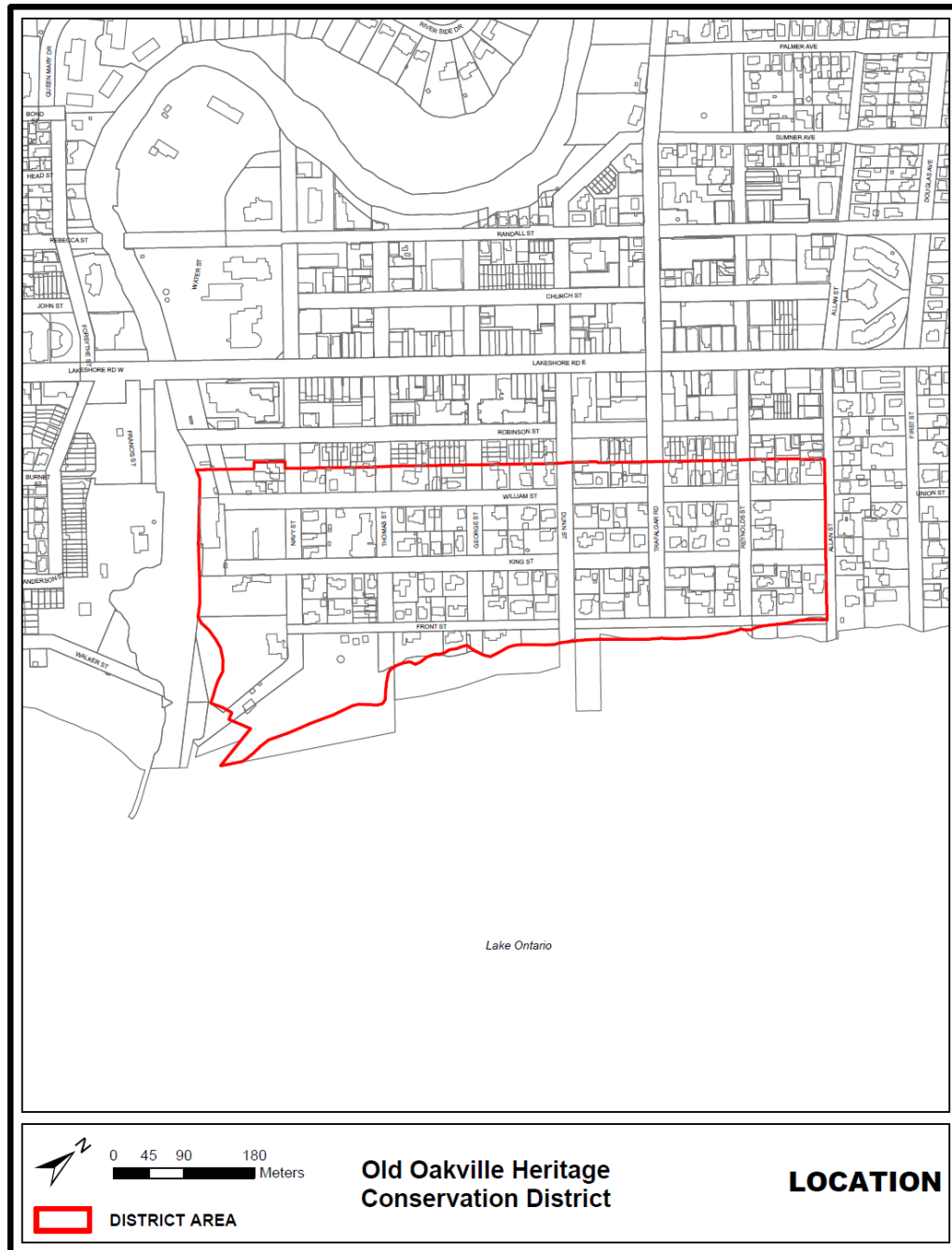
CLERK

**SCHEDULE "A" TO  
BY-LAW 2025-054**



**Yellow areas represent the properties at 8 Navy Street, 110-114 King Street and a portion of 144 Front Street that are being added to the existing Old Oakville Heritage Conservation District through this by-law amendment**

## SCHEDULE "B" TO BY-LAW 2025-054



**Old Oakville Heritage Conservation District as of March 17, 2025  
Consolidated District Map**

THE CORPORATION OF THE TOWN OF OAKVILLE

BY-LAW NUMBER 2025-063

A By-law to adopt the Old Oakville Heritage Conservation District Plan and Guidelines dated March 2025 under subsection 41.1 (2) of the Ontario Heritage Act.

**WHEREAS** pursuant to subsection 41.1 (2) of the *Ontario Heritage Act*, R.S.O., 1990, c. O.18, if, on or before the day the *Ontario Heritage Amendment Act*, 2005 received Royal Assent, the council of a municipality had passed a by-law designating one or more heritage conservation districts, it may pass a by-law adopting a heritage conservation district plan for any one of the designated districts

**WHEREAS** the Town of Oakville on October 20, 1981 passed By-law 1981-144 as later amended by By-law 1982-44 on March 1, 1982 and by By-law 1987-266 on September 1, 1987 and By-law 2025-054 on March 17, 2025 to designate an area of the Town as a Heritage Conservation District under section 41 (1) of the *Ontario Heritage Act* RSO 1980, c.337, “the Act”;

**WHEREAS** the Official Plan for the Town of Oakville contains policies relating to the establishment of heritage conservation districts;

**WHEREAS** the Old Oakville Heritage Conservation District Plan meets the requirements of subsections 41.1 (5) and (5.1) of the *Act*, regarding the contents of a heritage conservation district plan;

**WHEREAS** the requirements of subsection 41.1 (6) of the *Act* regarding consultation have been met including information relating to the proposed Old Oakville Heritage Conservation District Plan, including a copy of the proposed Plan, being made available to the public, at least one public meeting being held with respect to the proposed Plan on February 3, 2025, and the Town of Oakville municipal heritage committee, the Heritage Oakville Advisory Committee, being consulted with respect to the proposed Plan at their meeting on February 25, 2025;

**WHEREAS** the requirements of subsections 42.1 (7), (8) and (9) of the *Act* have been met regarding notice of public meeting with the Town Clerk having given notice of a public meeting to discuss the proposed Old Oakville Heritage Conservation District Plan, the public meeting having taken place at least 20 days after the said notice was given and any person attending the public meeting having been given an opportunity to make oral representations with respect to the Plan;

**WHEREAS** the requirements of subsection 42.1 (10) of the *Act* have been met with Town Council having ensured that information was provided to persons attending the public meeting explaining that, in accordance with section 41 (8) of the *Act*, a person who did not raise objections to the adoption of the proposed Old Oakville Heritage Conservation District Plan by making oral representations under section 41 (9) of the *Act* or written submissions under section 41 (11) of the *Act* may later be denied an opportunity to appeal the passing of this by-law adopting the Plan under section 41 (2) of the *Act*;

**WHEREAS** the requirements of subsection 42.1 (11) of the *Act* have been met by Town Council considering any written submissions with respect to the proposed Old Oakville Heritage Conservation District Plan before this by-law adopting the Plan was made;

**WHEREAS** the requirements of subsection 42.1 (12) of the *Act* have been met by Town Council providing copies of the proposed Old Oakville Heritage Conservation District Plan to any person upon request;

**COUNCIL ENACTS AS FOLLOWS:**

1. The Plan set out in Schedule “A” attached hereto and forming part of this by-law is hereby adopted as the Heritage Conservation District Plan and Guidelines for the Old Oakville Heritage Conservation District.
2. This by-law shall come into force in accordance with section 41 of the *Ontario Heritage Act*, R.S.O. 1990, either on the day following the last day of the prescribed appeal period or as otherwise provided by section 41(10) of the *Act*.

PASSED this 17th day of March, 2025

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MAYOR

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CLERK

**SCHEDULE “A” to BY-LAW 2025-063**

***(Heritage Conservation District Plan and Guidelines follows on next page)***

**March 2025**

# **Old Oakville Heritage Conservation District Plan and Guidelines**

Prepared by:  
**Planning and Development Services**  
**Town of Oakville**  
1225 Trafalgar Road  
Oakville, ON L6H 0H3

With support from:  
**Oakville Lakeside Residents Association**



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Cover Image: 1835 Plan of Oakville, Edward Pa

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## GLOSSARY OF ABBREVIATIONS

ARA – Archaeological Research Associates Ltd.  
CHVI – Cultural Heritage Value or Interest  
CHL – Cultural Heritage Landscape  
CHRIA – Cultural Heritage Resource Impact Assessment  
HCD – Heritage Conservation District  
MCM – Ministry of Citizenship and Multiculturalism  
OBC – Ontario Building Code  
OHA – *Ontario Heritage Act*  
O. Reg. – Ontario Regulation  
OP – Official Plan  
PPS – Provincial Policy Statement  
S&G – Standards and Guidelines for the Conservation of Historic Places in Canada

Full Glossary Found in Section 8.



# 1 | INTRODUCTION

## 1.1 Background of the Old Oakville Heritage Conservation District

On January 2, 1979, Council passed By-law 1979-003, being a bylaw of intent defining the area south of Robinson Street, east of the Sixteen Mile Creek and west of Allan Street to be studied as a Heritage Conservation District (HCD) under the *Ontario Heritage Act*.

Three public meetings were held with the Local Architectural Conservation Advisory Committee and the Planning and Development Services Department to review the Study.

On July 7, 1980, Council adopted the final report on the HCD and stipulated that the document was to be a policy document for the administration of alterations to buildings within the Old Oakville Heritage Conservation District (the District or the Old Oakville HCD).

The Ministry of Citizenship and Culture later endorsed the document on February 16, 1981. By-laws 1981-144 and 1982-044, which formally designated the Old Oakville Heritage Conservation District, were approved by the Ontario Municipal Board on July 5, 1982.

The Old Oakville HCD is a largely residential area that includes 128 lots as shown in Map 1.

Since the Old Oakville HCD By-law was passed, there have been numerous changes to heritage legislation, including updates to the *Ontario Heritage Act* in 2005, 2021 and 2023. To ensure the Old Oakville HCD follows current heritage practices, a new HCD Study and a new Plan and Guidelines for the District is required.

The HCD Study was approved by Town Council on February 5, 2024. The HCD Study includes a Statement of Cultural Heritage Value or Interest which outlines the heritage value of the district, and its heritage attributes. The Statement is supported by an inventory of properties, as well as the development of five Character Areas and a streetscape overlay within the District.

This document is the second part of the District update process: the updated Plan and Guidelines (the Plan). The Plan provides the basis for the careful management and protection of the area's cultural heritage value and its heritage attributes, including buildings, spaces, and landscape features.

This Plan has been developed with the community through an extensive consultation process (see Appendix A).



## 1.2 What is a Heritage Conservation District?

The *Ontario Heritage Act* (OHA or the Act) is the key provincial legislation that enables municipalities to conserve, protect, and manage heritage properties and areas. There are two parts to the Act that concern cultural heritage: Part IV enables a municipality to designate individual properties that are of cultural heritage value or interest; and Part V enables a municipality to designate groups or areas of properties that demonstrate cultural heritage value.

Prior to designating a district, it is required by the Act to study an area to identify the Cultural Heritage Value or Interest (CHVI) of a prospective district.

Ontario Regulation 9/06 as amended by Ontario Regulation 569/22 sets out the criteria for designation of an HCD. Specifically, 3. (2).1 notes that 25 percent or more of the properties within the boundaries must meet two or more of those criteria.

The Town of Oakville's Official Plan, *Livable Oakville*, requires that cultural heritage resources, including HCDs, be protected and conserved per applicable legislation and recognized heritage protocols.

The Town of Oakville has a Cultural Heritage Landscape Strategy, further described in Section 4.9. The Old Oakville HCD is a significant cultural heritage landscape, as all HCDs are. This is because HCDs include not only built structures, but also natural heritage features, lot patterns and setbacks, transportation routes and other associated patterns of development, and recognize the importance of the landscape as a whole.

The purpose of this Plan is to guide and manage change to protect and conserve the cultural heritage value and heritage attributes of the District.

## 1.3 Intent of the Plan

HCDs focus on the conservation of cultural heritage resources within the District boundary. This designation protects what exists and manages change to prevent negative impacts to the District's cultural heritage value and heritage attributes.

An HCD does not keep an area 'frozen in time' or require restoration of buildings to a specific time. It is intended to permit responsible, meaningful changes that are compatible with the character of the District.

HCDs are concerned with the visible heritage attributes from the public realm, including buildings, landscapes and open spaces. District designation does not impact interior alterations.

This Plan is intended to provide the framework to maintain and enhance the District's heritage attributes for the overall benefit of the community and future generations.

This Plan will be used when a property owner decides to alter their property.

## 1.4 OHA Requirements: Heritage Conservation District Plans

As noted in the HCD Study, the Plan contains a number of provisions that satisfy the requirements of Subsection 41.1(5) of the *Ontario Heritage Act*, including the following:

- A statement of the objectives to be achieved in maintaining the area as a Heritage Conservation District (Section 0)
- A statement explaining the cultural heritage value or interest of the Heritage Conservation District (Section 2.1)
- A description of the heritage attributes of the Heritage Conservation District and of properties in the District (Section 2.2)
- Categorization of properties according to whether they are ‘contributing’ (having cultural heritage value) or ‘non-contributing’ (not having cultural heritage value) (Section 2.3)
- Design guidelines for alterations and additions to buildings and structures that are considered to have heritage value (Section 5.3)
- Design guidelines for alterations and additions to buildings and structures that are considered to have no or limited heritage value (Section 5.4)
- Guidelines on new construction as infill development (Section 5.5)
- Guidelines on demolition and removal of buildings and structures (Section 5.3.1.3 and 5.4.1.3)
- Landscape conservation guidelines for both public and private property (Sections 5.7 and 5.8)
- Guidelines for streetscape improvements within the HCD (Section 5.8)
- Consideration of the Character Areas in development of the guidelines (Section 5.2)
- Recommended changes to municipal planning and administrative procedures (HCD Study)
- Up-to-date information on current federal and provincial legislation and Town of Oakville processes and policies as they relate to the HCD (Section 4)
- Descriptions of alterations or classes of alterations that can be carried out without obtaining a heritage permit under section 42 of the *Ontario Heritage Act* (Section 6)

## 1.5 What is a Heritage Permit?

All properties in the Old Oakville HCD are designated under Part V of the *Ontario Heritage Act*. Under Section 42 of the *Ontario Heritage Act*, a permit is required for the ‘erection, demolition, removal or alteration of a building or structure or any part of a property’ within a District, regardless of whether the property is identified as ‘contributing’ or ‘non-contributing’.

Property owners should use this Plan when planning any maintenance work, repairs, alterations, additions or new construction on their property.

Owners of property within the Old Oakville HCD must apply for a heritage permit for any alteration that might result in the loss, removal, obstruction, replacement, damage or destruction of one or more heritage features on the property or impact the heritage attributes of the District.

Proposed alterations are reviewed by town staff and, if applicable, the Heritage Oakville Advisory Committee and Council, for consistency with this Plan, as well as any other applicable heritage legislation.



## 1.6 When is a Heritage Permit Required?

Property owners must contact Heritage Planning staff to arrange a pre-consultation meeting prior to the submission of a heritage permit application to ensure the proposed application meets the relevant requirements. Heritage Planning staff can also help owners with the application process.

Work within the District falls into three categories:

### 1. **Minor**

A minor heritage permit is needed when small to medium changes to a property are proposed that may have a negligible impact on the cultural heritage value of the District and/or its heritage attributes.

If the proposed minor work is deemed to be in keeping with this Plan, then Planning and Development Services staff, by way of the town's Delegation By-law 2022-021, can approve the work on behalf of Town Council.

This process can take 5-7 business days from the receipt of a complete application.

If Planning and Development Services staff cannot support a minor heritage permit application for approval or approval with conditions, the heritage permit application is required to follow the process for a major heritage permit application.

### 2. **Major**

A major heritage permit is needed when significant changes or modifications are proposed which will have, or has the potential to have, major impacts on the cultural heritage value and/or the heritage attributes of the District.

Major heritage permits are reviewed by the Heritage Oakville Advisory Committee whose recommendations are then provided to Town Council for the final decision.

This process can take 60-90 days from the receipt of a complete application.

### 3. **Exempt**

Section 7 outlines the works that are exempt from a heritage permit. Exempted actions may include proposed works that are:

- undertaken within a small area; or
- small in scope and confined to areas that are out of sight from public view; or
- constitute routine maintenance; or
- do not impact any cultural heritage values or attributes.

#### **Note:**

A heritage permit application/approval does not preclude consideration of other applicable policies and regulations, including (but not limited to) applications for zoning amendments, minor variances, site plan, building permits, sign permits, site alteration, or tree removal permits.

Town staff and, when needed, Heritage Oakville Advisory Committee members will review all properties with an understanding of unique site-specific considerations and should be made aware of other approvals required by the applicant at the time of submission for their heritage permit application.

## 1.7 How to Use the Plan

The flowchart below shows which sections of the Plan are used to manage proposed changes:

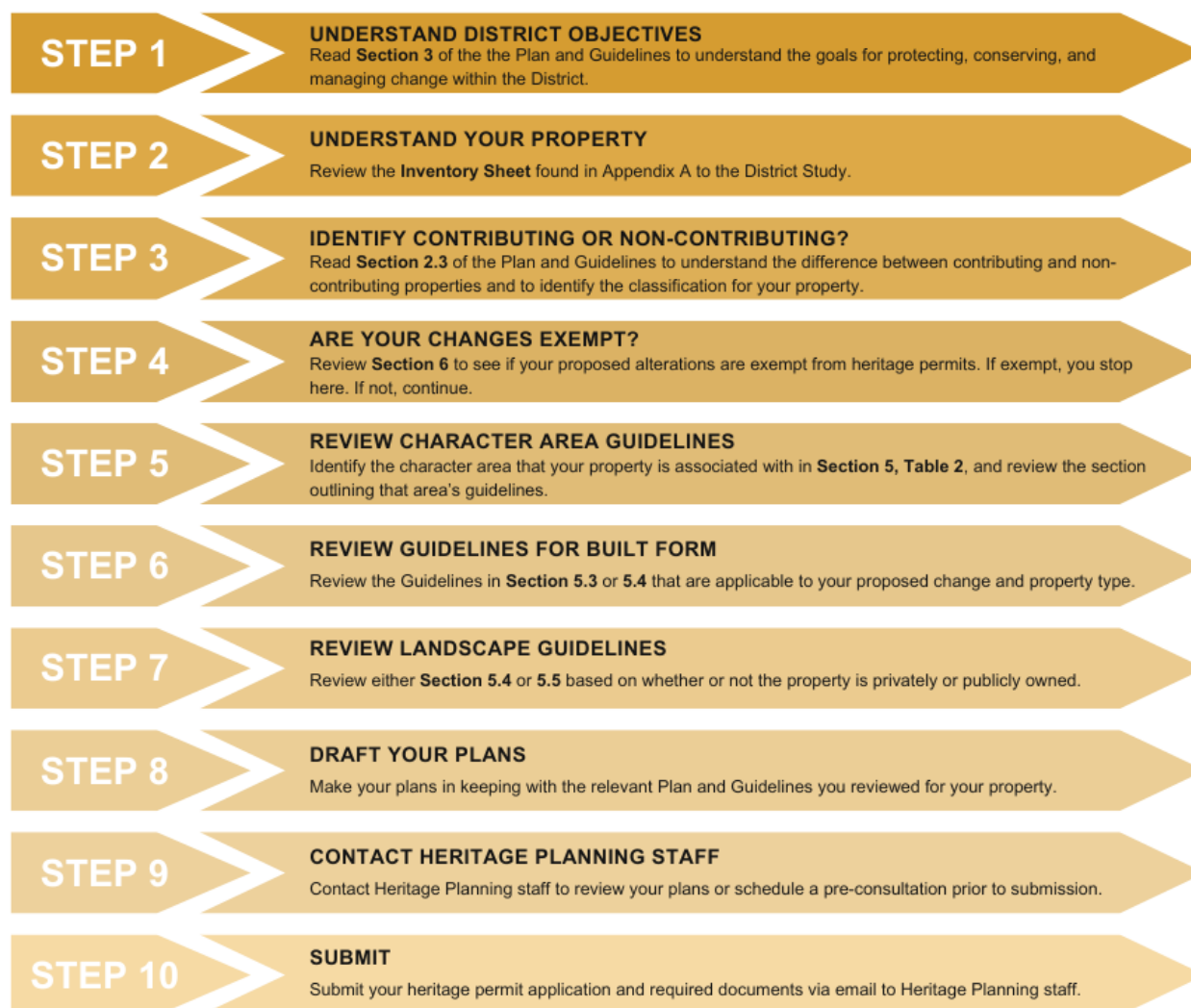


Figure 1: How to use the plan





# 2 | CULTURAL HERITAGE VALUE OF THE DISTRICT

## 2.1 Statement of Cultural Heritage Value or Interest

The Old Oakville Heritage Conservation District (the District) comprises 128 lots within an irregularly shaped boundary lined by Robinson Street, Allan Street, Navy Street and Lake Ontario. The lots within these boundaries represent several phases of boom and bust from the historic settler community. The District is largely residential except for two churches, a private recreational facility and several town-owned parks.

The District boundary follows the southern property line of Robinson Street, extending from Sixteen Mile Creek to the middle of Allan Street.

The District is a significant cultural heritage landscape. The undulating topography, Lake Ontario and Sixteen Mile Creek shorelines and views, mature vegetation, and two centuries of settlement are part of this organically evolved cultural heritage landscape.

The District's low-density scale and height, large parcel sizes, mixed building setbacks, openness at intersections, street tree cover, wide viewsheds, and permeable fencing types create an open landscape and pedestrian-scaled experience. This is enhanced by the historic road grid configuration, typology and block size.

The District can be understood through a Character Area framework that identifies distinct historic streetscape and landscape features. Five distinct streetscape and landscape Character Areas and one streetscape overlay were identified in the Old Oakville Heritage Conservation District Study: Waterfront Open Space, Old Oakville Settlement Area, Gully Landscape, Mixed Residential Development Pattern, St. Andrew's Traditional, and the Key Streetscape Overlay. The character areas and streetscape overlay contribute to the overall cultural heritage value or interest and the heritage attributes of the District.

### **Design and Physical Value**

**The Old Oakville Heritage Conservation District has design/physical value as a representative example of an organically evolved cultural heritage landscape. Its origins are a historic harbourside settler residential community dating from the early-to-mid 19<sup>th</sup> century. The District reflects a variety of architectural styles from the 19<sup>th</sup> and early 20<sup>th</sup> centuries that contribute to a varied, yet cohesive streetscape.**

The District has retained much of the lot patterns and historic street grid based on Deputy Surveyor J.H. Castle's 1833 Town of Oakville plan. Over half of the buildings of the District were constructed between 1830 and 1900 by families of different means and professions. Another quarter of the properties in the District were constructed in the second historic residential building boom between 1900 and 1930. This is reflected in the variety of architectural forms, including Georgian, Neoclassical, Classic Revival, Cottage, Gothic Revival, Edwardian and their vernacular expressions.

Viewed individually, these buildings represent their architectural styles. Viewed collectively, they create a robust and varied streetscape of residential buildings interspersed with churches and parkland. The District has a village-like feel due to its historic street grid, low-density residences with lower lot coverages. Modest-sized historic homes line intimate streetscapes designed for pedestrians. The character of the area also includes designed green spaces such as Lakeside Park, the Erchless Estate, and the semi-natural river harbour on Sixteen Mile Creek.

**The Old Oakville Heritage Conservation District has design/physical value as it contains a unique concentration of designed green spaces along the shoreline of Lake Ontario and steep banks along Sixteen Mile Creek.**

These designed spaces include: Lakeside Park, Market Square, Dingle Park and George Street Parkette, the Oakville Harbour on Sixteen Mile Creek, and the Erchless Estate. The evolution of this area began as a woodland through the 1868 inspired legacy of Mayor W. F. Romain who led the restoration of landscape to the pre-colonial canopy and Oakville's "grove-like aspect."

### **Historical and Associative Value**

**The Old Oakville Heritage Conservation District has historical/associative value due to its direct association with the themes of early 19<sup>th</sup> century commercial development, early 20<sup>th</sup> century industrial and residential booms, and 20<sup>th</sup> century recreation that have contributed to the overall growth and identity of the Town of Oakville.**

In the 19<sup>th</sup> century, the Oakville Harbour was not only home to important local industries and businesses but was also a busy shipping port for local exports including wheat and lumber. While the commerce for Oakville was initially founded on wood products and wheat, the area became associated with shipbuilding when William Chisholm established a shipyard on Sixteen Mile Creek at the top of Navy Street. Oakville became well known around the Great Lakes and elsewhere for the good quality of the large ships and schooners built here.

When commercial activity shifted to the rail line north of the downtown, the District stagnated for some years before residential growth began to climb again following the first World War. Commercial shipbuilding in the Oakville Harbour had declined due the popularity of the railway; however, the building of smaller watercraft continued to make Oakville well known. This supported re-orientation from industrial and commercial uses to publicly accessible recreation around the mouth of the harbour and along the Lake Ontario shoreline.

Oakville's position on Lake Ontario makes it a prime destination for day-trippers and vacationers. Lakeside Park was established in 1897, and several structures within the District have direct associations with recreation, such as the Oakville Club and Oakville Lawn Bowling Club. The area along the mouth of Oakville Harbour and shoreline of Lake Ontario remains an important recreational hub of the Town of Oakville.

**The Old Oakville Heritage Conservation District has historical/associative value through its direct associations with Oakville's founding family, the Chisholms, other settler families who were instrumental to the growth of Oakville in the 19<sup>th</sup> century, and William Sinclair Davis, one of Oakville's most successful businesspeople and real estate brokers.**

The District is part of the land purchased by Colonel William Chisholm in 1828 from the Mississauga Reserve lands to develop a port at the mouth of Sixteen Mile Creek. Oakville was the result of foresight and planning on the part of Chisholm, who was aware of the commercial possibilities of a harbour at the mouth of Sixteen Mile Creek and the value of the river's waterpower for manufacturing. Chisholm, considered the settler founder of Oakville, opened the first Customs House on the Erchless property in 1834. The townsite was surveyed in 1833 and Oakville grew around the commercial harbour with Market Square established in 1833. Many of the first buildings in the District were constructed by the shipbuilders and carpenters who worked in the Oakville Harbour.

The settler families who lived in the District influenced the early industrial, commercial and social growth of Oakville. These families include, but are not limited to: the Andrews, the Marlatts, the Williams, the McCorquodales, the Ryans, the Pattersons, the Sumners and the Barclays.

William Sinclair Davis, who lived just outside the District on First Street, was a key player in the residential boom in the District between 1900-1930 and constructed many of the buildings in the District that date to this period.

**The Old Oakville Heritage Conservation District has historical/associative value through its direct association with two places of worship that are significant to the community: St. Andrew's Catholic Church and St. Jude's Anglican Church.**

Each of these churches influenced the residential growth in the blocks surrounding them, provided social, educational and spiritual support for residents of the District and beyond, and have remained important in the community since their respective constructions. St. Andrew's Catholic Church also supported St. Mary's School, which was located to the east of the historic church building and the convent of the Sisters of Notre Dame.

**The Old Oakville Heritage Conservation District has historical/associative value through its potential to yield information that contributes to the understanding of the pre-contact Indigenous inhabitants.**

Sixteen Mile Creek and the surrounding area is part of the treaty land and territory of the Mississaugas of the Credit First Nation as well as the traditional territory of the Huron-Wendat and the Haudenosaunee peoples. The Mississaugas called the river Nanzuhzaugewazog meaning 'having two outlets', a reference to the shallow, gravelly mouth dividing the river in two and used the river for fishing, especially salmon. The British Crown recognized this area as Haudenosaunee land in the 1701 Fort Albany Nanfan Treaty and Six Nations' rights to these lands have never been ceded. The Mississaugas of the Credit ceded their lands on the Sixteen Mile Creek under Treaty 22 on February 8, 1820, to the British Crown as part ongoing European colonization and settlement of Indigenous territories. The confluence at the mouth of Sixteen Mile Creek and Lake Ontario continues to hold significance for the Mississaugas of the Credit First Nation and the Six Nations of the Grand River.

### **Contextual Value**

**The Old Oakville Heritage Conservation District has contextual value due to its physical, functional, visual, and historical links to its surroundings.**

The District consists of terrain that gently slopes towards Lake Ontario, becoming steeper towards the Sixteen Mile Creek. The District grew around the harbour at the mouth of Sixteen Mile Creek in the early 19<sup>th</sup> century. Residences line the original town street grid, with some properties retaining their original layout. Open space along the waterfront provides active and passive recreational amenities to serve the neighbourhood and visitors from within and outside of Oakville. The north-south streets terminate at Lake Ontario, providing views of the lake.

**The Old Oakville Heritage Conservation District has contextual value because it is important in defining, maintaining, or supporting the character of an area. Together, the properties in the District have a distinct character.**

The District has a village-like feel due to its historic street grid, low-density residences with lower lot coverages. Modest-sized historic homes line intimate streetscapes designed for pedestrians. The District's mature vegetation and tree canopy provides cover over the streetscape and public lands. The sloping topography, both towards the Lake and into a central gully, create variations along the street that helps to define the distinct character of the District.

**The Old Oakville Heritage Conservation District has contextual value because it is recognized as a landmark.**

Located along the shore of Lake Ontario, the District itself is a landmark with its paths and parks, views to the lake and the unique historic streetscapes. The District also has numerous individual properties that are landmarks to the community, including the Erchless Estate, Lakeside Park, St. Andrews Catholic Church and St. Jude's Anglican Church.

## 2.2 Description of Cultural Heritage Attributes

**The Old Oakville Heritage Conservation District has design/physical value as a representative example of an organically evolved historic harbourside residential community dating from the early 19<sup>th</sup> century and early 20<sup>th</sup> centuries. The District reflects a variety of architectural styles and a concentration of designed green spaces along the shoreline of Lake Ontario and steep banks along Sixteen Mile Creek. All these elements contribute to the District's varied, yet cohesive, streetscape. The District contains the following heritage attributes that reflect these values:**

- The lot patterns and historic street grid based on the 1833 survey and original quarter-acre lots
- Contributing properties of modest scale, massing, height and lower lot coverages
- Structures dating from the early 19<sup>th</sup> to early 20<sup>th</sup> century representing a variety of architectural styles in vernacular interpretations/materials that reflect the District's organic evolution
- Traditional construction, materials and architectural features of *heritage buildings*, including stucco and wood cladding; gable and hip roof forms; multipaned wood framed windows and wood shutters
- The orientation of main entrances towards the street
- Side yard driveways with rear or side yard garages and outbuildings
- Corner lots with open views/vistas
- Varied building setbacks
- Low-lying permeable fencing and soft landscaping along sidewalks and curbs
- Mature trees on both private and public lands highly visible from the public realm
- The contributing properties adjacent to St. Andrew's Catholic Church with generous building setbacks, larger lot sizes, similar architectural character, and the predominant use of stucco
- The contributing properties adjacent to St. Jude's Anglican Church with large front and side yard buildings setbacks, generous spacing between buildings and use of stucco cladding
- Front Street, Navy Street and Water Street rights-of-way
- Public open spaces including Lakeside Park, Market Square, Dingle Park and George Street Parkette
- The significant cultural heritage landscape of the Oakville Harbour, specifically the semi-natural river mouth of Sixteen Mile Creek
- The significant cultural heritage landscape of the Erchless Estate



- Five distinct streetscape and landscape Character Areas and one streetscape overlay that acknowledge distinct features associated with the organic evolution of the District from the early 19<sup>th</sup> century through to the early 20<sup>th</sup> century.

**The Old Oakville Heritage Conservation District has historical/associative value for its direct associations with the early 19<sup>th</sup> century commercial development of the village of Oakville, with 20<sup>th</sup> century recreation and town building, and with Oakville's founding family, the Chisholms, other settler families and early 20<sup>th</sup> century developer William Sinclair Davis. The District contains the following heritage attributes that reflect this value:**

- The lot patterns and historic street grid based on the 1833 survey and original quarter-acre lots
- Contributing properties that date from the early 19<sup>th</sup> to early 20<sup>th</sup> century directly associated with Oakville's founding family, other settler families and early 20<sup>th</sup> century developer William Sinclair Davis
- Contributing properties with buildings dating from the early 19<sup>th</sup> to early 20<sup>th</sup> century representing a variety of historic architectural styles and materials that are reflective of the District's organic evolution
- The contributing properties including architectural style, form and materials of buildings historically associated with St. Andrew's Catholic Church and St. Jude's Anglican Church
- The open garden and lawn space to the west of St. Jude's Church (between King Street and William Street)
- The Oakville Club
- The Oakville Lawn Bowling Club
- The public open spaces along Sixteen Mile Creek and Lake Ontario
- The Oakville Harbour Cultural Heritage Landscape heritage attributes within the District as outlined in Bylaw 2020-125
- The Erchless Estate Cultural Heritage Landscape heritage attributes within the District as outlined in Bylaw 2019-057

**The Old Oakville Heritage Conservation District has historical/associative value for its potential to yield information that contributes to the understanding of the pre-contact Indigenous inhabitants. The District contains the following heritage attributes that reflect this value:**

- The known and potential archaeological resources
- The waters and shorelines of Lake Ontario and the Sixteen Mile Creek
- The pedestrian trail leading from Dingle Park to Allan Street.

**The Old Oakville Heritage Conservation District has contextual value because it is important in defining, maintaining, or supporting the character of an area. The District contains the following heritage attributes that reflect this value:**

- The lot patterns and historic street grid based on the 1833 survey and original quarter-acre lots
- Contributing properties of modest scale, massing, height and lower lot coverages
- Structures dating from the early 19<sup>th</sup> to early 20<sup>th</sup> century representing a variety of architectural styles in vernacular interpretations/materials that reflect the District's organic evolution
- The openness on public and private lands at corner intersections

- The road configuration combining traditional town form and laneways
- Varied building setbacks
- Low-lying permeable fencing and soft landscaping along sidewalks and curbs
- The mature trees on both private and public lands highly visible from the public realm
- Five distinct streetscape and landscape Character Areas and one streetscape overlay acknowledge distinct features associated with the organic evolution of the District from the early 19<sup>th</sup> century through to the early 20<sup>th</sup> century.

**The Old Oakville Heritage Conservation District has contextual value due to its physical, functional, visual and historical links to its surroundings. The District contains the following heritage attributes that reflect this value:**

- The lot patterns and historic street grid based on the 1833 survey and original quarter-acre lots
- Contributing properties of modest scale, massing, height and lower lot coverages
- The widening at corner intersections and the road configuration combining traditional town form and laneways
- The steep banks along Sixteen Mile Creek and the gentle sloping topography within the District – north/south and east/west
- The built form of contributing properties that respond to the changing topography including heights of buildings that follow the topographical slopes, terraces, retaining walls and exposed foundations
- Views as shown in Map 2: Views and vistas:
  - Long views to Lake Ontario along and at the road terminus of Navy Street, Thomas Street, George Street, Dunn Street, Trafalgar Road, Reynolds Street and Allan Street, fronting onto the waterfront open space.
  - Navy Street's wide viewshed into Lake Ontario as it opens into Lakeside Park
  - Long views from the District to Downtown Oakville along and at the road terminus (e.g. Navy Street, Thomas Street, George Street, Dunn Street, Trafalgar Road, Reynolds Street and Allan Street)
  - Long viewsheds to St. Jude's and St. Andrew's churches
  - Continuous vistas to the lake, except in the gully streetscape, along Front Street
  - Continuous vistas to Lake Ontario, Sixteen Mile Creek and the harbour along the trail system within the waterfront open space system, including: Erchless Estate, Lakeside Park, George Street Parkette and Dingle Park
- Large building setbacks, porous fencing and low-height soft landscaping supporting openings in the streetscape throughout the District, especially at road intersections framing residential viewsheds
- Five distinct streetscape and landscape Character Areas and one streetscape overlay acknowledge distinct features, views and vistas associated with the organic evolution of the District from the early 19<sup>th</sup> century through to the early 20<sup>th</sup> century

**The Old Oakville Heritage Conservation District has contextual value because it is recognized as a landmark. The District contains the following heritage attributes that reflect this value:**

- The Erchless Estate Cultural Heritage Landscape heritage attributes as outlined in By-law 2019-057
- The Oakville Harbour Cultural Heritage Landscape heritage attributes within the District as outlined in Bylaw 2020-125
- St. Andrew's Catholic Church

- St. Jude's Anglican Church
- The Waterfront trail
- Public open spaces including Lakeside Park, Market Square, Dingle Park and George Street Parkette.





## 2.3 Property Categorizations: Contributing and Non-Contributing

All properties within the District are designated under Part V of the OHA.

Ontario Regulation (O. Reg) 9/06 as amended by O. Reg 569/22 sets out the criteria for designation of an HCD. Specifically, 3. (2).1 requires that 25 per cent or more of the properties within the boundaries must meet two or more of the criteria. The Old Oakville Heritage Conservation District meets this threshold. This work is based on the inventory created as part of the HCD Study (see Appendix A of the HCD Study), which documented and evaluated each building according to O. Reg 9/06.

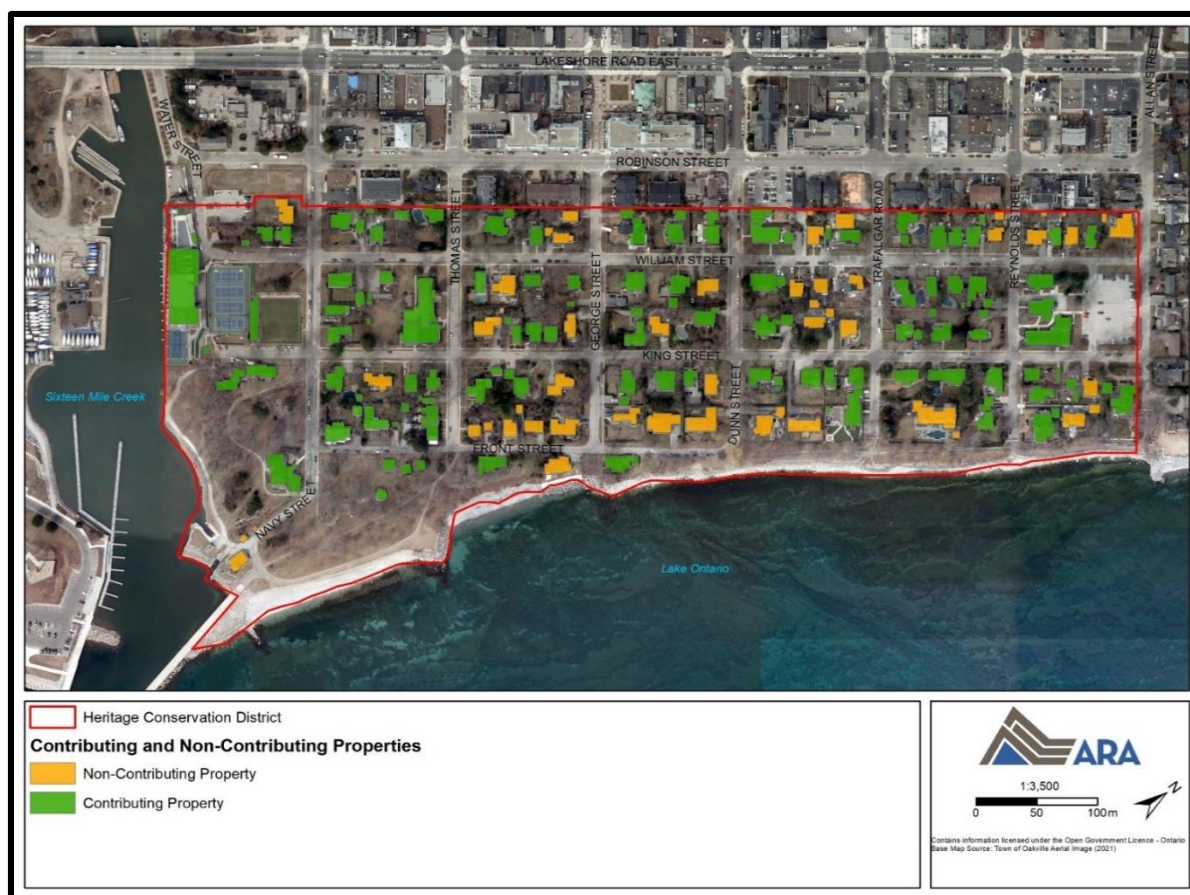
There are two categories of properties found in the District:

- a. **Contributing properties** exhibit physical heritage attributes which directly contribute to the cultural heritage value or interest of the District. Most contributing properties contain a heritage building. Ninety properties within the District (70 per cent) have been identified as contributing properties. Removing or significantly altering these properties would negatively impact the heritage character of the District. Contributing properties are subject to specific policies which are outlined in Section 5.3.
- b. **Non-contributing properties** do not meet two or more of the criteria outlined in O. Reg 9/06 and therefore do not exhibit significant cultural heritage value. Most non-contributing properties contain a non-heritage building. While they did not meet the test of two criteria under O. Reg 9/06, many non-contributing properties in the District are considered “good neighbours” in terms of their site placement, design, scale and massing. Their proximity to, and evolution alongside of contributing properties give them the potential to impact the heritage character of neighbouring properties, their Character Area and the District. The properties that are considered “good neighbours” are identified in Appendix E. Non-contributing properties are subject to specific policies which are outlined in Section 5.4.

Table 1 lists all properties within the District and an indication of the status of each property (contributing or non-contributing). The property status is also mapped in



Good neighbour properties are shown in Map 4.



Map 3: Contributing and Non-Contributing Properties



Map 4: "Good Neighbour" Properties

Table 1: Property Status (Contributing or Non-Contributing) in the District

Property Address	Status	Property Address	Status
21 Dunn St	Non-Contributing	155 King St	Contributing
30 Dunn St	Non-Contributing	177 King St	Non-Contributing
43 Dunn St	Contributing	181 King St	Contributing
53 Dunn St	Contributing	184 King St	Contributing
65 Dunn St	Contributing	187 King St	Contributing
66 Dunn St	Contributing	191 King St	Contributing
69 Dunn St	Contributing	208 King St	Non-Contributing
143 Front St	Contributing	212 King St	Contributing
144 Front St	Contributing	213 King St	Contributing
176 Front St	Contributing	219 King St	Non-Contributing
181 Front St	Non-Contributing	222 King St	Contributing
187 Front St	Non-Contributing	230 King St	Contributing
194 Front St	Non-Contributing	233 King St	Contributing
204 Front St	Contributing	250 King St	Contributing
212 Front St	Contributing	260 King St	Non-Contributing
221 Front St	Non-Contributing	262 King St	Contributing
235 Front St	Non-Contributing	263 King St	Non-Contributing
240 Front St	Contributing	268 King St	Contributing
22 George St	Non-Contributing	274 King St	Contributing
23 George St	Non-Contributing	275 King St	Non-Contributing
44 George St	Non-Contributing	288 King St	Contributing
68 George St	Non-Contributing	290 King St	Contributing
110 King St	Contributing	295 King St	Contributing
114 King St	Contributing	302 King St	Contributing
146 King St	Non-Contributing	309 King St	Contributing
154 King St	Contributing	312 King St	Contributing



Property Address	Status	Property Address	Status
340 King St	Non-Contributing	24 Thomas St	Contributing
350 King St	Contributing	26 Thomas St	Contributing
19 Navy St	Contributing	29 Thomas St	Contributing
2 Navy St	Contributing	32 Thomas St	Contributing
8 Navy Street	Contributing	50 Thomas St	Contributing
29 Navy St	Contributing	53 Thomas St	Contributing
41 Navy St	Contributing	65 Thomas St	Contributing
44 Navy St	Contributing	68 Thomas St	Contributing
45 Navy St	Contributing	23 Trafalgar Rd	Non-Contributing
53 Navy St	Contributing	25 Trafalgar Rd	Non-Contributing
54 Navy St	Contributing	26 Trafalgar Rd	Contributing
64 Navy St	Contributing	43 Trafalgar Rd	Contributing
65 Navy St	Contributing	65 Trafalgar Rd	Contributing
68 Navy St	Non-Contributing	68 Trafalgar Rd	Non-Contributing
70 Navy St	Non-Contributing	56 Water St	Contributing
21 Reynolds St	Contributing	115 William St	Contributing
22 Reynolds St	Contributing	145 William St	Contributing
23 Reynolds St	Non-Contributing	148 William St	Contributing
27 Reynolds St	Contributing	160 William St	Contributing
31 Reynolds St	Contributing	180 William St	Non-Contributing
41 Reynolds St	Contributing	185 William St	Contributing
47 Reynolds St	Contributing	186 William St	Contributing
53 Reynolds St	Contributing	187 William St	Contributing
18 Thomas St	Contributing	188 William St	Contributing
20 Thomas St	Contributing	195 William St	Non-Contributing
21 Thomas St	Non-Contributing	200 William St	Contributing

Property Address	Status	Property Address	Status
214 William St	Contributing	296 William St	Contributing
215 William St	Contributing	297 William St	Contributing
225 William St	Contributing	301 William St	Contributing
226 William St	Contributing	302 William St	Contributing
234 William St	Non-Contributing	307 William St	Contributing
258 William St	Non-Contributing	308 William St	Contributing
263 William St	Non-Contributing	313 William St	Non-Contributing
266 William St	Non-Contributing	323 William St	Non-Contributing
273 William St	Contributing	329 William St	Contributing
274 William St	Non-Contributing	333 William St	Non-Contributing
288 William St	Contributing	339 William St	Contributing
295 William St	Contributing	349 William St	Non-Contributing

## 2.4 Streetscape and Landscape Character Areas

The Old Oakville HCD is unique because of the combination of elements that create its streetscape character and is a significant cultural heritage landscape. The undulating topography, Lake Ontario and Sixteen Mile Creek shorelines, views in and out of the District, mature tree growth and vegetation, and two centuries of European settlement have created a special character in this area.

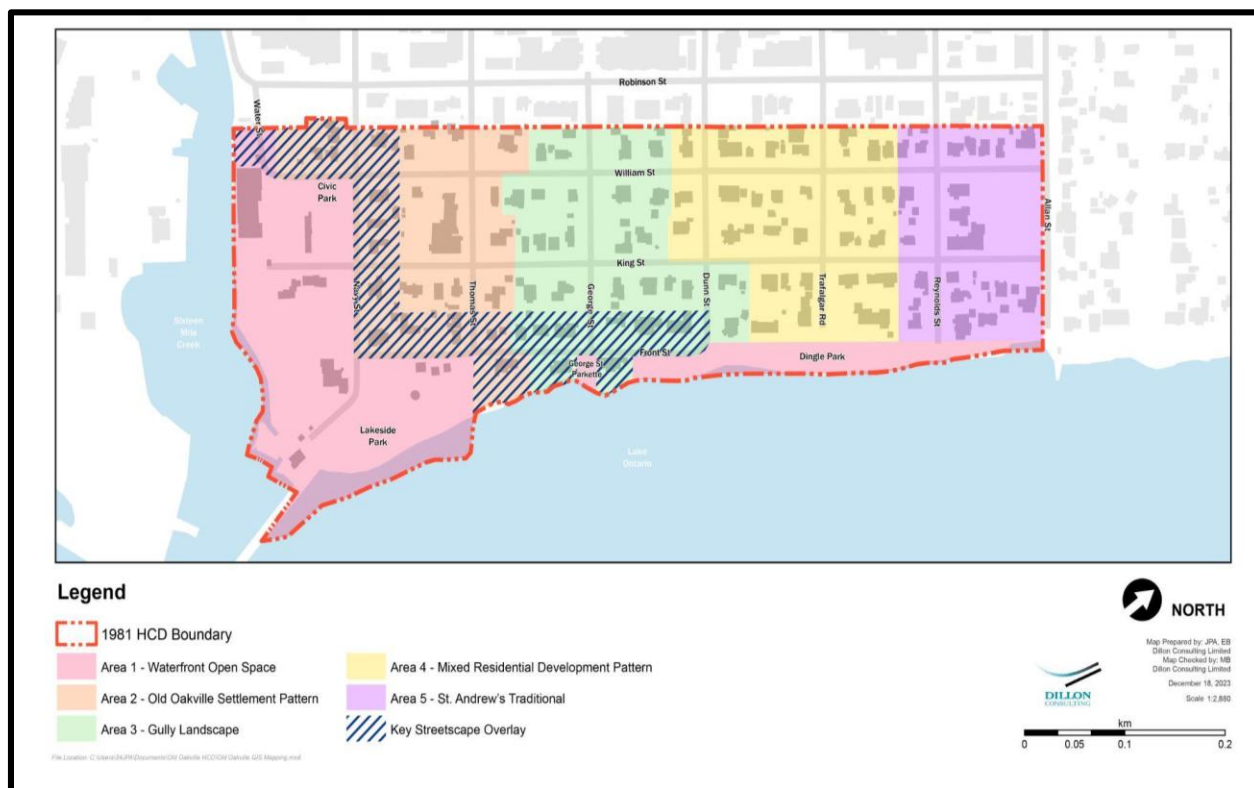
The District's low-density residences, large parcel sizes, mixed building setbacks, widenings at road intersections, street lining/framing tree cover, wide viewsheds and permeable fencing types contributes to its open landscape setting and enjoyable pedestrian experience. The road grid configuration and block size create a very desirable and walkable neighbourhood south of the commercial downtown.

Building on the original 1982 Old Oakville HCD Plan that viewed the area through a 'block' framework, as well as the updated cultural heritage values of the District, the HCD Study introduced a Character Area framework to acknowledge distinct features through the historic, streetscape and landscape analysis of the District.

Five distinct streetscape and landscape Character Areas were established in the HCD Study and in this Plan have area-specific site design guidance that works together with the general guidelines to manage change in the District. The five Character Areas and one streetscape overlay are categorized as:

- **Area 1 - Waterfront Open Space**
- **Area 2 - Old Oakville Settlement Area**
- **Area 3 - Gully Landscape**
- **Area 4 - Mixed Residential Development Pattern**
- **Area 5 – St. Andrew’s Traditional**
- **Key Streetscape Overlay**

Additional streetscape and landscape Character Area guidance specific to each category are outlined in Section 5.2.



Map 5: Streetscape and Landscape Characterization in Old Oakville





# 3 OBJECTIVES OF THE HERITAGE CONSERVATION DISTRICT

*Oakville Aerial Photo - 1954*

### 3.1 Introduction

The overall objective of this Plan is the conservation and management of the District's heritage attributes so that its cultural heritage value is protected. This requires understanding the District as a significant cultural heritage landscape, including the built form, spatial relationships and natural elements of the District streetscapes. The District Study provides the information and analysis of the history and context of the District area and should be referred to for additional information when required.

The cultural heritage value of the District as set out in Section 2 of this Plan consists of its design and physical value, its historical and associative value and its contextual value. The heritage attributes of the District include its built form, evolved lot pattern and historic street grid, streetscape, mature trees, vistas and views and public realm.

Specific objectives for the Plan are intended to provide guidance to property owners, tenants, Council, the Heritage Oakville Advisory Committee, town staff and others who have interests in the District when contemplating any changes to the District. They are consistent with the objectives that have been set by the Town of Oakville for the other HCDs in Oakville, as well as heritage best practices. All future changes in the District must be consistent with the objectives of the Plan.

### 3.2 Specific Objectives for the Plan

- a. To conserve, maintain and enhance the cultural heritage value of the District as expressed through its heritage attributes, contributing properties, streetscape and landscape Character Areas, public realm and land use;
- b. To conserve and enhance views from the public realm identified in this Plan that contribute to the District as a whole and the Character Areas as subsets;
- c. To conserve, maintain and enhance the distinctive assemblage of contributing properties and streetscapes that are an organically evolved CHL. This CHL is a historic harbourside village residential community with historic structures dating from the early-19th century into the mid-20th century;
- d. To conserve and maintain the historic scale, massing and low-density residential character of the built forms, patterns and visual appearance of the Character Areas and the District as a whole;
- e. To conserve and maintain the physical form, scale and architectural features of the range of architectural styles of contributing properties identified in the District;
- f. To conserve, maintain and enhance contributing properties within the District;
- g. To encourage the ongoing maintenance, retention, and adaptation of contributing properties, rather than demolition and replacement. Heritage permits for demolition of heritage buildings on contributing properties are considered rare and a last resort and should not be approved by Council unless no other viable option is available;

- h. To provide residents and property owners with the necessary information regarding appropriate methods of maintenance and conservation through the Plan so that building and repair activities can be undertaken;
- i. To facilitate the restoration of heritage buildings on contributing properties based on a thorough examination of archival and existing evidence and restoration best practices;
- j. To ensure that alterations to contributing properties are compatible to and sympathetic with the heritage attributes of the property, Character Area and District and consistent with the Plan;
- k. To encourage improvements to non-contributing properties that are compatible with the Character Area and the District cultural heritage values and attributes;
- l. To ensure that new development and additions conserve and enhance the cultural heritage value of its Character Area and the District. New development and alteration shall respect the scale, massing and the streetscape character, including contributing built forms, as well as identified views and vistas;
- m. To encourage thoughtful and sympathetic architectural style and form in the design of new development, additions and alterations that are compatible with adjacent contributing properties, the Character Area and the District's cultural heritage values and attributes;
- n. To maintain and enhance the tree canopy cover and park-like settings associated with green spaces visible from the public realm on private properties, public streetscapes and along the shoreline of Lake Ontario and the banks of Sixteen Mile Creek;
- o. To maintain and protect a consistent pedestrian experience of the District through connectivity including sidewalks, trails, street furniture, and lighting;
- p. To conserve, maintain and enhance the five streetscape and landscape Character Areas and one streetscape overlay as set out in Section 5.2 of this Plan;
- q. To conserve the open spaces including Lakeside Park, Market Square, Dingle Park, George Street Parkette and the significant CHLs and their relationship to adjacent contributing properties;
- r. To conserve, maintain and protect the identified views and vistas in Map 2 and the openness of private and public spaces at corner intersections;
- s. To conserve, maintain and protect the pedestrian scale of existing roads and streetscapes;
- t. To encourage and support environmentally friendly materials and 'green' initiatives where they do not negatively impact the cultural heritage value and heritage attributes of the District;
- u. To ensure public works maintain and conserve the cultural heritage values and heritage attributes of the District; and

- v. To ensure that development and alterations adjacent to the District conserve the District's cultural heritage value.



# 4 MUNICIPAL POLICY FRAMEWORK



## 4.1 Introduction

An HCD protects and conserves the cultural heritage value and heritage attributes by managing physical changes to existing properties and ensuring that new development complements the existing heritage resources. The management of changes to properties within an HCD falls under the purview of the OHA. Some changes are also governed by a number of provisions under the *Planning Act*, such as official plans, zoning by-laws, and site plan control.

The successful maintenance and protection of an HCD relies on local planning policies, by-laws and initiatives that complement and support the conservation measures of this Plan. The following section outlines the provincial and municipal legislation/policies that help to manage development within the District.

## 4.2 Provincial Legislation

The Province of Ontario has made a clear commitment to the conservation of significant cultural heritage resources through its legislation and policies, including the *OHA*, the *Planning Act* and the *Provincial Planning Statement, 2024 (PPS)*.

Section 4.6.1 of the PPS relating to Cultural Heritage and Archaeology states:

*Protected heritage property, which may contain built heritage resources or cultural heritage landscapes, shall be conserved.*

The PPS defines “significant”, in regard to cultural heritage and archaeology, as:

*... in regard to cultural heritage and archaeology, resources that have been determined to have cultural heritage value or interest. Processes and criteria for determining cultural heritage value or interest are established by the Province under the authority of the Ontario Heritage Act.*

Further, the PPS defines “conserved” as:

*... the identification, protection, management and use of built heritage resources, cultural heritage landscapes and archaeological resources in a manner that ensures their cultural heritage value or interest is retained. This may be achieved by the implementation of recommendations set out in a conservation plan, archaeological assessment, and/or heritage impact assessment that has been approved, accepted or adopted by the relevant planning authority and/or decision-maker. Mitigative measures and/or alternative development approaches should be included in these plans and assessments.*

The PPS also states:

*4.6.4. Planning authorities are encouraged to develop and implement:*  
*a) archaeological management plans for conserving archaeological resources; and*  
*b) proactive strategies for conserving significant built heritage resources and cultural heritage landscapes.*



The equivalence given to conserving built heritage resources and cultural heritage landscapes in the PPS is important, as it reinforces the broad scope of the OHA beyond its initial focus on built heritage. Heritage conservation districts are significant to communities not just for their built structures, but also their landscape and streetscape character.

The PPS functions together with the OHA by the shared principle that cultural heritage resources shall be conserved.

The OHA sets out the procedures for evaluating and protecting heritage resources at the provincial and municipal levels. This includes the use of Ontario Regulation 9/06 as the means for determining if a property has cultural heritage value. Ontario Regulation 9/06 as amended by Ontario Regulation 569/22 sets out the criteria for designation of an HCD. Specifically, 3. (2).1 notes that 25 per cent or more of the properties within the boundaries must meet two or more of those criteria.

While not applicable for the management of all changes within an HCD, certain provisions under the *Planning Act* can be used to negotiate protection or conservation measures under plans of subdivision or condominium, zoning by-law amendments, site plan approvals, and consents. Condition of approval relating to heritage conservation may be used in land division and the creation of new lots.

Zoning by-law amendment provisions, or variations thereof, may stipulate retention of properties or uses within specified heritage buildings as of the date of passing of the by-law and retention of buildings, structures and features may also be specified in site plans.

Authority may also be derived from Section 2(d) of the *Planning Act* (identifying a provincial interest in heritage conservation) and related provincial planning statements.

## 4.3 The Livable Oakville Plan

An Official Plan is a document required by the *Planning Act* that sets out municipal council or planning board's policies on how land in your community should be used.

Section 16 of the *Planning Act* requires that Official Plans contain:

*goals, objectives and policies established primarily to manage and direct physical change and the effects on the social, economic, built and natural environment of the municipality or part of it, or an area that is without municipal organization.*

The Town of Oakville's Official Plan, known as the Livable Oakville Plan, came into force in May 2011. Section 5 of the Livable Oakville Plan sets out the policy direction for cultural heritage resources and states that:

*Conservation of cultural heritage resources forms an integral part of the Town's planning and decision making. Oakville's cultural heritage resources shall be conserved so that they may be experienced and appreciated by existing and future generations, and enhance the Town's sense of history, sense of community, identity, sustainability, economic health, and quality of life.*

Section 5.2 of the Livable Oakville Plan provides the town authority to establish heritage conservation districts and adopt heritage conservation district plans for each district.

## Properties Adjacent to the Old Oakville HCD

As per the PPS Section 4.6.3:

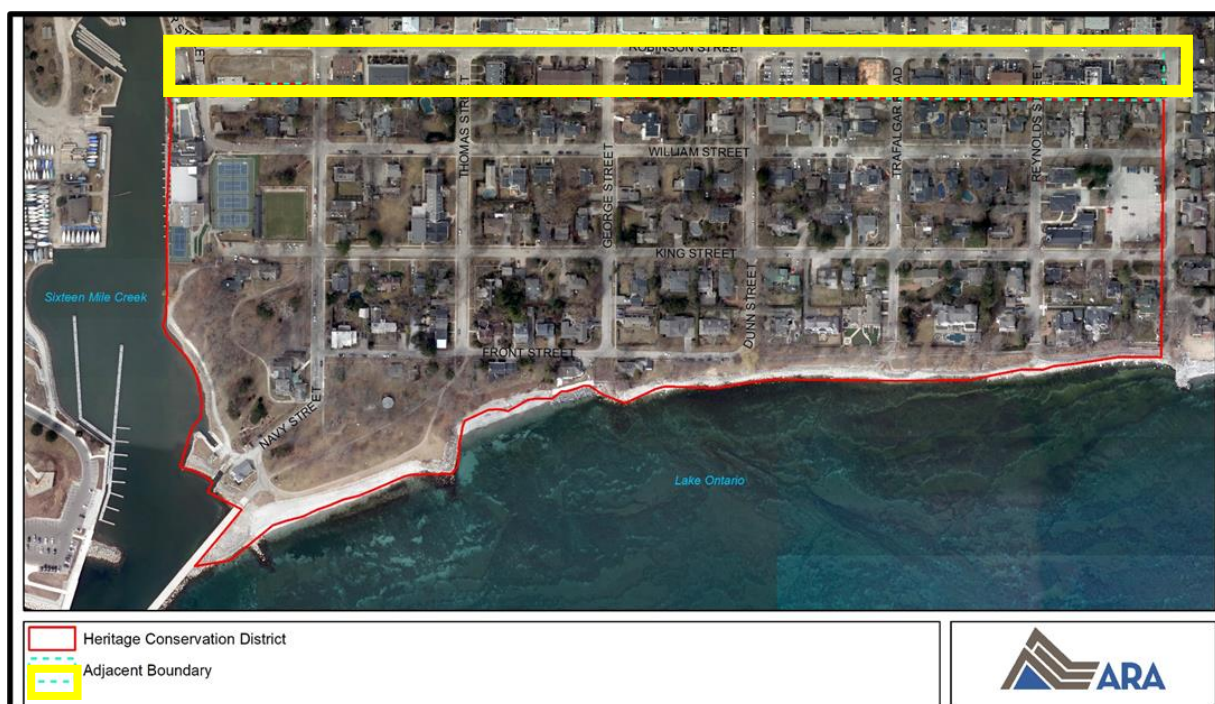
*Planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property unless the heritage attributes of the protected heritage property will be conserved.*

Any proposals for new construction adjacent to, or in the immediate vicinity of the District, is required to consider the heritage attributes set out in Section 2.2 of this Plan and Guidelines to ensure they are conserved.

As per the Livable Oakville Plan, Heritage Impact Assessments (HIAs) may be required when any development is proposed adjacent to an HCD. The Terms of Reference for HIAs has been set out by the Planning and Development Department. HIAs for properties adjacent to the District shall demonstrate how the cultural heritage value of the District is being conserved.

For clarification on the definition and delineation of 'adjacent' properties for the Old Oakville HCD, refer to

Map 6 that shows properties that are adjacent to the north of the District along Robinson Street. Adjacent properties to the east of the District are guided by First and Second Street HCD Plan and to the west, the Oakville Harbour CHL Conservation Plan and are subject to the requirements of their respective plans.

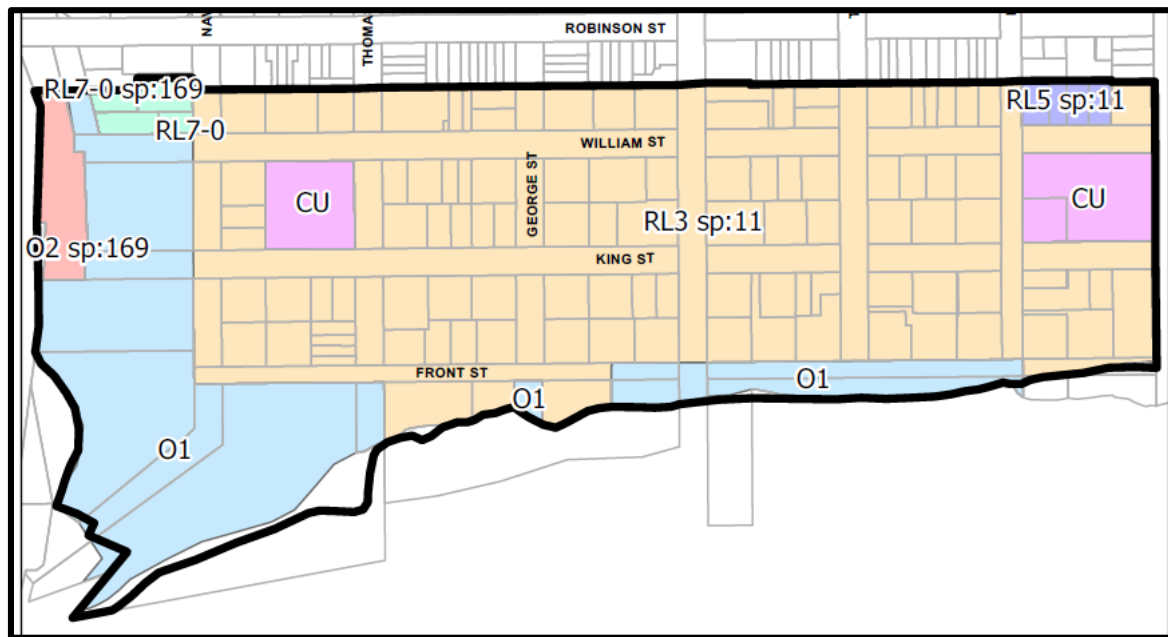


Map 6: Map of Adjacent Properties to the north of the Old Oakville HCD



## 4.4 Town of Oakville Zoning By-Law 2014-014

The Zoning By-Law is a set of regulations which lists permitted use and building design requirements in each of the different zones. At the time of approval of this Plan, there are several zoning provisions applicable within the District boundary under By-law 2014-014. The most prominent zoning designation is Residential Low 3 (RL3) with a Special Provision to ensure that building heights are consistent with the character of the area, as well as lot coverage. The small section of Residential Low 5 (RL5) along the north side of William Street between Reynolds Street and Allan Street is subject to Special Provision 11. There are four properties zoned as Residential Low 7 with the suffix -0 at the intersection of Navy Street and William Street.



Map 7: Old Oakville zoning from Town of Oakville Zoning By-law 2014-014

Two church properties are zoned as 'Community Use.' Under this designation, various uses associated with places of worship as indicated are permitted. The lands designated as Open Space 1 (OS1) includes town owned parks and Open Space (OS2) includes private open space pertaining to uses such as golf courses, accessory retail space, and sports facilities. The Oakville Club has a special provision within this designation under Special Provision 169.

### 4.4.1 Update to Zoning By-law 2014-014

The town is currently in the process of updating its residential zoning by-law, providing an opportunity to improve existing zoning provisions that pose challenges for new development's height and massing to be in accordance with the intent of this Plan. Changes made to the zoning within the District should maintain the overall cultural heritage value of this area and its heritage attributes.

## 4.5 Site Plan Control

The town's Site Plan By-law establishes the classes of development requiring site plan approval, exemptions and the delegation of powers under the *Planning Act*.

The *More Homes Built Faster Act, 2022*, Bill 23, came into force on November 28, 2022, introducing significant changes to Ontario's site plan control process to accelerate affordable housing development. This Act removes exterior design matters (except some related to sustainability) from site plan approvals and grants all property owners the as-of-right ability to construct up to three residential units per lot. Under this evolving legislation, far fewer properties in the District will require site plan approval. However, development proposals would still require heritage permit approval and, in some cases, an HIA.

Town staff and the Heritage Oakville Advisory Committee will continue to review and provide feedback on planning and development proposals for heritage properties. This includes assessing alterations, removals, or demolitions using the planning tools available under current legislation.

## 4.6 Demolition Control

The Livable Oakville Plan states that in any instance where an application includes proposals to demolish a designated heritage resource, it must be considered in accordance with the provisions of the OHA and the Livable Oakville Plan.

Section 42 of the OHA allows municipalities to prevent the demolition or removal of buildings within HCDs prior to obtaining a permit. Further, the OHA holds that a municipal heritage committee must be consulted on all applications for demolition permits in the District. The demolition process is described later in this Plan.

### Residential Rental Property Demolition

The town enacted By-law 2023-101 in July of 2023 to regulate the demolition and conversion of residential rental property in the town. The entire geographic area of the town is designated as the demolition control area and no person shall demolish a structure unless they are issued a demolition control permit in accordance with this By-law. The primary intent of this by-law is to conserve buildings with six or more dwelling units to prevent the premature loss of building stock.

## 4.7 Property Standards By-law

The Property Standards By-law provides direction related to property maintenance. This by-law addresses matters such as structural adequacy, foundations, walls, columns, beams, floors, roofs, balconies, stairs, heating and ventilation, and mechanical aspects. Standards are also included for yards, lighting, fences, and vacant properties.

There is a section in the Property Standards By-law specific to heritage properties designated under Part IV and Part V of the OHA. The focus of these standards is to protect and maintain the heritage attributes of contributing properties and ensure that a permit is obtained prior to work being undertaken. There are also sections regarding the repair of properties, replacement of heritage attributes, clearing properties, and vacant properties.

## 4.8 Private Tree Protection By-law

The town has a Private Tree Protection By-law that provides protection to trees located on privately owned lands. A tree removal permit is required for:

- any trees that measure 15 centimetres or more in diameter at breast height (DBH),
- dead and high-risk trees (noting that ash trees and buckthorn are exempt from fees),
- any tree that is 15 centimetres or more in diameter being removed as part of a development application; and
- any hedge with stems that measure 15 centimetres or more in diameter.

A heritage permit may also be required for tree removal, as discussed later in this Plan.

## 4.9 Cultural Heritage Landscape Strategy

In January 2014, the town endorsed the Cultural Heritage Landscapes Strategy (the Strategy), which set the foundation to identify, inventory, and assess candidate cultural heritage landscapes (CHLs) and to conserve significant CHLs. The Old Oakville HCD is an Organically

Evolved Landscape, which is a landscape that “results from an initial social, economic, administrative, and/or religious imperative and has developed in its present form in response to its natural environment”. It is considered a ‘Continuing Landscape’ within the Organically Evolved category which “retains an active social role in contemporary society closely associated with the traditional way of life, and which the evolutionary process is still in progress.”

The Strategy identifies processes to protect CHLs. These processes include but are not limited to:

- Guide the proposed development to ensure compatibility with and respect of CHLs; seek opportunities to incorporate CHLs in whole or part into the design where possible;
- Conserve and protect by using the most appropriate planning tool (heritage designation under the OHA, conservation under subsection 2(d) of the *Planning Act*); and,
- Conduct consultation for both proactive and reactive assessments of CHLs.

The overlap of the CHLs within the Old Oakville HCD is shown in Map 6.

## 4.10 Oakville Harbour Cultural Heritage Landscape

The Oakville Harbour Cultural Heritage Landscape, designated under By-law 2020-125, has a Conservation Plan specific to its values and attributes that shall be used to manage change in the CHL area, in addition to the guidelines provided in this Plan. The boundary of the CHL is shown in Figure 3.



Figure 2: Oakville Harbour CHL boundary

## 4.11 Erchless Estate Cultural Heritage Landscape Strategy

The Erchless Estate Cultural Heritage Landscape, designated under By-law 2019-05, specific to its values and attributes that shall be used to manage change in the CHL area, in addition to the guidelines provided in this Plan. The boundary of the CHL is shown in Figure 3.

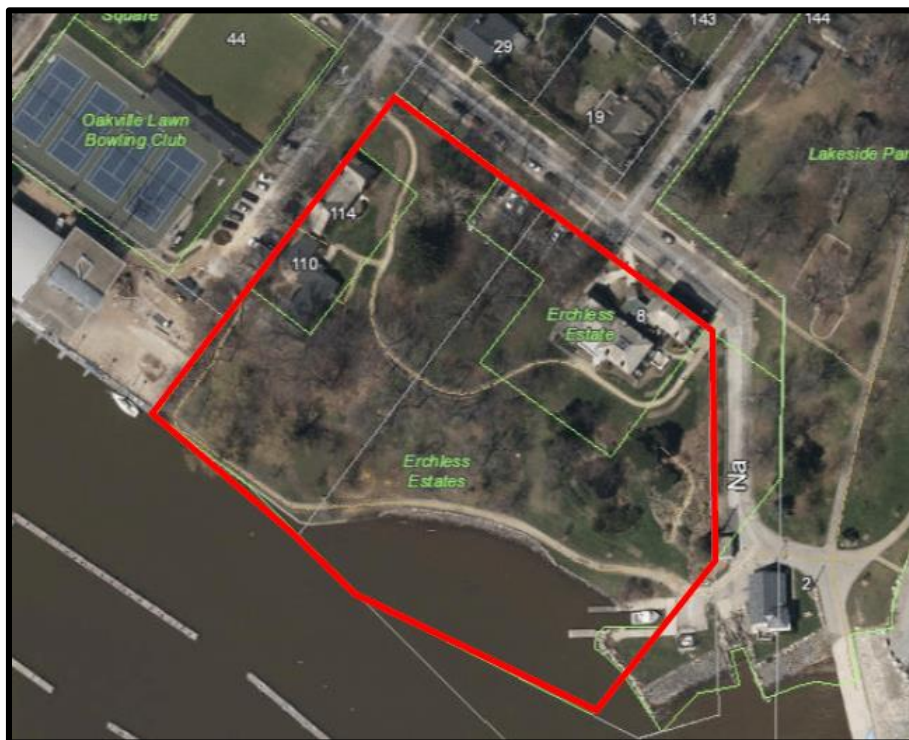
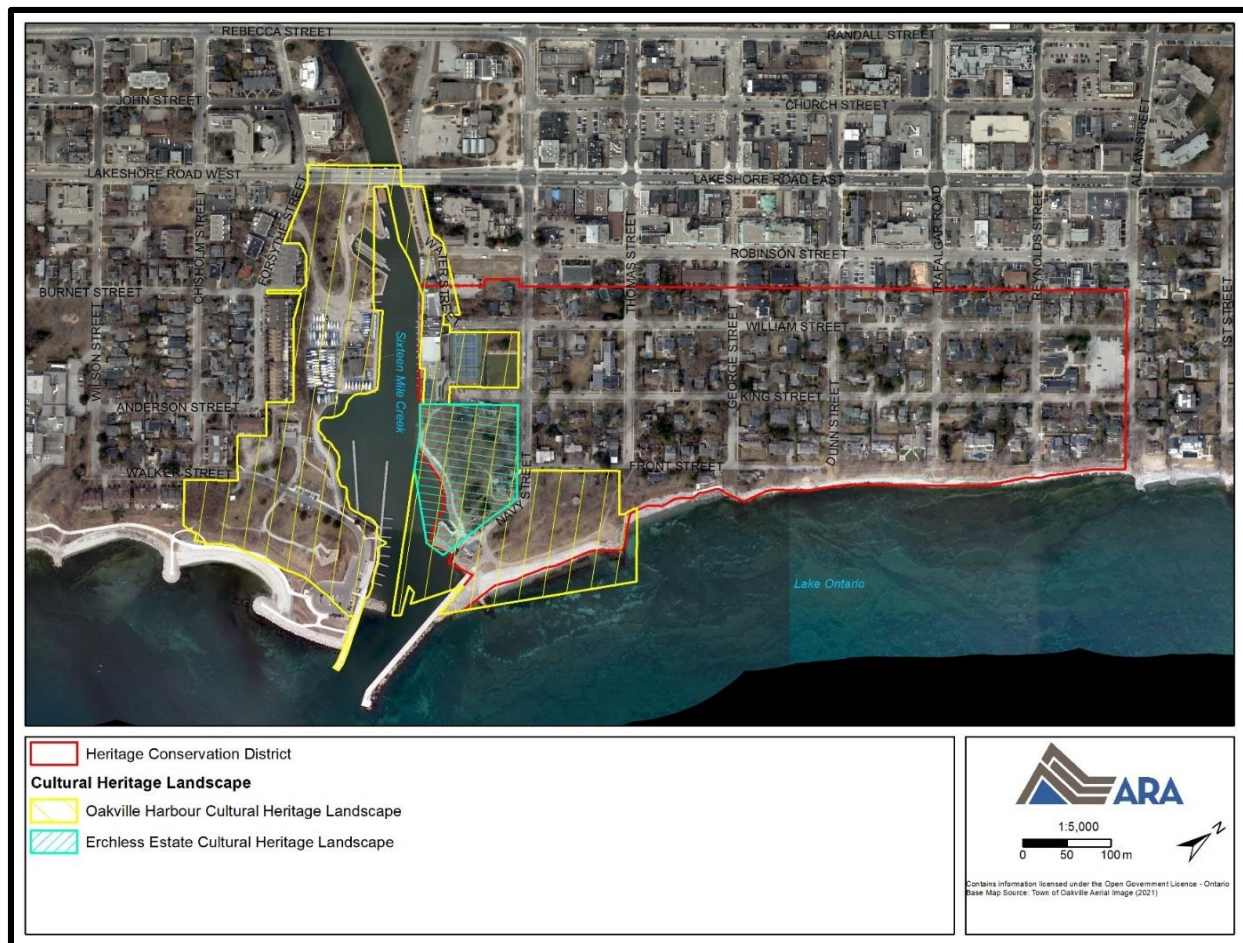


Figure 3: Erchless Estate CHL boundary





Map 8: HCD and CHL Boundaries





# 5 | GUIDELINES FOR MANAGING CHANGE

## 5.1 Introduction

This Plan provides direction to manage change at three levels: the District as a whole; the Character Area guidelines and the guidelines for contributing and non-contributing properties. The objectives for the District set out in section 3 are high level guidance when considering alterations to properties. Next to be reviewed is the Character Area guidelines that set the context for the area immediately surrounding the property. Finally, the guidelines for contributing and non-contributing properties provides the finest level of guidance for individual properties.

Design guidelines are required to manage physical changes to ensure that property owners use appropriate building forms, scale and massing, materials, construction methods, and site layouts that preserve the character the District. 'Best Practice' guidelines are noted in the 'Alterations to Contributing Properties' section and are intended to be the benchmark standard for all minor and major alterations within the District.

These guidelines must be considered by town staff, the Heritage Oakville Advisory Committee, Council, and property owners and their agents, when considering or reviewing applications for changes to properties within the District.

The design guidelines are divided into the following sections:

- **Character Areas**
  - **Area 1 - Waterfront Open Space**
  - **Area 2 - Old Oakville Settlement Area**
  - **Area 3 - Gully Landscape**
  - **Area 4 - Mixed Residential Development Pattern**
  - **Area 5 - St. Andrew's Traditional**
  - **Key Streetscape Overlay**
- **Alterations to Contributing Properties**
- **Additions to Contributing Properties**
- **Alterations and Additions to Non-Contributing Properties**
- **New Development – Primary Structures**
- **New Development – Detached Additional Residential Units**
- **Landscape and Site Guidelines for Publicly Owned Lands (Contributing and Non-Contributing)**
- **Landscape and Site Guidelines for Privately Owned Lands (Contributing and Non-Contributing)**

Refer to the flowchart on the next page for guidance on how to prepare for alterations to your property.





Figure 4: How to use the Plan

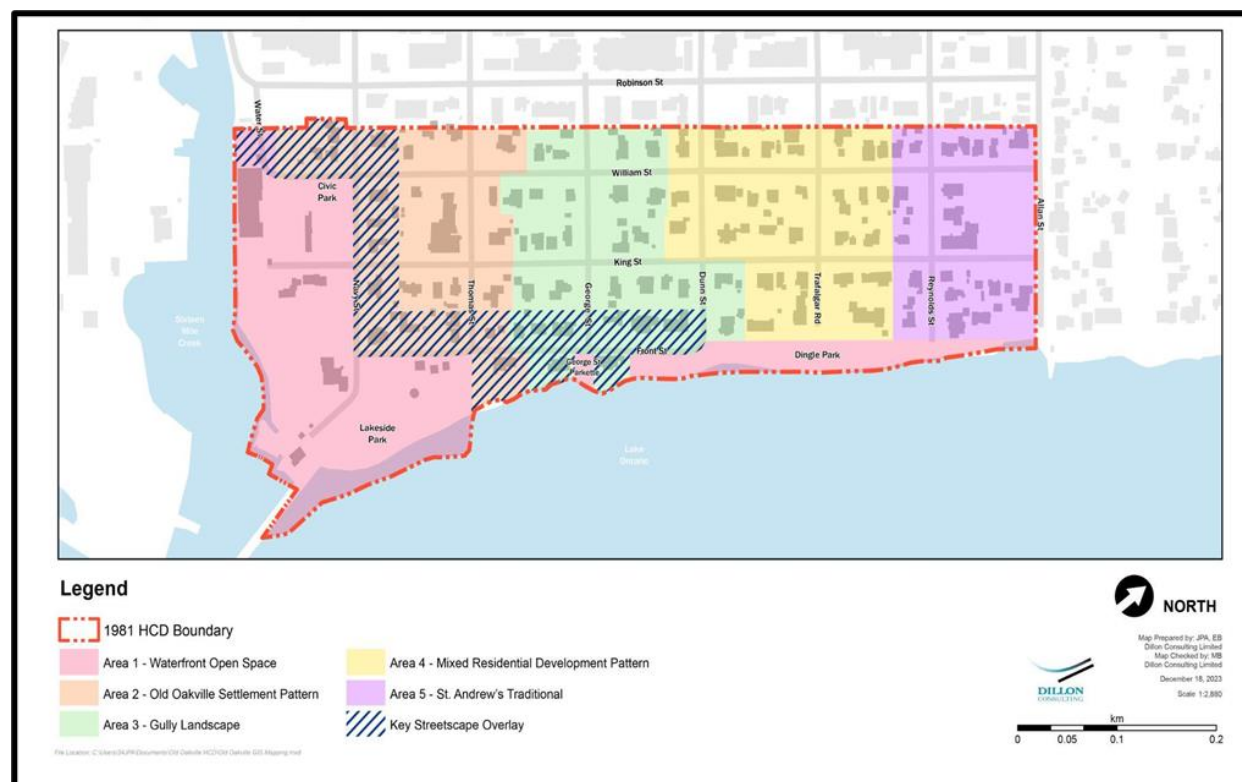
## 5.2 Character Area Guidelines

The Old Oakville Heritage Conservation District, when first designated under the OHA in 1982, used a Plan that relied on an analysis of the area through a 'Block structure' framework. Guidance was provided through each defined 'Block' to ensure that changes considered both individual buildings and the impact on the streetscape.

The updated Old Oakville Heritage Conservation District Study, completed in 2024, introduced a 'Character Area' framework that acknowledges heritage attributes through the historic, streetscape and landscape analysis of the District's evolution. Five distinct streetscape and landscape Character Areas have been identified as contributing to the overall cultural heritage values of the District.

- **Area 1 - Waterfront Open Space**
- **Area 2 - Old Oakville Settlement Area**
- **Area 3 - Gully Landscape**
- **Area 4 - Mixed Residential Development Pattern**
- **Area 5 - St. Andrew's Traditional**
- **Key Streetscape Overlay**

Table 2 includes the property addresses for each Character Area. This section provides the streetscape and landscape character guidelines for each Character Area that must be considered by property owners, town staff, the Heritage Oakville Advisory Committee and Council when reviewing applications for change.



Map 9: Old Oakville Character Areas

Table 2: Property Address List by Character Area

Area 1 - Waterfront Open Space	Area 2 - Old Oakville Settlement Area	Area 3 - Gully Landscape	Area 4 - Mixed Residential Development Pattern	Area 5 - Street Andrew's Traditional	Key Streetscape Overlay
144 Front St 204 Front St 240 Front St 110 King St 114 King St 2 Navy St 8 Navy St 44 Navy St 54 Navy St 56 Water St	143 Front St* 176 Front St* 146 King St 154 King St 155 King St 177 King St 19 Navy St* 29 Navy St* 41 Navy St* 45 Navy St* 53 Navy St* 64 Navy St* 65 Navy St* 68 Navy St* 70 Navy St* 18 Thomas St* 20 Thomas St* 21 Thomas St* 24 Thomas St* 26 Thomas St* 29 Thomas St 32 Thomas St 50 Thomas St 53 Thomas St 65 Thomas St 68 Thomas St 115 William St* 145 William St 148 William St 160 William St	21 Dunn St 30 Dunn St 181 Front St* 187 Front St* 194 Front St* 212 Front St* 235 Front St* 221 Front St* 22 George St* 23 George St* 44 George St 68 George St 181 King St 184 King St 187 King St 191 King St 208 King St 212 King St 213 King St 219 King St 222 King St 230 King St 250 King St 180 William St 185 William St 186 William St 187 William St 188 William St 195 William St 200 William St 214 William St 215 William St 225 William St 226 William St	43 Dunn St 53 Dunn St 65 Dunn St 66 Dunn St 69 Dunn St 233 King St 260 King St 262 King St 263 King St 268 King St 274 King St 275 King St 288 King St 290 King St 295 King St 302 King St 23 Trafalgar Rd 25 Trafalgar Rd 26 Trafalgar Rd 43 Trafalgar Rd 65 Trafalgar Rd 68 Trafalgar Rd 234 William St 258 William St 263 William St 266 William St 273 William St 274 William St 288 William St 295 William St 296 William St 297 William St 301 William St 302 William St	309 King St 312 King St 340 King St 350 King St 21 Reynolds St 22 Reynolds St 23 Reynolds St 27 Reynolds St 31 Reynolds St 41 Reynolds St 47 Reynolds St 53 Reynolds St 307 William St 308 William St 313 William St 323 William St 329 William St 333 William St 339 William St 349 William St	143 Front St 176 Front St 181 Front St 187 Front St 194 Front St 212 Front St 221 Front St 235 Front St 22 George St 23 George St 19 Navy St 29 Navy St 41 Navy St 45 Navy St 53 Navy St 64 Navy St 65 Navy St 68 Navy St 70 Navy St 18 Thomas St 20 Thomas St 21 Thomas St 24 Thomas St 26 Thomas St 115 William St

\*Property also falls under the Key Streetscape Overlay area.

### **5.2.1 Area 1 - Waterfront Open Space**

The Waterfront Open Space Character Area reflects the District's open public land uses that evolved over the treaty lands and territory of the Mississaugas of the Credit First Nation and the traditional territories of the Huron Wendat and the Haudenosaunee into a settler founded 19th century commercial development and then 20th century recreation at the mouth of Sixteen Mile Creek.

The topography of this Character Area is varied, incorporating the steep valley slope and Sixteen Mile Creek valley, shoreline of Lake Ontario and the bottom of the gully at George Street Parkette that runs north to south, as well as Lakeside Park, Market Square and Dingle Park.

Two significant individual CHLs are also located here: the Oakville Harbour and the Erchless Estate. They contribute to the Waterfront Open Space Character Area's design and physical value, as well its historic evolution from woodland through to the legacy of Mayor W. F. Romain who led the restoration of landscape to the pre-colonial canopy and Oakville's "grove-like aspect."

The Waterfront Open Space Character Area's current function is active and passive recreational space. Two historical organizations have direct associations with recreational uses: the Oakville Club and Oakville Lawn Bowling Club. Buildings in this area are eclectic in their characters, reflecting their unique histories. Visual and physical access to the Lake Ontario and Sixteen Mile Creek shorelines is provided to pedestrians. Individual property addresses under this Character Area are listed in Table 2.

Waterfront Open Space guidance:

- a. Protect the narrow undefined "back lane" character of Water Street;
- b. Protect the narrow undefined "back lane" character of Front Street along Lakeside Park as part of the pedestrian walkway that follows the shoreline;
- c. Protect and enhance physical access to the public lands of the District along Lake Ontario and Sixteen Mile Creek shorelines;
- d. Protect, maintain and enhance a consistent pedestrian experience through the connectivity of the trails, street furniture, signage and lighting;
- e. Protect and conserve the topographical slope to the lake and harbour;
- f. Restore, maintain and enhance the tree canopy cover and park-like settings along the shoreline of Lake Ontario and bank of Sixteen Mile Creek;
- g. Integrate passive and active recreational activities that support community needs, and the cultural heritage values and heritage attributes of the District;
- h. Integrate historic and current Indigenous knowledge, traditions, activities and use into public open spaces through consultation with the Mississauga of the Credit First Nation, Six Nations of the Grand River and/or other Indigenous communities/groups such as Grandmother's Voice;

- i. Ensure public works maintain and conserve the cultural heritage values and heritage attributes of the District; and
- j. Commemorate two designated CHLs (i.e., Erchless Estate and the Oakville Harbour).



Streetscape and Landscape  
Character Analysis Area 1 - Waterfront  
Open Space



### **5.2.2 Area 2 - Old Oakville Settlement Pattern**

The Old Oakville Settlement Pattern Character Area is layered over the treaty lands and territory of the Mississaugas of the Credit First Nation and the traditional territories of the Huron Wendat and the Haudenosaunee. This landscape is part of the largely European settlement of the village of Oakville with a commercial centre around the port at the mouth of Sixteen Mile Creek. It has been a low-density residential neighbourhood since the early 19th century.

Many of the lots in this area are intact quarter-acre lots associated with the original 1833 street pattern, which provides a contemporary experience of the town's early character. The predominant architectural styles of the residential built form are: 19th Century Vernacular, Georgian Revival, and Neo-Classical.

The Old Oakville Settlement Pattern Character Area is framed by open spaces to the west and south, a mix of residential and park spaces along Navy, Front, Water and Thomas streets, mature tree canopy, and six intersection 'corners' that set the precedent for open corners within the District.

The Old Oakville Settlement Pattern Character Area also features the landmark of St. Jude's Anglican Church's tall bell tower that has multiple view lines within the area and the open church gardens at the west of the church building.

Individual property addresses under this Character Area are listed in Table 2.

Old Oakville Settlement Pattern guidance:

- a. Protect and maintain the openness at intersections and vistas between and through properties that are visible from the public realm;
- b. Protect and maintain the lot size, composition, height and setback of the built form along the east side of Navy Street as a contrast to the open space on the west side of Navy Street;
- c. Protect and maintain the ordered appearance of the wall along Navy Street at the Erchless Estate as a continuous element linking the length of the cultural heritage landscape;
- d. Protect and maintain the narrow undefined "back lane" character of the entrance to Front Street from Navy Street as an integral part of the pedestrian walkway system;
- e. Protect the built form along both sides of Thomas Street between Front Street and King Street as a cohesive streetscape with consistent narrow setbacks and materials;
- f. Protect the view lines and the deep front yard setbacks adjacent to and across from St. Jude's Anglican Church to conserve its landmark status in the area (King, Thomas and William streets);
- g. Protect and maintain the open spaces with gardens and mature trees around St. Jude's Anglican Church;



- h. Support additions and development that reflect and complement the historic architectural styles with unifying materials that are found in contributing properties such as traditional stucco and brick;
- i. Protect and maintain the views to the lake from Navy and Thomas streets; and
- j. Protect and enhance the tree canopy along Navy and Thomas streets.

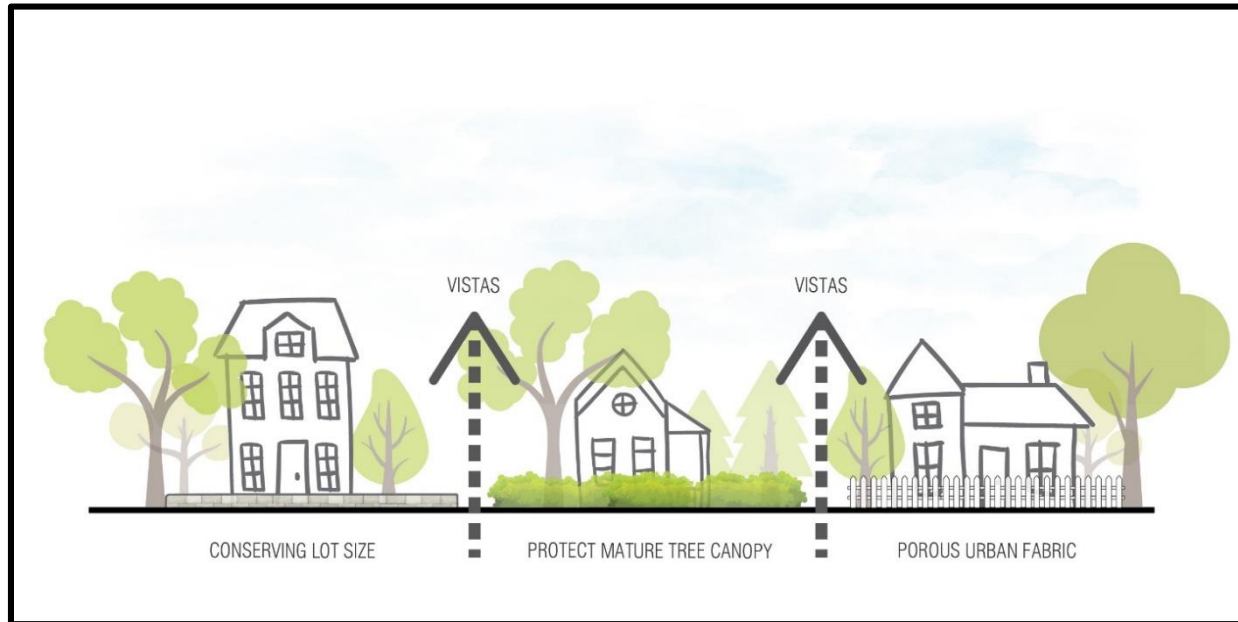
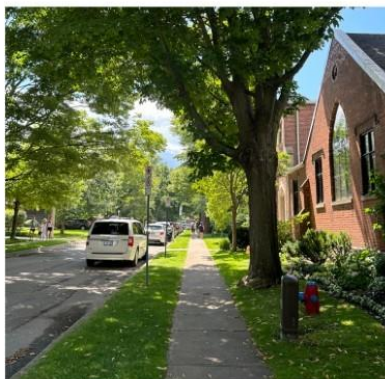


Figure 5: How to conserve views and vistas



Streetscape and Landscape  
Character Analysis Area 2 - Old  
Oakville Settlement Pattern



### **5.2.3 Area 3 - Gully Landscape**

The Gully Landscape Character Area is layered over the treaty lands and territory of the Mississaugas of the Credit First Nation and the traditional territories of the Huron Wendat and the Haudenosaunee and has been an evolving and growing low-density residential area since the largely European settlement spread outwards from the mouth of the Sixteen Mile Creek.

The area is characterized by a deep gully that primarily runs from the north to the south end of George Street, terminating at the George Street Parkette. The gully flows slightly deeper along Dunn Street terminating at Dingle Park. A gentler rise and fall of the topography is experienced from the west to the east, along William, King and Front streets within the Gully Landscape Character Area.

The area is well defined by mature tree canopies, reflecting Mayor W. F. Romain's 1868 tree restoration legacy. Because of the slope of the gully and the surrounding higher elevations, this Character Area offers very different viewpoints at higher and lower elevations that are unique within the District, including views to the lake and the downtown.

The built form has generous spacing between buildings and reflects a mix of architectural styles and periods due to the organic evolution of the area. In most cases, building heights step down and follow the topographical slopes creating vistas and views to the lake and the residential streetscape. Landscape elements, such as terracing, steps and retaining walls, are well-integrated into the prominent slopes of the area.

Individual property addresses under this Character Area are listed in Table 2.

Gully Landscape guidance:

- a. Protect the traditional quarter acre lot settlement pattern with appropriate lot sizes, front yard setbacks, openness at intersections, and vistas between properties;
- b. Protect and conserve both vertical (north to south slopes) and horizontal (east to west slopes) gully planes within the landscape through the adjustment of height and massing of new built form that responds appropriately to the grading;
- c. Protect and maintain the visual connectivity of residential properties to open spaces at intersections and mature tree canopies;
- d. Protect and maintain the openness of intersections in this area through low permeable fencing and soft landscaping;
- e. Protect and maintain the deep setback of houses along the north side of Front Street to emphasize Lake Ontario and the public open spaces of its shorelines and the contributing property at 212 Front Street;
- f. Support additions and development that reflect and complement the historic architectural styles with unifying materials that are found in contributing properties such as traditional stucco and brick;

- g. Protect and maintain the historic iron fencing that defines the street corners along Front Street and runs along the south side of King Street; and
- h. Strengthen the tree canopy within the area, particularly along Front Street.



Figure 6a: Building elevations and height following the topographical slope

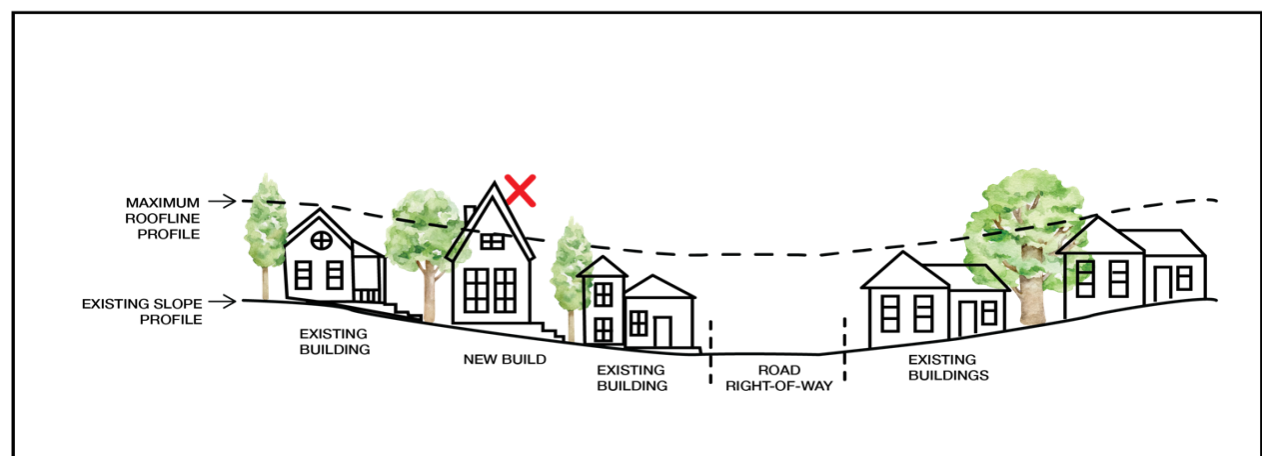


Figure 6b: Building elevations and height following the topographical slope



Streetscape and Landscape  
Character Analysis Area 3 - Gully  
Landscape



### **5.2.4 Area 4 - Mixed Residential Development Pattern**

The Mixed Residential Development Pattern Character Area is layered over the treaty lands and territory of the Mississaugas of the Credit First Nation and the traditional territories of the Huron Wendat and the Haudenosaunee. It is a low-density residential streetscape with development spanning the town's largely European settlement between 1830 and 1900, into the small-town building boom between 1900 and 1930 and then densification into post 1980s construction. As a result, this Character Area has a mix of architectural styles and materials, varied building setbacks, lot sizes and fencing that create distinct characteristics.

These distinctions include the following:

- The west section of the north side of King Street has grand and spaciouly scaled buildings with the two contributing properties at the corner of Dunn Street setback from both streets;
- The south side of King Street closes in moving from west to east with building setbacks coming closer to the street; and low soft landscaping on the north side;
- The west section of William Street is anchored by the contributing properties at the intersections with Dunn Street and Trafalgar Road and also by the use of low permeable fencing and soft landscaping;
- The east section of William Street has consistent setbacks, and the subtle rhythms of building facades create a well-ordered appearance. Despite different construction periods for buildings along the south side of William Street between Trafalgar Road and Reynolds Street, there is a unity of materials and a variety of gable roof forms;
- Like King Street, the east section of William Street closes in moving from west to east with building setbacks coming closer to the street.

Common features within this area include a Traditional Town road typology (e.g. two lanes of traffic and sidewalks on one or both sides of the road), framed by mature trees.

Individual property addresses under this Character Area are listed in Table 2.

Mixed Residential Development Pattern guidance:

- a. Protect the traditional quarter acre lot settlement pattern with appropriate lot sizes, front yard setbacks, openness at intersections, and vistas between properties;
- b. Protect the varied front yard setbacks, building heights and massing to retain the distinct streetscape character established through the evolution of this area;
- c. Support additions and development that reflect the historic architectural styles with unifying materials that are found in contributing properties such as traditional stucco, horizontal cladding, and brick;

- d. Protect and enhance the openness and vistas of the intersections in this area through low permeable fencing and soft landscaping; and
- e. Support and enhance the mature tree canopy on private lands.



Streetscape and Landscape  
Character Analysis Area 4 - Mixed  
Residential Development Pattern



### **5.2.5 Area 5 - St. Andrew's Traditional**

The St. Andrew's Traditional Character Area is layered over the treaty lands and territory of the Mississaugas of the Credit First Nation and the traditional territories of the Huron Wendat and the Haudenosaunee. The St. Andrew's Traditional Character Area is defined by its more than 180-year historical association with St. Andrew's Catholic Church. As a key landmark, St. Andrew's is the important and dominant feature of this area.

The residential built form adjacent to the church has generous setbacks, large lot sizes, and architectural character that complements St. Andrew's as a focal point. The Character Area has development spanning the town's largely European settlement between 1830 and 1900, into the small-town building boom between 1900 and 1930 and then densification into post 1980s construction. The predominant cladding materials used are light coloured stucco and horizontal frame cladding, giving visual unity to the area.

The streetscape is characterized by significant open space around the church, porous fencing and low height landscaping at the adjacent intersections, that create long view lines of St. Andrew's. Reynolds Street is characterized by its mature tree canopy, reflecting Mayor W. F. Romain's 1868 tree restoration legacy.

Individual property addresses under this Character Area are listed in Table 2.

St. Andrew's Traditional guidance:

- a. Protect the traditional quarter acre lot settlement pattern with appropriate lot sizes, front yard setbacks, openness at intersections, and vistas between properties;
- b. Protect the deep front yard property setbacks, heights and massing of buildings along King, Reynolds and Williams streets to reinforce the prominence of St. Andrew's;
- c. Protect and enhance the openness of the intersections in this area and views between residences, as well as the long view lines to St. Andrew's through low open fencing and soft landscaping;
- d. Support additions and development that reflect the historic architectural styles with unifying materials that are found in contributing properties such as traditional stucco and horizontal frame cladding;
- e. Support new development that protects the open corners surrounding St. Andrew's and its ancillary buildings, maintains the low heights and deep setbacks of surrounding properties, and uses complementary and unifying architecture and materials, to ensure St. Andrew's remains the focal point;
- f. Protect the narrow undefined "back lane" character of Allan Street as part of the pedestrian walkway that leads to Lake Ontario;



Streetscape and Landscape  
Character Analysis Area 5 - St.  
Andrews Traditional



### 5.2.6 Key Streetscape Overlay

The Key Streetscape Overlay is a significant and unique interface between open space, private properties and transit ways along Water Street, Navy Street and Front Street. These streetscapes define the intact historic residential edge within the District, the historic industrial and commercial harbour functions of Navy Street and Water Street, and the narrow one-way lane of Front Street.

This overlay provides supplementary character defining elements that enhance specific properties in:

- Area 2 – Old Oakville Settlement Patterns
- Area 3 – Gully Landscape

Navy Street, Front Street and Water Street are three important roads within the District that have higher public profiles and utility due to their adjacency to open spaces and highly visited Town of Oakville landmarks. They are also associated with views that need to be protected and preserved.

- Navy Street, terminating at the lake, characterized by the mature tree canopy that reflects Mayor W. F. Romain's 1868 tree restoration legacy, as well as framing views to the lake.
- Front Street is a very narrow and intimate one-way road that supports on-road pedestrian connections between Lakeside Park, George Street Parkette and Dingle Park.
- The steep valley slope towards the Sixteen Mile Creek is emphasized through terraces, retaining walls and exposed foundations within the built form. The built form also complements the narrow laneway character of Water Street.
- The built form of the homes along the east side of Navy Street, as well as 64 Navy Street and 115 William Street on the west side, date to the early European settlement, with an established uniform street wall that relies on the built form setbacks, low 1 ½ - 2 storey heights, low permeable fencing and soft landscaping.
- The importance of Navy Street residential streetscape edge is contrasted by the openness in the public realm on the west side of Navy Street including Market Square, the Lawn Bowling Club, Lakeside Park and the cultural heritage landscape of Erchless Estate.
- The built form along Front Street has a significant relationship with the adjacent public open space and parks. This area is an important historic and current gateway to visual and physical access to Lake Ontario.
- The built form on the south side of Front Street has small setbacks given the narrow depth of the lots, while the homes along the north side of Front Street have deeper setbacks that give emphasis to the narrow laneway and the presence of the Lake.

Key Streetscape Overlay guidance includes:

- a. Protect and strengthen Navy Street and Front Street as important streetscapes that have relationships with the historic European settlement of Oakville and their evolving use as public open space and parks. They serve an important function as gateways to visual and physical access to Lake Ontario and the Sixteen Mile Creek.
- b. Protect and strengthen the Water Street retaining walls and terracing of built form along the street that respond to the steep topographical slopes of the District;
- c. Protect and conserve the uniform street wall and defined residential street edges through continuous front yard setbacks, building heights, low permeable fencing and soft landscaping along Navy Street, Front Street and Water Street;
- d. Protect and conserve the historic iron fencing and stone pillars along Front Street;
- e. Protect and conserve open spaces and open views through continuous low profile permeable fencing and soft low-profile landscaping; and,
- f. Strengthen and enhance the mature street tree canopy and protected views.

## 5.3 Guidelines for Contributing Properties

### 5.3.1 General Guidelines

#### 5.3.1.1 Understanding

Alterations and additions to a contributing property shall be based on a firm understanding of the historic fabric of that property and how it contributes to the cultural heritage value of the property's Character Area and the District.

The Inventory Sheets developed for each property as part of the District Study (and found in Appendix A of the District Study) shall be used to help understand property history, past alterations/additions, site context, cultural heritage value, noting that the Inventory Sheets are not intended to be a complete list of all features of a property.

- (1) Interventions to a contributing property shall consider:
  - a. the historic architectural style and identified period of significance of the building(s);
  - b. changes that have been made to the building(s)/property over time, noting that historic alterations may have value along with the original;
  - c. making the changes physically and visually compatible with and subordinate to the historic fabric of heritage buildings; and
  - d. the current condition of the heritage building and/or other property features.
- (2) Where historic fabric is damaged or deteriorated, determine the extent of the damage prior to planning any interventions to determine the appropriate scope of work and to preserve the building fabric.
  - a. Replacement cladding materials that match historic materials (such as stucco) retain the importance of the historic cladding, meaning that the cladding of a building does not lose its overall significance if replacement is required, and historic materials should be matched as closely as possible.
- (3) Alterations and additions to a contributing property may be permitted by the town when the following has been completed:
  - a. the impact of the proposed alterations and/or additions on the contributing property, the Character Area of the property and the District have been identified:
    - i. this means making the changes physically and visually compatible with and subordinate to the historic fabric of the building; and,
    - ii. ensuring that the proposed changes meet the objectives set out in Section 3 of this Plan;
  - b. on corner lots, special consideration has been given for both public facing elevations and their impact on the streetscapes; and
  - c. appropriate mitigation measures have been proposed.

- (4) A Heritage Impact Assessment (HIA) may be required to be completed in accordance with the Town of Oakville's Development Application Guidelines for Heritage Impact Assessments.

### **5.3.1.2 Compliance**

- (1) Current codes and standards pertaining to health and safety, security, accessibility and sustainability requirements shall be adhered to in a way that does not negatively impact the heritage character of the contributing property and the District. This includes but is in no way limited to the Ontario Building Code (OBC) and Accessibility for Ontarians with Disabilities Act (AODA).

Note: OBC, Part 11, Section 11.5, Compliance Alternatives may provide some relief for requirements contained in OBC Part 3, 4, 6 or 8 where the chief building official is satisfied that the compliance with the requirement is impracticable because it is detrimental to the preservation of a heritage building.

### **5.3.1.3 Demolition and Relocation**

- (1) These guidelines apply to the full and partial demolition, and relocation of heritage buildings on contributing properties in the District:
- a. The demolition of heritage buildings on contributing properties in the District is not permitted, except in exceptional circumstances (i.e., where the property has suffered severe damage due to unforeseen events).
  - b. A structural engineering assessment, prepared by a structural engineer licensed to practice in Ontario and with experience with historic structures, may be required where the structural integrity of the heritage building is compromised, and demolition is being sought because of severe damage to the building structure. A peer review of the structural engineering report may be required, at the applicant's expense. Demolition will only be permitted if the town concurs with the structural assessment and/or peer review.
  - c. Proposed changes to a heritage building shall not result in a condition that renders the structure vulnerable to structural failure.
  - d. All mitigation measures shall be taken during construction to ensure that the heritage building does not result in a condition that renders the structure vulnerable to structural failure.
  - e. A heritage permit for the demolition of a heritage building in full or in part shall not be granted until the design for the replacement building or alterations has been submitted and approved. The design of replacement buildings and alterations must comply with the guidelines for new development and/or alterations as set out in this Plan.
  - f. Relocation of a heritage building within the lot may be permitted on a case-by-case basis. The new location of the heritage building shall meet all appropriate guidelines in this Plan, including but not limited to, spaces around buildings, views, landscaping and the impacted Character Area.
  - g. Removal or demolition of a heritage attribute and/or structure on a Part IV property within the District may require a notice of intention to demolish to be submitted in accordance with the OHA.

**5.3.1.4 Lot Severances and Assembly**

- a. Protect the traditional quarter acre lot settlement pattern with appropriate lot sizes, front yard setbacks, openness at intersections, and vistas between properties;
- b. Consent applications will be evaluated on a case-by-case basis for lot severances and assembly in the District.
- c. All severances must conform to provincial and local policies and by-laws including, but not limited to, the in-effect Official Plan and Zoning By-law.
- d. The lot size of any infill property or severance shall reflect the character of the surrounding lot fabric and impacted Character Area.

**5.3.1.5 Spaces Around Buildings**

- a. Maintain traditional distances between buildings visible from the public realm and the picturesque rhythm of buildings with streetscapes.
- b. Maintain traditional views and orientation of heritage buildings from the public realm.
- c. Prominent building features shall not be blocked or obscured with fencing, hardscaping features, service or utility equipment, garages or other accessory buildings.
- d. Maintain historical means of access to contributing properties, including driveways and walkways.
- e. Special consideration may apply to buildings of atypical orientation, on lakefront lots and/or on corner lots.



### 5.3.2 Alterations to Contributing Properties

#### 5.3.2.1 Historic Fabric

- (1) Alterations to a contributing property shall be physically and visually compatible with the historic fabric of the property and the property's Character Area, with regard to architectural style, detailing and materials.
  - a. Alterations shall not create a false sense of historical development by adding heritage building elements from other places, properties or historic periods, and shall not combine features that never coexisted on the building.
  - b. When reinstating historical architectural elements, ensure that the design of forms, materials and detailing are based on appropriate historical photographs or documentation.
- (2) Alterations to a contributing property shall minimize loss of historic fabric.
  - a. Repair damaged or deteriorated historic fabric rather than replacing it.
  - b. When replacements are necessary, replace *historic fabric in kind*, using the same form, materials and details as the original

#### Best Practices – Historic fabric

Reinstate historic architectural elements that have been removed, neglected or obscured when undertaking alterations to a Contributing Property.

#### 5.3.2.2 Roofs

- (1) Protect and maintain historic roof forms and features, including dormers, eaves, eavestroughs and downspouts, soffits, finials, decorative trim and other features related to specific architectural styles.
  - a. Repair rather than replace historic roof features using historically appropriate materials.
  - b. Replace only roof features that have deteriorated beyond repair, rather than replacing an entire roof or roofline.
  - c. Protect and maintain original or historic eavestroughs, downspouts and flashing wherever possible. If repair is not possible due to condition, replacement with matching materials is required.
  - d. Where documentary evidence of original or historic eavestrough and downspout profiles exist, the reinstatement of these features is encouraged.
- (2) Roofing materials shall be replaced in kind, matching the form, materials and details of the historic roof system. The exception to 'in kind' replacement is restoration of the roof material or details to an earlier style/material based on evidence and documentation.
  - a. Asphalt shingles and cedar shingles are appropriate roofing materials for the District. The use of alternative materials that mimic the appearance of asphalt or cedar may be considered on a case-by-case basis.
  - b. Slate may be historically appropriate for more prominent buildings, such as St. Jude's Anglican Church.
  - c. Metal roofing materials shall not be permitted for full roof systems and may only be used for accents where compatible with the architectural style of the heritage building, Character Area and District.

- d. Membranes may be considered on flat roofs.
  - e. Roofing materials such as PVC, terracotta and ceramic that do not traditionally exist in the District are not permitted.
  - f. Aluminum or PVC soffits and fascia shall not be permitted on heritage buildings where they are not already installed.
  - g. New flashing should be coloured to match the wall or roof materials against which it is located.
- (3) Rooftop equipment and drainage elements shall be discreetly integrated and shall not negatively impact the character of the heritage building, Character Area and the District.
- a. New skylights, green roofs, roof ventilation equipment, plumbing vents, solar cells and other stacks may be permitted, providing they are located on side or rear elevations that are not visible from the public realm.
  - b. New eavestroughs and downspouts shall be appropriately designed to manage water properly and direct drainage away from building foundations.

### **Examples of Roofs**



Figure 7: 143 Front Street – Cedar shake gable roof with dormers and stucco cladding





Figure 8: 212 Front Street (left) – Cedar shake hip roof with dormers and stucco cladding  
Figure 9: 176 Front Street (right) – Cedar shake gable roof with stucco cladding



Figure 10: 329 William Street (left) – Cedar shake hip roof with stucco cladding  
Figure 11: 307 William Street (right) – Asphalt hip roof with horizontal wood cladding



Figure 12: 65 Navy Street (left) – Asphalt gable roof on house and garage with stucco cladding  
Figure 13: 301 William Street (right) – Asphalt gable roof with stucco cladding



Figure 14: 27 Reynolds Street – Asphalt gable roof with shake cladding

#### Best Practices - Roofs

Determine the cause of any distress, damage or deterioration of a roof system through investigation, monitoring and minimally invasive testing techniques. It is in the property owner's best interest to address any of these issues as soon as possible to prevent further deterioration or additional complications.

Where structural faults or problems in the historic fabric exists, the replacement of structural materials with contemporary materials may be permitted on a case-by-case basis. Any changes visible on the exterior of the property shall be physically and visually compatible with the heritage building, following recognized conservation practices.

Where historic alterations have been made to address structural faults or problems in the historic roof design, they should be retained so long as they do not have a negative impact on the heritage character of the building. If they do have a negative impact on the heritage character of the building, removal and restoration is encouraged.

Where evidence exists, reinstate original or historic details or materials that have been removed from the roof system, including chimneys, finials, eavestrough/downspouts and other decorative or functional elements.

Completely remove existing materials, such as shingles, before applying new roofing.



### 5.3.2.3 Chimneys

- (1) When a historic chimney is no longer in use, cap and conserve the chimney rather than removing it. Proposals to remove replacement and/or non-historic chimneys will be assessed on a case-by-case basis.
- (2) New chimneys may be permitted, providing they match the design and architectural style of the heritage building and existing chimneys, where applicable.

#### *Examples of Chimneys*



Figure 15: 53 Dunn Street (left)

Figure 16: 295 King Street (right)

### 5.3.2.4 Dormers

- (1) Protect and maintain historic dormers.
  - a. The removal or obstruction of historic dormers on public-facing elevations of a contributing property shall not be permitted.
  - b. Repair rather than replace historic dormers using historically appropriate materials.
  - c. Replace only dormer materials that have deteriorated beyond repair, rather than replace an entire dormer. Replacements, in whole or part, shall be in kind, matching the form, materials and details of the historic dormer.
  - d. For windows within dormers, refer to the guidelines in sub-section 5.3.2.5.
- (2) New dormers may be permitted on heritage buildings only where they are compatible with the architecture of the heritage building.
  - a. New dormers shall complement the design and scale of the roof, windows and any existing dormers.
  - b. New dormers shall not be permitted on public-facing portions of the original or historic portion of a heritage building and should be located on rear or interior-facing side elevations or additions.

- c. New dormer windows should be square or vertically proportioned and should have the same proportions or be slightly smaller than the uppermost windows in the elevation below.

#### Best Practices - Dormers

Where evidence exists, reinstate original or historic dormers or details or materials that have been removed from dormers.

Dormer body proportions should be driven by the dormer window proportions.

#### **5.3.2.5 Windows**

- (1) Protect and maintain the location, orientation, shape and size of historic window openings.
  - a. The removal or obstruction of historic window openings is strongly discouraged. Consideration may be given to alterations to historic windows that are located on side or rear elevations in a manner that does not diminish the character of the heritage building, Character Area or District.
  - b. Maintain historic solid-to-void ratios and the historic rhythm of windows and bays.
- (2) Protect and maintain historic window features, including trim, sills, surrounds, brick moulds, materials, frame, sashing, muntins, hardware, glazing and storm windows.
  - a. Repair rather than replace historic window features.
  - b. Replace only those specific window features that have deteriorated beyond repair, rather than replacing an entire window unit.
  - c. Historic wood trim and surrounds shall be protected and maintained. If repair/replacement is required due to poor condition, materials and profile/design shall match the existing.
  - d. Historic stone or masonry sills, surrounds or voussoirs shall be protected and maintained. If repair/replacement is required due to poor condition, materials and profile/design shall match the existing.
- (3) The Town of Oakville's Heritage Window Replacement Guidelines shall be used to determine if original/historic windows can be replaced.
  - a. Should replacement windows be approved, the following applies:
    - i. Maintain the pattern of window divisions in their original configurations.
    - ii. Glue-on or snap-on muntins (i.e. window grilles) shall not be permitted. Muntins shall be true divided lights or simulated divided lights with dark spacers at every muntin. Simulated divided lights should be integral to the window sash. Aluminum muntins may be used where stronger muntins are required to support sealed window units while maintaining the original thinner muntin profiles. These muntins shall be made integral to the sash frame.
    - iii. New windows that replace existing wood windows shall be wood. Aluminum-clad wood may be permitted on side or rear elevations that are not visible from the public realm, providing the original moulding profiles have been reproduced.
    - iv. Existing leaded windows shall be replaced with new leaded windows to match.
    - v. Vinyl and fiberglass windows shall not be permitted. They are not compatible with the character of heritage buildings, Character Areas and the District, and do not

- adequately replicate wood windows in their detailing, finishes, profiles and colour as their sashes, frames and mouldings are often much flatter in appearance.
- vi. Ensure that all sills are sloped away from the exterior wall, with drip edges when they extend beyond the face of the exterior cladding.
  - vii. Remove, repair and reuse existing brick moulds. When these cannot be salvaged, their size, profile and detailing shall be replicated. Ensure that the original relationship between the brick moulds, the window trim and the window frame are maintained.
  - viii. Remove, repair and reuse existing window trim. When these cannot be salvaged, their material, size, profile and detailing shall be replicated. Ensure that the original relationships between brick moulds, trim and frame are maintained. Ensure that the original relationship between the window trim and the exterior cladding is maintained.
- (4) New window openings may be permitted only where their location, alignment, proportions, materials and design is compatible with the architecture of the heritage building, Character Area and District as exemplified by the historic rhythm of windows and bays. New window openings should be located on side or rear elevations of heritage buildings.
- a. Materials and design of new windows shall be compatible with historic windows on the same elevation.
  - b. Where traditionally operable window styles are used in new window openings, the new window shall also be operable.

### ***Examples of Windows & Shutters***



Figure 17: 26 Thomas Street (left) – Wood window with storm

Figure 18: 329 William Street (right) – Wood windows with storms

**Best Practices - Windows**

Reinstate historic windows that have been removed or blocked, based on appropriate documentary evidence.

Replace newer unsympathetic windows based on appropriate documentary evidence.

Historic wood windows, when properly maintained, can last 60 to 100 years. Aluminum, vinyl or fiberglass windows cannot be repaired and need to be replaced. Modern sealed window units have a 15 to 20-year life span. Consider the following alternatives prior to replacing historic windows or designing new windows:

- Reinstate exterior storm windows that have been removed. A single glazed window with an exterior storm window can be as effective as a sealed window unit.
- Retrofit sealed glazing units into the historic sashes as an alternative to replacing the whole window.

Replacement glazing should be considered only when the existing glazing is damaged, or the historic sash is being retrofitted with sealed glazing units.

Replace all damaged weather-stripping to ensure air tightness of window assembly. The performance of single-glazed windows will be significantly improved by proper weather-stripping that reduces air infiltration.

Regularly maintain the caulking around the windows.

The sills are often the most damaged features of windows, and can be replaced using dutchman repairs, leaving the window frame in place. Sashes can be removed for in-shop repairs.

Historically operable windows should maintain their operability. Avoid painting windows shut or otherwise sealing them in a manner that is difficult to reverse.

**5.3.2.6 Shutters**

(1) Protect and maintain historic shutters.

- a. The removal (without replacement) or permanent obstruction of historic shutters shall not be permitted.
- b. Repair rather than replace historic shutters.
- c. Replace only the shutter elements (e.g. individual louvers) that have deteriorated beyond repair, rather than replacing an entire shutter.

(2) When shutter replacement is necessary, shutters shall be replaced in kind, matching the form, materials and details of the original.

(3) New shutters may be permitted only where they are physically and visually compatible with the architecture of the heritage building.



- a. Attach shutters to the window casing rather than the wall. Hinges and hooks should be used to ensure shutters are functional.
- b. Design shutters in a style appropriate to the architectural style of the heritage building, including panelled or louvered styles. Louvers may be fixed or operable.
- c. The dimensions of shutters shall be one-half the width of the sash they are covering, allowing them to effectively cover the window if closed.
- d. Wooden shutters shall be painted in a colour appropriate for the materiality and colours of the heritage building.



Figure 19: 308 William Street (left) – Wood windows with storm and operating louvered shutters  
Figure 20: 65 Navy Street (right) – Wood windows with thermal pane and operating louvered shutters



Figure 21: 215 William Street – Wood windows with storm and operating shutters

**Best Practices – Shutters**

Reinstate historic shutters that have been removed or blocked, based on documentary evidence.

Replace newer unsympathetic shutters, such as those that are too narrow, based on documentary evidence.

Restore historic shutters that have been removed from their hinges and attached to the wall on either side of the window. New hardware should be used to re-hang the shutters so that they are operable.

**5.3.2.7 Entrances**

- (1) Protect and maintain the location, size and orientation of historic entrances.
  - a. The removal or obstruction of historic entrance openings shall not be permitted.
  - b. Maintain historic entrances as functioning entrances.
  - c. Maintain historic hierarchies of entrances on buildings, where they exist.
- (2) Protect and maintain historic entrance features, including doors, door surrounds, materials, glazing, lighting and steps.
  - a. Repair rather than replace historic entrance features.
  - b. Replace only entrance features that have deteriorated beyond repair, rather than replacing the entire entrance.
- (3) When the replacement of entrance features is necessary, features shall be replaced in kind, matching the form, appearance, materials and details of the original.
  - a. Existing wood doors shall be replaced with wood doors to match.
  - b. Aluminum doors that mimic wood panelling may be considered on a case-by-case basis for side or rear elevations that are not visible from the public realm, providing they effectively replicate wood doors in their detailing, finishes and colour.
  - c. Wood is the most appropriate material for screen doors. Aluminum doors that mimic wood panelling may be considered for side or rear elevations not visible from the public realm on a case-by-case basis, providing they effectively replicate wood screen doors in their detailing, finishes and colour.
  - d. Sliding doors and other doors that do not swing shall not be permitted on heritage buildings.
- (4) New entrances on the historic portion of a heritage building may be permitted only where their location, alignment, proportions, materials and design is compatible with the architecture of the heritage building, as exemplified by the historic entrances and the rhythm of bays.
  - a. New entrances shall be subordinate to the primary historic entrance in terms of location and design. New entrances should be located on side or rear elevations, not visible from the public realm.
  - b. Doors and door surrounds of new entrances shall be detailed in a style and materials appropriate to the architecture of the heritage building.



- c. Wood panelled doors are most appropriate for the District. Aluminum doors that mimic wood panelling may be considered for side or rear elevations not visible from the public realm on a case-by-case basis, providing they effectively replicate wood doors in their detailing, finishes and colour.

### **Examples of Entrances and Doors**



Figure 22: 29 Navy Street (left) – Main entrance wood door with surround facing street

Figure 23: 212 Front Street (right) – Main entrance wood door with wood storm door facing street



Figure 24: 212 King Street – Main entrance wood door facing street



Figure 25: 145 William Street – Main entrance wood door facing street

#### *Best Practices – Entrances*

Reinstate historic entrance openings that have been removed or blocked, based on appropriate documentary evidence.

Replace newer unsympathetic entrance features based on appropriate documentary evidence.

Improve weather protection and energy efficiency of existing doors through re-puttying and replacing or installing weather-stripping, adjusting hardware, and sealing openings and joints, rather than replacing the historic doors.

#### **5.3.2.8 Foundations**

- (1) Retain sound or partially deteriorated foundation walls that are repairable. Stabilize exterior walls by using structural reinforcement, weather protection, or correcting unsafe conditions as required.
- (2) When the foundations of buildings or porches have deteriorated excessively, re-build damaged foundation walls or piers rather than demolishing the structure. Install temporary support for the structure while the damaged wall or pier is reconstructed.
- (3) Existing original or historical foundations shall not be clad or covered with contemporary materials including parging or painting (especially over unpainted masonry).



### **Examples of Foundations**



Figure 26: 115 William Street – Lakestone foundation

#### **Best Practices – Foundations**

Where historic foundations are extensively deteriorated, contemporary methods may be used to construct new foundations. Where visible above grade, ensure the new foundations are clad in salvaged stone wherever possible or new stone material that matches the original or historic style and quality of the historic material.

Improve the drying ability of exterior wall assemblies through suitable heating and/or ventilation measures wherever possible.

Painting or covering over historic masonry with contemporary materials can prevent proper monitoring and maintenance of the exterior building materials and create new issues such as spalling and mortar deterioration. Instead, repair and/or replace the existing exterior building materials with like material.

Where original or historic masonry is concealed with later cladding materials, removal and repair is highly encouraged and recommended. Use historic documentation to rehabilitate the foundations.

Protect and maintain existing foundations by ensuring that site drainage is directed away from building and porch foundations.

### **5.3.2.9 Porches, Verandahs and Porticos**

- (1) Protect and maintain historic porches, verandahs, and porticos and their features, including posts, brackets, railings, steps and roofs.
  - a. The removal or obstruction of historic porch, verandah and portico features shall not be permitted.
  - b. Repair rather than replace historic features.
  - c. Replace only the specific features that have deteriorated beyond repair, rather than replacing an entire porch, verandah or portico.
- (2) New porches, verandahs and porticos may be permitted as additions to heritage buildings, provided that they are in a style that is appropriate for the architecture of the heritage building, Character Area and District, and are physically and visually compatible with the heritage building in terms of location, orientation, design and materials.
  - a. When architectural elements such as columns and entablatures are used on new porches, verandahs or porticos, they should conform to classical proportions.
  - b. Replacement materials for porches, verandahs and porticos should be wood with brick and stone used for piers or bases. The use of composite and engineered wood or wrought iron may be considered on a case-by-case basis.

### **Examples of Porches, Verandahs and Porticos**



Figure 27: 53 Navy Street (left)– Open wrap-around porch

Figure 28: 19 Navy Street (right) – Glassed-in side porch





Figure 29: 154 King Street (left) – Open wrap-around porch  
 Figure 30: 27 Reynolds (right) – Covered porch



Figure 31: 295 & 297 William Street (left) – Open front porch  
 Figure 32: 212 King Street (right) – Covered open porch

### *Best Practices – Porches, Verandahs and Porticos*

Where porches, verandahs and porticos are extensively deteriorated or missing parts, replace in kind where there are surviving prototypes. Use salvaged material wherever possible or new materials that match the original or historic style and quality.

Reinstate historic porches, verandahs and porticos that have been removed or blocked, based on appropriate documentary evidence.

Replace newer unsympathetic porch, verandah and portico features based on appropriate documentary evidence.

### 5.3.2.10 Garages and Coach Houses

- (1) Protect and maintain historic garages and coach houses.
- (2) Alterations to existing historic garages or coach houses shall not negatively impact the contributing property, Character Area or District in terms of the garage's architectural style, massing, design, materials, colour and location.
- (3) Non-historic garages or coach houses on contributing properties may be removed or replaced, provided any replacement is subordinate to and compatible with the character of the contributing property, Character Area and District. The replacement structure shall follow the guidelines outlined in Additions to Contributing Properties, Section 5.3.4.14 'Garages and Coach Houses'.

### Examples of Garages and Coach Houses



Figure 33: 19 Navy Street – Detached garage



Figure 34: 29 Thomas Street (left) – Detached garage (restored barn)



Figure 35: 32 Thomas Street (right) – Detached garage





Figure 36: 22 Reynolds Street (left) – Detached garage  
Figure 37: 65 Navy Street (right) – Detached garage



Figure 38: 66 Dunn Street – Attached 1 ½ storey garage with appearance of detachment through single storey connection to main residence





Figure 39: 145 William Street (left) – Separate coach house set back in rear yard

Figure 40: 53 Navy Street (right) – Coach House set back and facing William Street away from main residence



Figure 41: 43 Trafalgar Road (left) – Attached garage with appearance of detachment through single storey connection to main residence

Figure 42: Trafalgar Road (right) – Attached single car garage with shed appearance setback to rear of property

#### *Best Practices – Garages and Coach Houses*

Determine the cause of damage or deterioration to historic garages and coach houses through investigation, monitoring and minimally invasive testing techniques.

Address signs of damage and deterioration immediately, so as not to cause further degradation.



### 5.3.2.11 Utility Service Equipment

- (1) Utility and service equipment shall not negatively impact the contributing property, Character Area or the District and shall be located so as to minimize its visibility from the public realm.
  - a. Service hardware (such as utility meters, cable TV and telephone connections), commercial mechanical elements (such as dryer vents, heat reclamation vents, furnace and water heater exhausts, gas fireplace exhausts and kitchen exhausts), and ground mounted electrical and mechanical hardware (such as heat pumps, transformers and air conditioning units) shall not be located on main elevations and should not be visible from the public realm. If visible from the public realm, screen appropriately using landscaping features. (See Section 5.7- Landscape and Site Design Guidelines for Privately Owned Lands, for requirements on screening)
  - b. New aboveground infrastructure, including hydro lines, should be buried.
  - c. Solar panels may be permitted if oriented so that they do not compromise the character of the contributing property, Character Area or District and are preferred on side and rear elevations.

#### Best Practices – Utility Service Equipment

Find solutions to incorporate sustainable building initiatives without having a negative impact on the character of contributing properties, Character Areas and the District.

### 5.3.2.12 Exterior Walls

- (1) Protect and maintain historic architectural features of exterior walls, including cladding materials, exposed structural elements (such as pilasters and half-timbering) and decorative elements (such as articulated brickwork, quoining and date stones).
  - a. The removal or obstruction of historic architectural features of exterior walls shall not be permitted.
  - b. Alterations and maintenance work should not remove, cover or obscure the patina of age or irregularities found in older work and materials. Patina is the result of the natural aging of a material and provides it with a protective coating.
  - c. Repair rather than replace historic materials.
  - d. Replace only specific sections of materials that have deteriorated beyond repair whenever possible. Document the patterns, profiles and detailing of materials prior to repairing or replacing them.
  - e. Where historic materials have deteriorated beyond repair, replace deteriorated materials in kind, maintaining the compositions, size, finishes, patterns, tooling and colours of the original. Replacement materials should also match the physical characteristics of the original such as vapour permeability and compressive strength.
  - f. Historically unpainted masonry surfaces shall not be painted.
  - g. Maintain historically painted surfaces, including stucco, wood clapboard and wood shingles.
  - h. New finishes or coatings that alter the appearance of historic materials shall not be applied, especially where these finishes are substitutes for the repair of historic materials. Staining of new masonry to match historic masonry may be permitted with products that do not alter the permeability of the masonry. A heritage permit will be

required to approve colour, product and staining technique. Mortar mixes should be coloured to match and staining over top may not be permitted.

- i. Exterior Insulation and Finish Systems (EIFS) shall not be permitted. In addition to being aesthetically incompatible with the character of contributing properties, Character Areas and the District, this material can promote mould growth and retain moisture in building fabrics that were intended to breath.

### ***Examples of Exterior Walls***



Figure 43: 18, 20 and 26 Thomas Street (left) – Example of horizontal wood cladding  
Figure 44: 307 William Street (right) – Example of horizontal wood cladding



Figure 45: 53 Dunn Street – Example of horizontal wood cladding





Figure 46: 53 Thomas Street (left) – Example of stucco cladding

Figure 47: 22 Reynolds Street (right) – Example of stucco cladding



Figure 48: 29 Thomas Street (left) – Example of stucco cladding

Figure 49: 19 Navy Street (right) – Example of early brick cladding



Figure 50: 350 King Street (left) – Example of late 1800s brick cladding

Figure 51: 302 William Street (right) – Example of early 1900s brick cladding with open front porch





Figure 52: 309 King Street (left) – Example of stucco cladding with shake dormers

Figure 53: 43 Trafalgar Road (right) – Combination brick and shake siding with asphalt roof

### Best Practices – Exterior Walls

Regularly inspect and maintain the exterior materials on heritage buildings.

Ensure that water shedding, drainage and management elements are functional: that sills, cap stones and other exposed horizontal are sloped with drip edges; that the ground is sloped away from the foundations to prevent splash-back and provide proper drainage; that gutters and downspouts are functional; that sealants and flashing are in good condition; that projecting eaves are maintained; and that masonry joints are sound.

When cleaning, use the gentlest means possible so as to remove soiling while maintaining the patina of the historic materials. Protect adjacent surfaces including the landscaping. Perform mock-ups prior to cleaning to ensure that the chosen method will not negatively impact the historic materials.

Determine and address the causes of material deterioration prior to developing any maintenance, repair or replacement scope of work.

Assess the condition and composition of the stucco and its substrate prior to developing any repairs strategies. Document the original finishes' texture and colour prior to undertaking any work.

Remove deteriorated paint prior to repainting. When removing paint, use the gentlest means possible. Select colours for repainting that are compatible with the architectural style, cladding materials and colour of the heritage building.

Replace newer unsympathetic exterior cladding materials based on historic documentation.

### 5.3.2.13 Guidelines for Specific Materials

The guidelines below provide additional direction for specific materials. Refer to Parks Canada's Standards and Guidelines for the Conservation of Historic Places for additional guidelines and best practices relating to specific cladding materials (i.e. masonry, wood, concrete, metals, glass, plaster and stucco).

(1) Protect and maintain the historic appearance of wood finishes and elements.

- a. Identify the species of wood prior to doing any repairs or replacements. For dutchman repairs, the pieced-in sections should, whenever possible, match the species of the existing element being repaired.
- b. When replacing and/or repairing wood siding, shingles and other wood elements, document their pattern, size, detailing, profile, and colour prior to removal.
- c. Replace *in kind* any wood element including wood siding, shingles, trim, half-timbering, decorative elements, railings, stairs, porch columns and finishes. Composite and engineered wood may be considered on a case-by-case basis, providing they effectively replicate the historic cladding, trim or other sections of the exterior wall.
- d. Replicate historic siding and shingle patterns when replacing or repairing them.
- e. Ensure that the original relationships between the trim and siding are maintained.
- f. Historic wood siding and shingles shall not be clad over.
- g. Metal, vinyl or plastic composite siding shall not be permitted.
- h. Eaves, soffits or fascias shall not be permitted to be clad or flashed in metal.

(2) Protect and maintain the historic appearance of masonry (brick and stone).

- a. Sandblasting masonry surfaces shall not be permitted. This will remove the harder, more durable kilned face of the brick exposing its softer interior.
- b. Brick or stone shall not be painted, unless it has already been painted. Repainting already painted masonry is permitted.
- c. When removing paint from masonry surfaces, do so in a manner that does not damage the historic materials. Protect adjacent surfaces and landscaping. A heritage permit is required for paint removals.
- d. When replacing bricks, ensure that the new bricks match the appearance and physical properties of the original. Modern bricks are stronger, smaller and less vapour permeable than historic bricks. The use of a stronger brick in a historic wall assembly can accelerate the deterioration of the surrounding historic bricks. Historic bricks are still available from specialty suppliers. Staining new bricks to match historic brick may be permitted, see guidance in Section 5.3.2.12 Exterior Walls.
- e. Repoint brick masonry using a physically and visually compatible mortar mixture and traditional pointing methods, recreating the original tooling and joint profile. The pointing mortar should be softer and more vapour-permeable than the masonry. When cutting or raking out joints, use appropriate methods to ensure that the arises of the bricks or stone are protected from damage. When rebuilding a section of historic masonry, ensure that the original coursing and joint widths are maintained.
- f. Protective sealants shall not be applied to masonry. These sealants are often vapour impermeable and will trap moisture within the masonry assembly accelerating its deterioration.

(3) Protect and maintain the historic appearance of the stucco surfaces.

- a. Exterior Insulation and Finish Systems (EIFS) shall not be permitted as a replacement material for stucco. In addition to being aesthetically incompatible with the heritage character of the District, this material can promote mould growth and retain moisture in building fabrics that were intended to breath.
- b. Repair the substrate, where required, to ensure the structural stability of the stucco finish.
- c. Retain and repair existing stucco finishes where possible. When replacement is required, replace stucco with in-kind materials, typically with a historically appropriate three-coat application. Reproduce the original finish, colour, strength and texture patterns.
- d. Ensure that the patching and repair materials used are physically and visually compatible with the historic stucco.
- e. Ensure that the paint systems used are physically and visually compatible with the historic stucco, especially with regards to vapour permeability.

### **5.3.3 Additions to Contributing Properties**

#### **5.3.3.1 Historic fabric**

- (1) Additions to a heritage building on a contributing property shall be physically and visually compatible with, complementary and sympathetic to and distinguishable from the historic fabric of the building, with regard to the location, massing, height, proportions, architectural style, detailing and materials of the addition.
  - a. An addition that is distinguishable from the heritage building is not required to be a different style of architecture. Cladding materials, differing rooflines and wall planes and even paint colour can be used to create a distinguishable addition.
  - b. Complementary additions physically and visually conserve or enhance the cultural heritage value and heritage attributes of the District.
    - i. To be physically complementary refers to the use of materials and construction methods that do not detract from or damage heritage attributes.
    - ii. To be visually complementary refers to the selection of materials and design, massing, proportions and details so as to conserve and enhance the District's cultural heritage value.
- (2) Additions to a heritage building shall minimize the loss of historic fabric.
  - a. Whenever possible, new additions should be reversible so that the form and integrity of the heritage building is maintained and not structurally compromised should the addition be removed later.

#### **5.3.3.2 Location**

- (1) Additions to a heritage building on a contributing property shall be located towards the rear of, or sufficiently set back from, the heritage building. The location for a new addition shall ensure that the character of the heritage building, Character Area and District is maintained.
- (2) Additions should be placed so they are considered to be subordinate to the heritage building on the contributing property in size, scale and detailing. Also refer to Section 5.3.1.5 Spaces Around Buildings, which provides additional guidance on the location of additions within the context of the streetscape.
- (3) Maintain prominent views of heritage buildings on contributing properties from the public realm. Do not block or obscure prominent views of heritage buildings from the public realm with new additions or their associated features.
  - a. Design new additions so that any significant landscape features of the existing property are maintained including mature trees and site topography.
  - b. Place new additions so they are parallel with the street, in concert with the heritage building, Character Area and District.
  - c. Buildings on corner sites are required to address all facing streets.

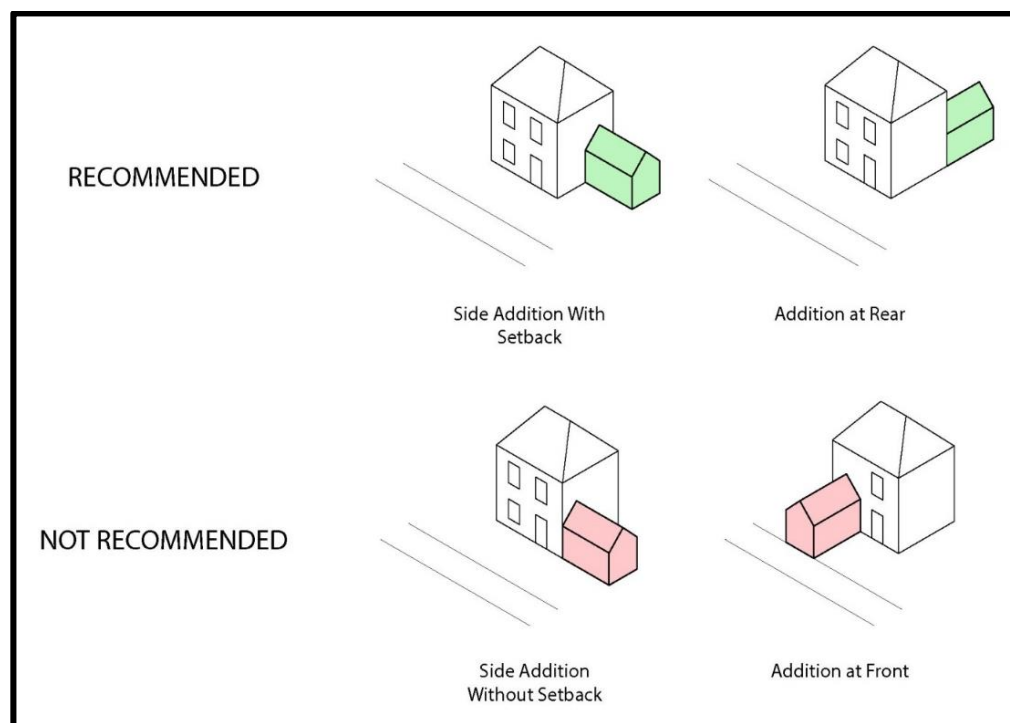


Figure 54: Suggestions on how to locate additions to contributing properties

### 5.3.3.3 Scale and Massing

- (1) The design of additions shall be subordinate to the heritage building; that is, the addition must not devalue or distract from the heritage building.
  - a. Rooflines should complement the form, materials and style of the heritage building, Character Area and District.
  - b. The proportions, placement, style and materials of windows and doors of additions should complement the heritage building, Character Area and District.
- (2) Massing and proportions of additions shall be simple and subordinate to the heritage building. Additions shall be sympathetic in scale and massing to the heritage building, Character Area and the District.
- (3) Additions shall not negatively impact the proportional symmetry of the heritage building.
- (4) Porches, verandahs and porticos are included in massing considerations for heritage buildings and should be designed so that they complement the proportions, placement, style and materials of the heritage building, Character Area and District.



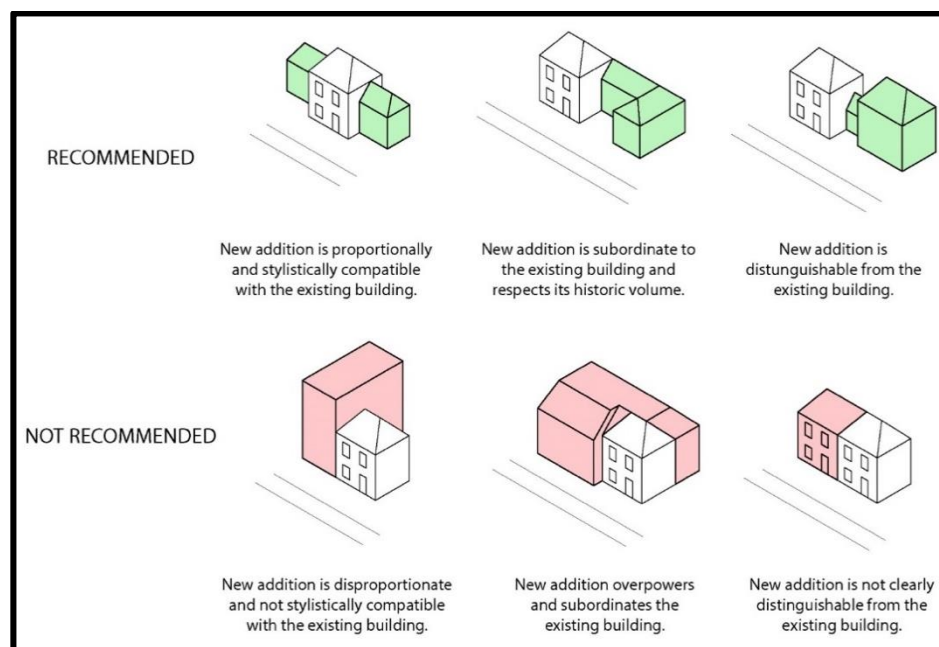


Figure 55: Guidelines for the scale and massing of additions to contributing properties

### 5.3.3.4 Height

- (1) Protect and maintain the historic low-rise scale of the District.
- (2) The overall scale, massing, design and height of the addition should be complementary to the heritage building and clearly secondary in terms of scale, massing and height.
- (3) Varying grades and elevation changes in the land shall be taken into account for additions. See the appropriate Character Area section in this Plan for additional guidance. The addition should not negatively impact the heritage building, Character Area or District.
- (4) An addition to a heritage building should be lower than the height of the heritage building on the property to clearly distinguish it from the original building. Additional height may be considered under exceptional circumstances on a case-by-case basis where there are demonstrated site constraints and an appropriate design has been proposed that does not negatively impact the heritage building, Character Area or District.

### 5.3.3.5 Style

- (1) The design of an addition should reflect or reference the architecture of the heritage building, in terms of roof form, massing, materials, windows and entrances.
  - a. Design additions so they are distinguishable from the historic portion of the heritage building.
  - b. An addition that is distinguishable from the heritage building is not required to be a different style of architecture. Cladding materials, differing rooflines and wall planes and even paint colour can be used to create a distinguishable addition.

### **5.3.3.6 Roofs**

- (1) When considering the location of an addition, consideration shall be given to the conservation of historic roof forms and features, including eaves, eavestroughs and downspouts, soffits, finials, decorative trim.
- (2) The roof form of an addition shall be physically and visually compatible with the heritage building, Character Area and District.
  - a. Gable roofs and hipped roofs are the most appropriate roof forms for the District.
  - b. Gambrel roofs may be acceptable on a case-by-case basis.
  - c. Mansard roofs are not appropriate for the District.
  - d. While not appropriate for entire structures or large additions, flat roofs and shed roofs may be permitted for small additions when compatible with the form and architectural style of the heritage building, Character Area and District.
- (3) Roofing materials used on additions shall be physically and visually compatible with the roofing materials of the main roof of the heritage building.
  - a. Asphalt shingles and cedar shingles are appropriate roofing materials for additions to heritage buildings on contributing properties. The use of alternative materials that mimic the appearance of asphalt or cedar may be considered on a case-by-case basis.
  - b. Metal roofing materials shall not be permitted for full roof systems and may only be used for accents where compatible with the architecture of the heritage building, Character Area and District.
  - c. Membranes may be considered on flat roofs.
  - d. Roofing materials such as PVC, terracotta and ceramic that do not traditionally exist in the District shall not be permitted.
  - e. Aluminum or PVC soffits and fascia shall not be permitted.
  - f. Flashing should be coloured to match the wall against which it is located.
- (4) Rooftop equipment and drainage elements shall be discreetly integrated and shall not negatively impact the character of the heritage building, Character Area and the District.
  - a. New skylights, green roofs, roof ventilation equipment, plumbing vents, solar cells and other stacks may be permitted, providing they are located on side or rear elevations that are not visible from the public realm.
  - b. New eavestroughs and downspouts shall be appropriately designed to manage water properly and direct drainage away from building foundations.

### **5.3.3.7 Chimneys**

- (1) Historic chimneys should not be removed to accommodate additions to heritage buildings.
- (2) Chimneys on additions may be permitted, providing they are complementary to the design and architectural style of any existing historic chimneys, the heritage building, Character Area and District.

### **5.3.3.8 Dormers**

- (1) Historic dormers that are integral to the character of the heritage building shall not be removed to accommodate additions.
- (2) New dormers may be permitted on additions to heritage buildings where they are compatible with the architecture of the heritage building and the addition.
  - a. New dormers shall complement the design and scale of the roof, windows, and any existing dormers on the heritage building.
  - b. New dormer windows should be square or vertically proportioned and should have the same proportions or be slightly smaller than the uppermost windows in the elevation below.

### **5.3.3.9 Windows**

- (1) Protect and maintain the location, orientation, shape and size of historic window openings.
  - a. The removal or obstruction of historic window openings to accommodate new additions is strongly discouraged. Consideration may be given to window removal to accommodate additions that are located on side or rear elevations that are not visible from the public realm if they do not negatively impact the character of the heritage building, Character Area or District.
  - b. Maintain historic solid-to-void ratios and the historic rhythm of windows and bays.
- (2) Windows in additions shall be carefully considered for their location, alignment, proportions, materials and design to ensure their compatibility with the architecture of the heritage building, as exemplified by the historic rhythm of windows and bays.
  - a. Where traditionally operable window styles are used in new window openings, new windows shall also be operable.
  - b. Horizontal banding of windows shall not be permitted on public facing elevations but may be considered on a case-by-case basis on side or rear elevations that are not visible from the public realm.
  - c. Blank and windowless walls are discouraged but may be considered on a case-by-case basis where the wall is not visible from the public realm.
  - d. The appropriate solid-to-void ratios and rhythm of windows and bays (glazing) shall be between 15-30% as shown in Figure 15 on all public facing elevations.
  - e. Windows in new additions should be wood. Aluminum-clad wood may be considered on a case-by-case basis, if they effectively replicate wood windows.
  - f. Vinyl and fiberglass windows shall not be permitted. They are not compatible with the character of heritage buildings, Character Areas and the District, and do not adequately replicate wood windows in their detailing, finishes, profiles and colour as their sashes, frames and mouldings are often much flatter in appearance.
  - g. Glue-on or snap-on muntins are not permitted. The use of true divided lights or full profile simulated divided lights with spacer bar are required when the proposed window has muntins. Simulated divided lights should be integral to the window sash. Aluminum muntins may be used where stronger muntins are required to support sealed window units while maintaining the original thinner muntin profiles. These muntins shall be made integral to the sash frame.
  - h. Window trim shall be wood, with stone or brick sills and voussoirs permitted on a case-by-case basis, depending on compatibility with the architecture of the heritage building.

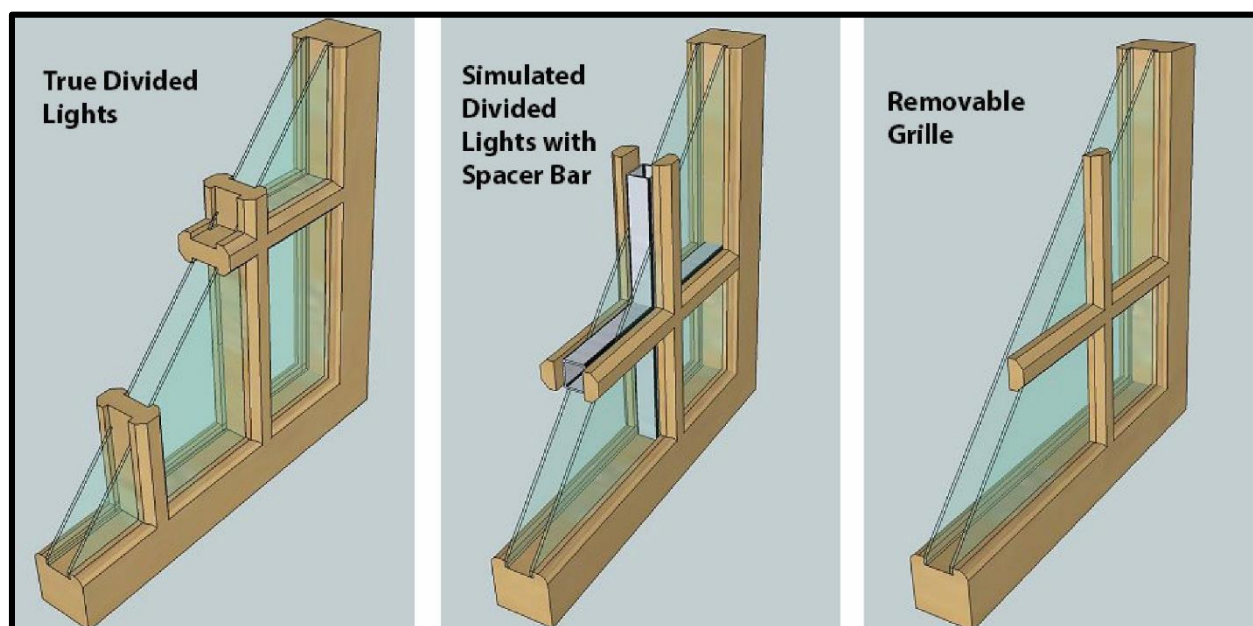


Figure 56: Examples of Muntin Bars – Removable grilles are not permitted.

#### 5.3.3.10 Shutters

- (1) Historic shutters should not be removed to accommodate additions to heritage buildings.
- (2) Shutters on additions may be permitted only where they are physically and visually compatible with the architecture of the addition and the heritage building.
  - a. Attach shutters to the window casing rather than the wall. Hinges and hooks should be used to ensure shutters are functional.
  - b. Design shutters in a style appropriate to the architectural style of the heritage building, including panelled or louvered styles. Louvers may be fixed or operable.
  - c. The dimensions of shutters shall be one-half the width of the sash they are covering allowing them to effectively cover the window if closed.
  - d. Wooden shutters shall be painted in a colour appropriate for the materiality and colours of the addition and the heritage building.

#### 5.3.3.11 Entrances and Doors

- (1) Historic entrances that are integral to the character of the heritage building shall not be removed to accommodate additions.
- (2) Entrances located on an addition to a heritage building shall be compatible, in terms of location, alignment, proportions, design and materials, with the architecture of the heritage building, as exemplified by the existing historic entrances and rhythm of bays.
  - a. New entrances shall be subordinate to the primary historic entrance, in terms of location and design. New entrances should be located on side or rear elevations, not visible from the public realm
  - b. Wood panelled doors are most appropriate for the District. Aluminum doors that mimic wood panelling may be considered on a case-by-case basis for additions that are not

visible from the public realm, providing they effectively replicate wood doors in their detailing, finishes and colour.

- c. Wood is the most appropriate material for screen doors. Aluminum and steel screen doors may be considered on a case-by-case basis on side or rear elevations that are not visible from the public realm.
- d. Sliding doors and other doors that do not swing shall not be permitted on elevations of additions that are visible from the public realm.
- e. Doors and door surrounds of new entrances shall be detailed in a style and materials appropriate to the architecture of the addition and heritage building.

#### **5.3.3.12 Foundations**

- (1) Additions shall be designed to ensure the impact to the heritage building foundation is as minimal as possible.
- (2) If applicable, new concrete foundations should be clad above grade with salvaged or new stone that is compatible with stone used on the heritage building.

#### **5.3.3.13 Porches, Verandahs and Porticos**

- (1) Protect and maintain historic porches, verandahs and porticos and their features, including posts, brackets, railings, steps and roofs. The removal or obstruction of historic porch, verandah and portico features to accommodate new additions should be avoided.
- (2) New porches, verandahs and porticos may be permitted on additions to heritage buildings, providing they are in a style that is appropriate for the architecture of the addition and the heritage building, and are physically and visually compatible with the heritage building, Character Area and District in terms of placement, orientation, design and materials.
  - a. Ensure new porches, verandahs and porticos on additions are subordinate to the heritage building.
  - b. When architectural elements such as columns and entablatures are used on new porches, verandahs or porticos, they should conform to classical proportions.
  - c. Materials for porches, verandahs and porticos should be wood with brick and stone used for piers or bases. The use of composite and engineered wood or wrought iron may be considered on a case-by-case basis.

#### **5.3.3.14 Garages and Coach Houses**

*For the purposes of this Plan, new garages on Contributing Properties will be addressed in this 'Additions to Contributing Properties' section. It is noted that new detached garages are always preferred to new attached garages.*

*For the purposes of this Plan, the intent of new garages (detached or attached) is primarily for residential storage (i.e. cars, household materials) with any additional living space as a secondary consideration.*

*For guidelines on new Detached Additional Residential Units on Contributing or Non-Contributing Properties, see Section 5.6.*

- (1) New detached garages are strongly preferred on contributing properties.
- (2) New attached garages may be permitted on a case-by-case basis, providing they are subordinate to and compatible with the heritage building, the context of the contributing property, Character Area and District.
- (3) New garages, whether detached or attached, shall be located and massed to be minimize their visibility from the public realm
  - a. Detached and attached garages shall be lower in profile than the heritage building and shall be complementary to the heritage building in design, materials and colour.
  - b. Landscaping treatments should be used to screen garages from the public realm.
  - c. New garages shall not block or obstruct views of the heritage building from the public realm.
  - d. New garages shall respond appropriately to changes in grade and topography to ensure they remain subordinate in the streetscape to the heritage building.
  - e. New attached garages shall be subordinate to the heritage building, located on rear or side elevations of the heritage building and shall minimize the loss of historic fabric. When located on side elevations, attached garages must be set back from the main elevation of the heritage building
  - f. New detached garages shall be located to the rear or side of the heritage building, set back from the heritage building.
  - g. Garage roofs, doors and windows shall be complementary to the architecture of the heritage building, Character Area and District.
  - h. Garage doors shall each be a single car width, with separated overhead doors for each bay.
  - i. Detached and attached garages shall have no more than two door bays.
  - j. When applying these guidelines to proposals for new garages on corner lots, special consideration may be given in recognition of their visibility from the public realm.

#### **5.3.3.15 Utility Service Equipment**

- (1) Utility and service equipment shall not negatively impact the contributing property, Character Area or the District and shall be located so as to minimize its visibility from the public realm.
  - a. Service hardware (such as utility meters, cable TV and telephone connections), commercial mechanical elements (such as dryer vents, heat reclamation vents, furnace and water heater exhausts, gas fireplace exhausts and kitchen exhausts), and ground mounted electrical and mechanical hardware (such as heat pumps, transformers and air conditioning units) shall not be located on main elevations and should not be visible from the public realm. If visible from the public realm, screen appropriately using landscaping features. (See Section 5.7- Landscape and Site Design Guidelines for Privately Owned Lands, for requirements on screening)
  - b. New aboveground infrastructure, including hydro lines, should be buried.
  - c. Solar panels may be permitted if oriented so that they do not compromise the character of the contributing property, Character Area or District and are preferred on side and rear elevations.

#### **5.3.3.16 Exterior Walls**

- (1) Protect and maintain historic exterior walls wherever possible, with the understanding that additions may require the removal of a portion of historic exterior walls. Removals should be carefully considered to minimize the impact to the historic materials and to ensure that the structural stability of the heritage building is not compromised.



- (2) The removal or obstruction of unique historic architectural features of exterior walls should be avoided.
- (3) Historic exterior walls that are proposed to be altered through removal or attachment to a new addition shall be fully investigated to determine structural stability and compatibility as part of a complete heritage permit application.
- (4) Exterior walls of an addition shall be compatible in terms of materials with the exterior form of the heritage building. Distinguishing the new addition from the heritage building can be achieved through the use of a sympathetic cladding material, different patterns and colours.
- (5) Traditional materials including wood, historically appropriate three-coat stucco and brick are appropriate cladding materials for additions to heritage buildings. Stone may be used for foundations to additions only.
- (6) Historically unpainted masonry surfaces of the heritage building shall not be painted to match new additions.
- (7) Exterior Insulation and Finish Systems (EIFS), vinyl and aluminum siding shall not be permitted on additions to heritage buildings.

### ***Examples of Additions***



Figure 57: 19 Navy Street – addition to rear and visible from side street; height of addition steps down; consistency in roof form and style of windows/doors and trim; complementary use of materials including brick and horizontal wood cladding





Figure 58: 273 William Street (left) – addition to west; height of addition matches; consistency in roof form and style of windows/doors and trim; complementary use of materials

Figure 59: 66 Dunn Street (right) – small addition to rear and visible from side yard. Height of addition matches; consistency in roof form and style of windows/doors and trim; complementary use of materials with shake cladding distinct from stucco



Figure 60: 53 Navy Street (left) – addition to rear and visible from William Street. Height of addition matches; consistency in gable roof form stucco cladding and public facing style of windows/doors and trim

Figure 61: 188 William Street (right) – addition to rear and visible from William Street. Height of addition is lower than original, combination of consistent stucco cladding and complementary horizontal wood cladding, wood windows in complementary style



Figure 62: 308 William Street (left) – addition to rear and visible from Reynolds Street. Height of addition is lower than original, consistent horizontal wood cladding and chimney style, wood French doors in complementary style

Figure 63: King Street (right) – small addition to rear and visible from side yard facing George Street. One storey “connection” to original creates impression of outbuilding; complementary gable shake roof and use of colour; distinction in cladding through use of horizontal wood and in window form

## 5.4 Guidelines for Non-Contributing Properties

### 5.4.1 General Guidelines

#### 5.4.1.1 Understanding

- (1) Alterations and additions to a non-heritage building and/or non-contributing property shall be based on a firm understanding of its Character Area and the District. Alterations and additions should be complementary to the Character Area and the District.
- (2) Complementary alterations and additions physically and visually conserve or enhance the cultural heritage value and heritage attributes of the District, even on non-contributing properties.
  - a. To be physically complementary refers to the use of materials and construction methods that do not detract from or damage the District's heritage attributes.
  - b. To be visually complementary refers to the selection of materials and design, massing, proportions and details so as to conserve and enhance the District's cultural heritage value.
- (3) Inspiration for design can be taken from the examples of 'Good Neighbours' (see Appendix E). 'Good Neighbours' are non-contributing properties that are appropriate for their Character Area and the District and have the following characteristics:
  - a. Their scale, height and massing are complementary to neighbouring properties.
  - b. Their materials and colours are appropriate for the District.
  - c. Their design inspiration has been taken from heritage buildings in the District but does not directly copy historic architecture.
- (4) Alterations and additions to a non-heritage building may be permitted when the following has been completed:
  - a. The impact of the proposed alterations and/or additions on the Character Area of the property and the District have been identified:
    - i. this means making the changes physically and visually compatible with the impacted Character Area; and,
    - ii. ensuring that the proposed changes meet the objectives set out in Section 3 of this Plan and Guidelines; and
    - iii. on corner lots, special consideration has been given for both public facing elevations and their impact on the streetscapes.
- (5) A Heritage Impact Assessment (HIA) may be required accordance with the Town of Oakville's Development Application Guidelines for Heritage Impact Assessments.

#### 5.4.1.2 Compliances

- (1) Current codes and standards pertaining to health and safety, security, accessibility and sustainability requirements shall be adhered to in a way that does not negatively impact the Character Area or the District. This includes but is in no way limited to the Ontario Building Code (OBC) and Accessibility for Ontarians with Disabilities Act (AODA).



- (2) New development should also refer to the Town of Oakville's Design Guidelines for Stable Residential Communities.

#### **5.4.1.3 Demolition**

- (1) The demolition of a non-heritage building on a non-contributing property may be permitted (in whole or part) through a heritage permit application. The application shall be accompanied by the plans for the proposed new addition or building on the property.
- (2) If permission to demolish a non-heritage building is granted through heritage permit approval, a demolition permit shall not be issued until complete plans for the replacement building have been submitted for a building permit.
  - a. Plans for replacement buildings must conform to this Plan as well as all other applicable town policies.
  - b. The physical demolition of the existing non-heritage building shall not take place until the issuance of the building permit for the new dwelling.
  - c. Securities for the construction of a new structure may be required to prevent empty lots within the District. Substantial progress shall be made in the construction of the replacement building within two years of the demolition of the previous building. If the delay is deemed to be unwarranted, securities may be forfeited, at the discretion of the Town of Oakville's Director of Planning and Development.
  - d. If construction of the replacement building is delayed, the town may require interim landscape treatment of the site. If the delay is deemed to be unwarranted, securities may be forfeited, at the discretion of the Town of Oakville's Director of Planning and Development.

#### **5.4.1.4 Lot Severances and Assembly**

- (1) Consent applications will be evaluated on a case-by-case basis for lot severances and assembly in the District.
- (2) All severances must conform to provincial and local policies and by-laws including, but not limited to, the in-effect Official Plan and Zoning By-law.
- (3) The lot size of any infill property or severance shall reflect the character of the surrounding lot fabric and the impacted Character Area.
- (4) Protect the traditional quarter acre lot settlement pattern with appropriate lot sizes, front yard setbacks, openness at intersections, and vistas between properties.
- (5) Applications for new development and additions bridging assembled lots will be considered on a case-by-case basis, according to the following criteria:
  - a. Traditional distances between buildings and the picturesque rhythm of buildings within streetscapes shall be maintained;
  - b. Long street elevations that would break the historic streetscape rhythm and negatively impact the Character Area shall not be permitted;
  - c. Any adjacent heritage building shall remain clearly distinct and identifiable; and
  - d. Any proposed linking structure shall be subordinate to the heritage building.

#### **5.4.1.5 Spaces Around Buildings**

- (1) Maintain traditional distances between buildings visible from the public realm and the picturesque rhythm of buildings with streetscapes.
- (2) Maintain traditional views and orientation of adjacent heritage buildings from the public realm.
- (3) Special consideration may apply to buildings of atypical orientation, on lakefront lots and/or on corner lots.

## **5.4.2 Alterations and Additions to Non-Contributing Properties**

### **5.4.2.1 Impact on Character Area and District**

- (1) Alterations and additions to a non-heritage building on a non-contributing property may be permitted where they are physically and visually compatible with the impacted Character Area and the District.

### **5.4.2.2 Scale, Massing and Location**

- (1) The scale, massing and location of an addition to a non-heritage building shall be physically and visually compatible with the impacted Character Area and the District.
- (2) Maintain prominent views of surrounding heritage buildings from the public realm. Do not block or obscure prominent views of adjacent heritage buildings from the public realm with additions to non-heritage buildings, landscaping, fencing, servicing or utility equipment.
- (3) Design new additions and alterations to non-heritage buildings so that any significant landscape features of the existing property are maintained, including mature trees and site topography. Use creative design solutions to integrate new buildings into the existing setting.
- (4) Locate new additions on non-heritage buildings parallel with the street, in concert with existing buildings in the Character Area and District.
- (5) If applicable, alterations and additions to non-heritage buildings on corner sites must address all public facing elevations.

### **5.4.2.3 Height**

- (1) Protect and maintain the historic low-rise scale of the District.
- (2) The overall scale, massing, design and height of the addition should be complementary to the existing building and clearly secondary in terms of scale, massing and height.
- (3) Varying grades and elevation changes in the land shall be taken into account for additions. See the appropriate Character Area section in this Plan for additional guidance. The addition should not negatively impact the existing building, Character Area or District.
- (4) An addition to a non-heritage building should be lower than the height of the non-heritage building on the property. Additional height may be considered under exceptional circumstances on a case-by-case basis where there are demonstrated site constraints and an appropriate design has been proposed that does not negatively impact the non-heritage building, Character Area or District.

### **5.4.2.4 Style**

- (1) Alterations to non-heritage buildings and new additions to non-heritage buildings should be compatible with the existing building, the Character Area and District.
- (2) Introduction of historic architectural details that have no context with the existing building should be avoided.

#### **5.4.2.5 Roofs**

- (1) The roof form of a non-heritage building or its additions shall be physically and visually compatible with that of the existing building, Character Area and District.
  - a. Gable roofs and hipped roofs are the most appropriate roof forms for the District.
  - b. Gambrel roofs may be acceptable on a case-by-case basis.
  - c. Mansard roofs are not appropriate for the District.
  - d. While not appropriate for entire structures or large additions, flat roofs and shed roofs may be permitted for small wings when compatible with the existing building, Character Area and District.
- (2) Roofing materials used on non-heritage buildings and their additions shall be physically and visually compatible with the roofing materials of the main roof of the existing building, the Character Area and District.
  - a. Asphalt shingles, cedar shingles are appropriate roofing materials for additions to non-heritage buildings. The use of alternative materials that mimic the appearance of asphalt or cedar may be considered on a case-by-case basis.
  - b. Metal roofing materials shall not be permitted for full roof systems and may only be used for accents where compatible with the architecture of the existing building, Character Area and District.
  - c. Membranes may be considered on flat roofs.
  - d. Roofing materials such as PVC, terracotta and ceramic that do not traditionally exist in the District shall not be permitted.
  - e. Aluminum or PVC soffits and fascia shall not be permitted.
  - f. Flashing should be coloured to match the wall against which it is located.
- (3) Rooftop equipment and drainage elements shall be discreetly integrated and shall not negatively impact the Character Area and District.
  - a. New skylights, green roofs, roof ventilation equipment, plumbing vents, solar cells and other stacks may be permitted, providing they are located on side or rear elevations that are not visible from the public realm.
  - b. New eavestroughs and downspouts shall be appropriately designed to manage water properly and direct drainage away from building foundations.



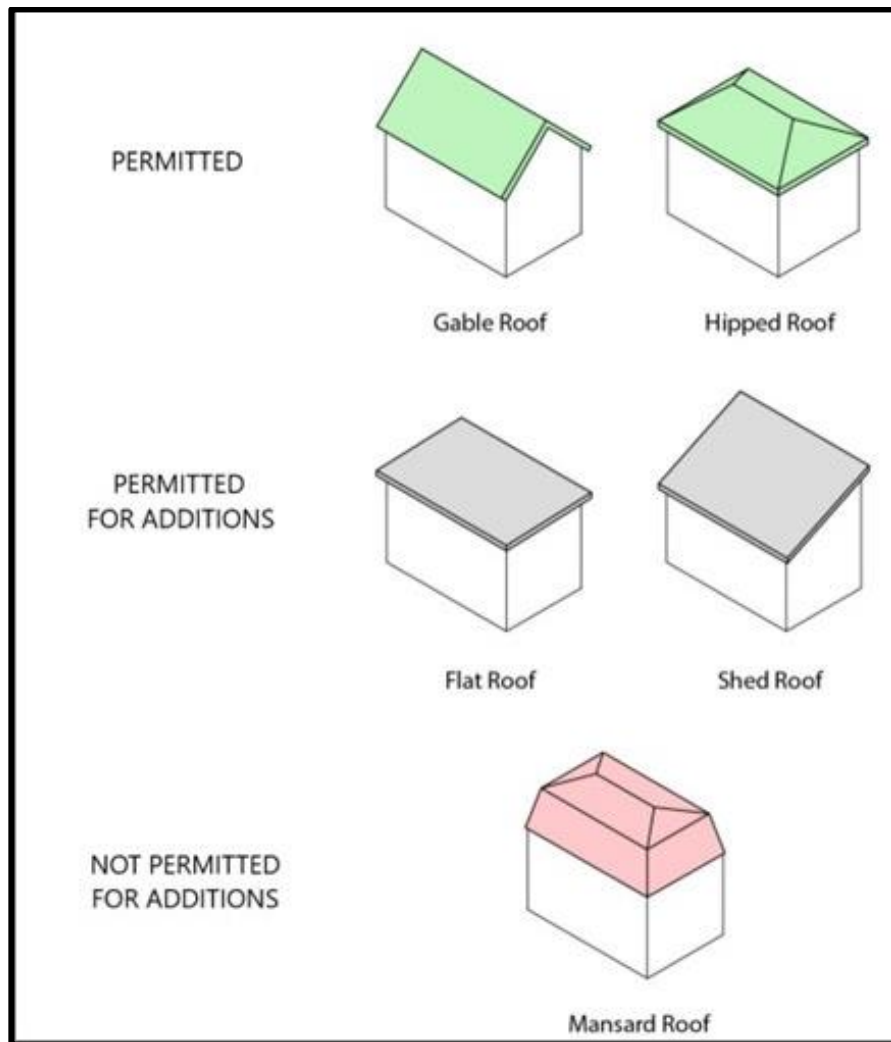


Figure 64: Examples of roof forms

#### **5.4.2.6 Chimneys**

- (1) Chimneys on non-heritage buildings and their additions may be permitted, providing they are complementary in scale, massing and materials to the existing building, Character Area and District.

#### **5.4.2.7 Dormers**

- (1) New dormers may be permitted on non-heritage buildings and their additions when they are compatible with the architecture of the existing building and the impacted Character Area.
  - a. Scale new dormers to complement the design and scale of the roof, windows, and any existing dormers on the existing building.

#### **5.4.2.8 Windows**

- (1) Windows in non-heritage buildings and their additions shall be carefully considered for their location, alignment, proportions, materials and design to ensure their compatibility with the architecture of the existing building, Character Area and District.
  - a. Where traditionally operable window styles are used in new window openings, new windows shall also be operable.
  - b. Horizontal banding of windows shall not be permitted on public facing elevations but may be considered on a case-by-case basis on side or rear elevations that are not visible from the public realm.
  - c. Blank and windowless walls are discouraged but may be considered on a case-by-case basis where the wall is not visible from the public realm.
  - d. The appropriate solid-to-void ratios and rhythm of windows and bays (glazing) shall be between 15-30% as shown in Figure 15 on all public facing elevations.
  - e. Windows in new additions to non-heritage buildings should match the existing windows in the building. Aluminum-clad wood may be considered on a case-by-case basis.
  - f. Vinyl and fiberglass windows are not permitted unless the existing windows in the non-heritage building are vinyl or fiberglass.
  - g. Glue-on or snap-on muntins are not permitted. Simulated divided lights should be integral to the window sash.
  - h. Window trim should be wood or aluminum clad-wood, with stone or brick sills and voussoirs permitted on a case-by-case basis, depending on compatibility with the architecture of the existing building.

#### **5.4.2.9 Shutters**

- (1) Shutters may be permitted on non-heritage buildings and their additions when they are physically and visually compatible with the architecture of the existing building, the Character Area and District.
  - a. Attach shutters to the window casing rather than the wall. Hinges and hooks should be used to ensure shutters are functional.
  - b. The dimensions of shutters shall be one-half the width of the sash they are covering allowing them to effectively cover the window if closed.

#### **5.4.2.10 Entrances and Doors**

- (1) Entrances located on a non-heritage building or its addition should be compatible with the existing building in terms of general arrangement, proportions, design and material, and not negatively impact the Character Area and District.
  - a. Principal entrances for non-heritage buildings shall be oriented towards the public realm.
  - b. Wood panelled doors are most appropriate for the District. Aluminum doors that mimic wood panelling may be considered on a case-by-case basis for additions that are not visible from the public realm, providing they effectively replicate wood doors in their detailing, finishes and colour.
  - c. Wood is the most appropriate material for screen doors. Aluminum and steel screen doors may be considered on a case-by-case basis on side or rear elevations that are not visible from the public realm.
  - d. Sliding doors and other doors that do not swing shall not be permitted on elevations of non-heritage buildings or their additions that are visible from the public realm.
  - e. Doors and door surrounds of new entrances shall be detailed in a style and materials appropriate to the architecture of the existing building.

#### **5.4.2.11 Foundations**

- (1) Non-heritage buildings and their additions should have unobtrusive foundations that do not detract from the Character Area and District.
- (2) New foundations on non-heritage buildings and their additions should not use cuts and types of stone or materials not in keeping with the Character Area and District.

#### **5.4.2.12 Porches, Verandahs and Porticos**

- (1) New porches, verandahs and porticos may be permitted on non-heritage buildings and their additions providing they are in a style that is appropriate for the architecture of the existing building, Character Area and District.
  - a. When architectural elements such as columns and entablatures are used on new porches, verandahs or porticos, they should conform to classical proportions.
  - b. Materials for porches, verandahs and porticos should be wood with brick and stone used for piers or bases. The use of composite and engineered wood or wrought iron may be considered on a case-by-case basis.

#### **5.4.2.13 Garages and Coach Houses**

*For the purposes of this Plan, new garages on Non-Contributing Properties will be addressed in the 'Alterations and Additions to Non-Contributing Properties' section. It is noted that new detached garages are always preferred to new attached garages.*

*For the purposes of this Plan, the intent of new garages (detached or attached) is primarily for residential storage (i.e. cars, household materials) with any additional living space as a secondary consideration.*

*For guidelines on new Detached Additional Residential Units on Contributing or Non-Contributing Properties, see Section 5.6.*

- (1) New detached garages are strongly preferred in the District.
- (2) New attached garages may be permitted on a case-by-case basis, providing they are subordinate to and compatible with the existing building, Character Area and District.
- (3) New garages, whether detached or attached, shall be located and massed to minimize their visibility from the public realm
  - a. Detached and attached garages shall be lower in profile than the existing building, and shall be complementary to the existing building, Character Area and District.
  - b. Landscaping treatments should be used to screen garages from the public realm.
  - c. New garages on non-contributing properties shall not block or obstruct views of surrounding heritage buildings from the public realm.
  - d. New garages shall respond appropriately to changes in grade and topography to ensure they remain subordinate in the streetscape to surrounding heritage buildings.
  - e. New attached garages shall be subordinate to the existing building and located on rear or side elevations of the existing building. When located on side elevations, attached garages must be set back from the main elevation of the existing building
  - f. New detached garages shall be located to the rear or side of the existing building.
  - g. Garage roofs, doors and windows shall be complementary to the architecture of the existing building, Character Area and District.
  - h. Garage doors shall each be a single car width, with separated overhead doors for each bay.
  - i. Detached and attached garages shall have no more than two door bays.
  - j. When applying these guidelines to proposals for new garages on corner lots, special consideration may be given in recognition of their visibility from the public realm.

#### **5.4.2.14 Utility Service Equipment**

- (1) Utility and service equipment shall not negatively impact the Character Area or the District and shall be located so as to minimize its visibility from the public realm.
  - a. Service hardware (such as utility meters, cable TV and telephone connections), commercial mechanical elements (such as dryer vents, heat reclamation vents, furnace and water heater exhausts, gas fireplace exhausts and kitchen exhausts), and ground mounted electrical and mechanical hardware (such as heat pumps, transformers and air conditioning units) shall not be located on main elevations and should not be visible from the public realm. If visible from the public realm, screen appropriately using landscaping features. (See Section 5.7 – Landscape and Site Design Guidelines for Privately Owned Lands, for requirements on screening)
  - b. New aboveground infrastructure, including hydro lines, should be buried.
  - c. Solar panels may be permitted if oriented so that they do not compromise the Character Area or District and are preferred on side and rear elevations.

**5.4.2.15 Exterior Walls**

- (1) Exterior walls of a non-heritage building and/or its addition shall be compatible in terms of materials with the exterior form of the existing building, Character Area and District.
- (2) Traditional materials including wood and brick are appropriate cladding materials for non-heritage buildings and/or their additions. Stone may be used as a foundation material only on a case-by-case basis and shall be a type and cut appropriate for the Character Area and District.
- (3) Exterior Insulation and Finish Systems (EIFS) and vinyl and aluminum siding shall not be permitted on non-heritages buildings or their additions.
- (4) Composite materials, such as wood siding or shingles may be considered on a case-by-case basis.

## 5.5 New Development – Primary Structures

***For the purposed of this Plan, this section shall apply to the development of new primary structures on contributing or non-contributing properties. Primary structures are the principal building, usually a residence, on a lot.***

***For guidelines on Garages and Coach Houses, refer to Sections 5.3.4.14 or 5.4.2.13.***

***For guidelines on new Detached Additional Residential Units, refer to Section 5.6.***

***For guidelines on new Outdoor Use Associated Structures, refer to Section 5.7.4.***

### 5.5.1 General Guidelines

#### 5.5.1.1 Good Neighbours

- (1) New development shall be based on a firm understanding of the impacted Character Area and District. New development should be complementary to the Character Area and the District.
- (2) Complementary alterations and additions physically and visually conserve or enhance the cultural heritage value and heritage attributes of the District, on both contributing and non-contributing properties.
  - c. To be physically complementary refers to the use of materials and construction methods that do not detract from or damage the District's heritage attributes.
  - d. To be visually complementary refers to the selection of materials and design, massing, proportions and details so as to conserve and enhance the District's cultural heritage value.
- (3) Since the inception of the District in 1981 there have been a number of new buildings constructed that are sympathetic to the character of the District and compatible with the neighbourhood. Inspiration for design can be taken from the examples of 'Good Neighbours' (see Appendix E). 'Good Neighbours' are non-contributing properties that are appropriate for their Character Area and the District and have the following characteristics:
  - a. Their scale, height and massing are complementary to neighbouring properties
  - b. Their materials and colours are appropriate for the District
  - c. Their design inspiration has been taken from heritage buildings in the District but does not directly copy historic architecture.
  - d. The location of the new development within the lot has conserved historic views.
  - e. The property has landscaping and fencing that is appropriate for the District.

#### 5.5.1.2 Impact on Heritage Character of the District

- (1) New development of a primary structure may be permitted where it is physically and visually compatible with the impacted Character Area and the District.
- (2) New primary structures shall contribute to the sense of place of the District and shall not negatively impact or detract from the heritage character of surrounding contributing properties within the impacted Character Area and the District.

### 5.5.1.3 Location, Scale and Massing

- (1) The scale and massing of new primary structures shall be physically and visually compatible with the impacted Character Area and the District.
- (2) Locate new primary structures so that prominent views of surrounding heritage buildings from the public realm are maintained. Do not block or obscure prominent views of surrounding heritage buildings from the public realm with landscaping, fencing, servicing or utility equipment.
- (3) Design new primary structures so that any significant landscape features of the existing property are maintained, including mature trees and site topography. Use creative design solutions to seamlessly integrate new development into the existing setting.
- (4) Place new primary structures parallel with the street, in concert with existing buildings in the Character Area and District.
- (5) New primary structures on corner sites must address all public facing streets.

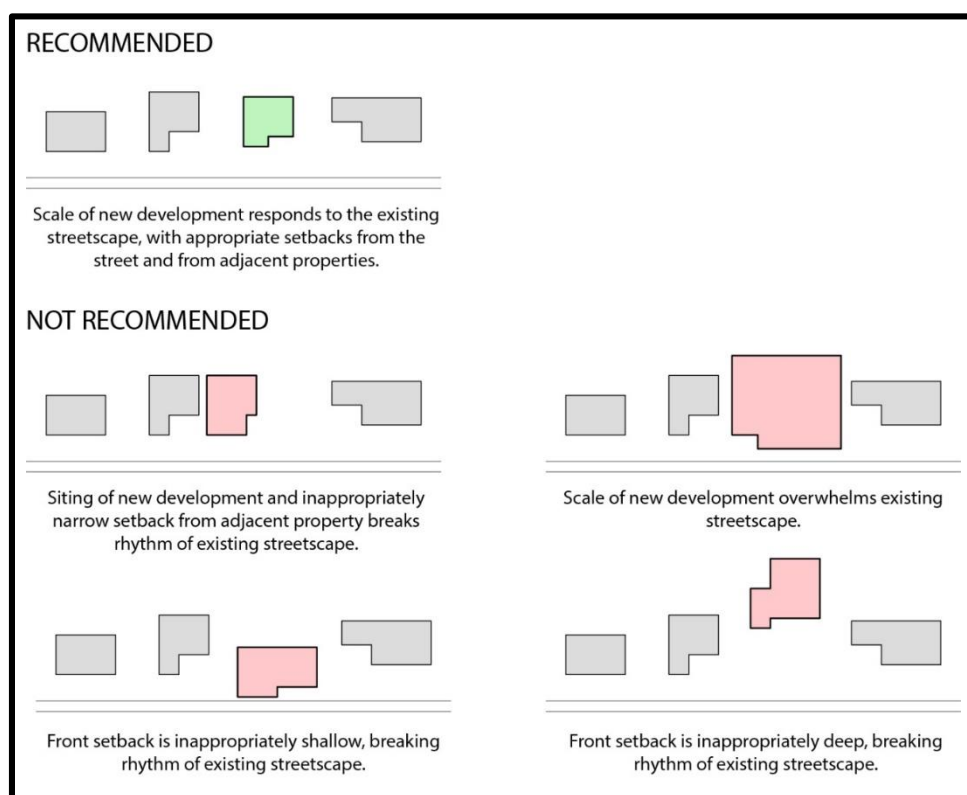


Figure 65: Guidelines for siting new development within the existing streetscape

### 5.5.1.4 Height

- (1) Protect and maintain the historic low-rise scale of the District.
- (2) New primary structures shall be compatible and complementary to surrounding properties, the Character Area and the District.



- (3) Varying grades and elevation changes in the land shall be taken into account for new primary structures. See the appropriate Character Area section in this Plan for additional guidance. The new primary structure should not negatively impact neighbouring buildings, Character Area or District.

#### **5.5.1.5 Style**

- (1) The architecture of new primary structures should be compatible with adjacent heritage buildings within the impacted Character Area and the District.
- (2) The architectural style of new buildings should be respectful of the local vernacular of the District and be compatible with neighbouring properties in the Character Area.

#### **5.5.1.6 Roofs**

- (1) The roof form of a new primary structure shall be physically and visually compatible with the Character Area and District.
  - a. Gable roofs and hipped roofs are the most appropriate roof forms for the District.
  - b. Gambrel roofs may be acceptable on a case-by-case basis.
  - c. Mansard roofs are not appropriate for the District.
  - d. While not appropriate for the entire roof system of new primary residences, flat roofs and shed roofs may be permitted for small portions of the building where compatible with the Character Area and District.
  - e. Flat roof sections that are part of a hipped roof structure cannot account for more than 15% of the roof of the primary structure.
- (2) Roofing materials used on new primary structures shall be physically and visually compatible with the roofing materials of the Character Area and District.
  - a. Asphalt shingles and cedar shingles are appropriate roofing materials for new primary structures. The use of alternative materials that mimic the appearance of asphalt or cedar may be considered on a case-by-case basis.
  - b. Metal roofing materials shall not be permitted for full roof systems and may only be used for accents where compatible with the architecture of the Character Area and District.
  - c. Membranes may be considered on flat roofs.
  - d. Roofing materials such as PVC, terracotta and ceramic that do not traditionally exist in the District shall not be permitted.
  - e. Aluminum or PVC soffits and fascia shall not be permitted
  - f. Flashing should be coloured to match the wall against which it is located.
- (3) Rooftop equipment and drainage elements shall be discreetly integrated and shall not negatively impact the Character Area and District.
  - a. New skylights, green roofs, roof ventilation equipment, plumbing vents, solar cells and other stacks may be permitted, providing they are located on side or rear elevations that are not visible from the public realm.
  - b. New eavestroughs and downspouts shall be appropriately designed to manage water properly and direct drainage away from building foundations.

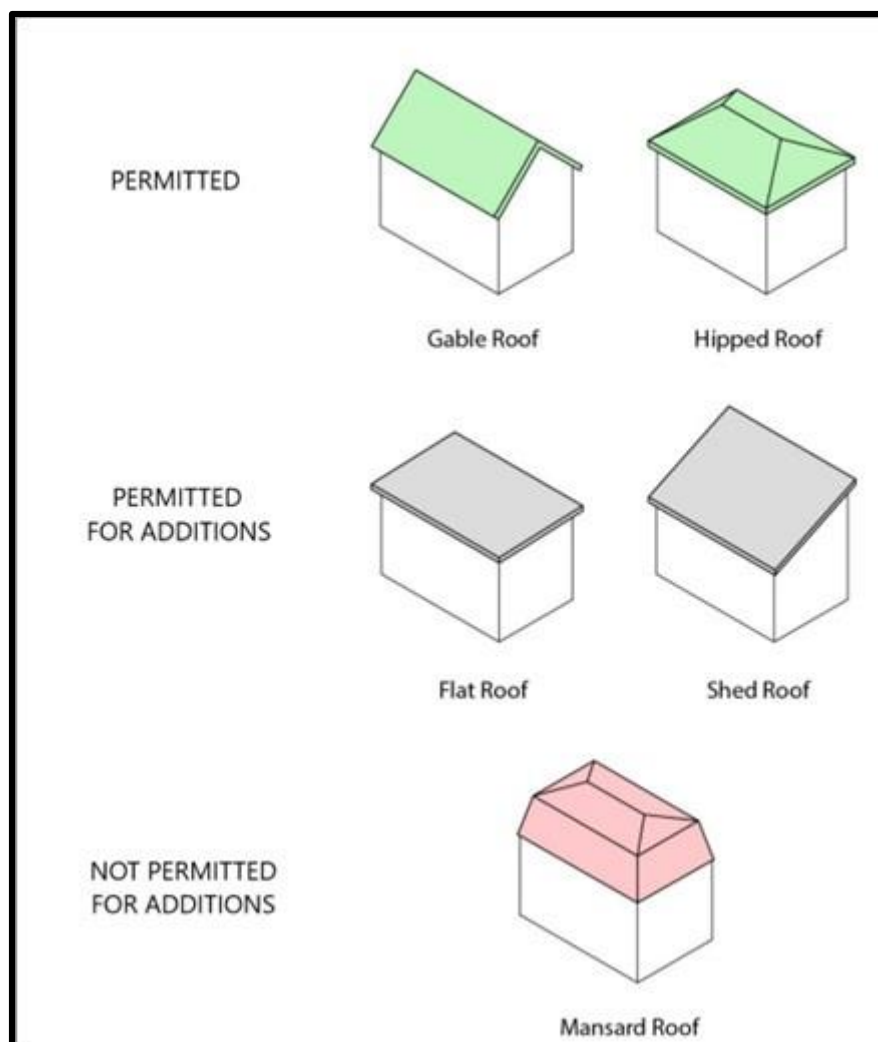


Figure 66: Guidelines for roof forms and roofs for additions in the District

#### **5.5.1.7 Chimneys**

- (1) Chimneys on new primary residences may be permitted, providing they are complementary in scale, massing and materials to the Character Area and District.

#### **5.5.1.8 Dormers**

- (1) Dormers may be permitted on new primary structures when they are compatible with the Character Area and District.
  - a. Scale new dormers to complement the design and scale of the roof, windows, of the Character Area.

### **5.5.1.9 Windows**

- (1) Windows in new primary structures shall be carefully considered for their location, alignment, proportions, materials and design to ensure their compatibility with the Character Area and District.
  - a. Where traditionally operable window styles are used in new window openings, new windows shall also be operable.
  - b. Horizontal banding of windows shall not be permitted on public facing elevations but may be considered on a case-by-case basis on side or rear elevations that are not visible from the public realm.
  - c. Blank and windowless walls are discouraged but may be considered on a case-by-case basis where the wall is not visible from the public realm.
  - d. The appropriate solid-to-void ratios and rhythm of windows and bays (glazing) shall be between 15-30% as shown in Figure 15 on all public facing elevations.
  - e. Windows in new development should be appropriate for the Character Area and District. While wood is preferred, aluminum-clad wood may be considered on a case-by-case basis.
  - f. Vinyl and fiberglass windows are not permitted.
  - g. Glue-on or snap-on muntins are not permitted. Simulated divided lights should be integral to the window sash.
  - h. Window trim should be wood or aluminum clad-wood, with stone or brick sills and voussoirs permitted on a case-by-case basis.

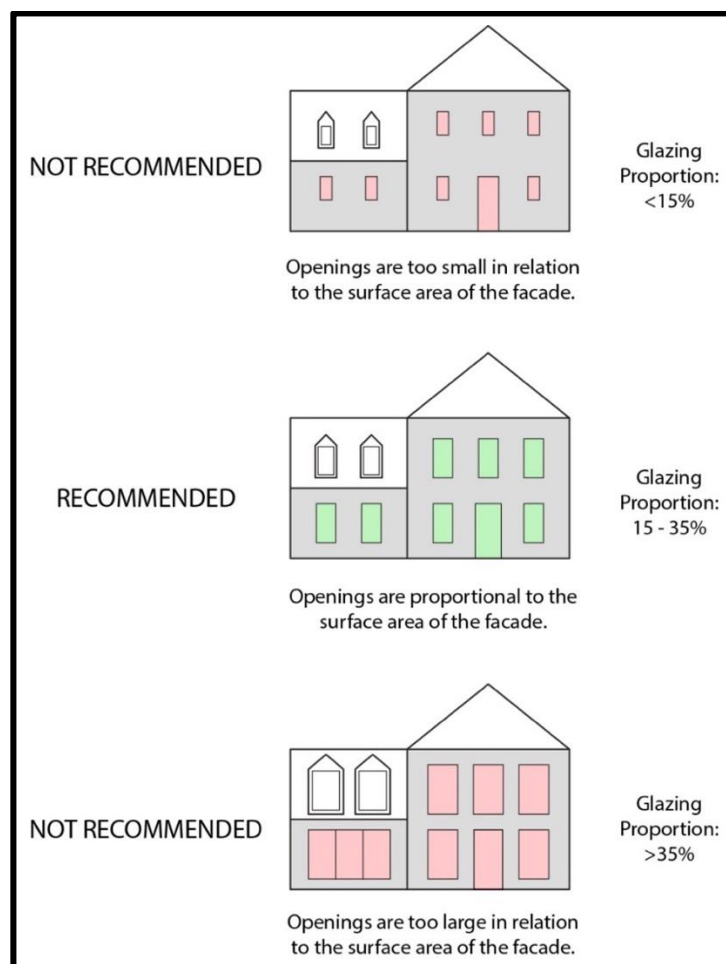


Figure 67: Guidelines for the solid-to-void ratio of new development

#### 5.5.1.10 Shutters

- (1) Shutters may be permitted on new primary structures when they are physically and visually compatible with the Character Area and District.
  - a. Attach shutters to the window casing rather than the wall. Hinges and hooks should be used to ensure shutters are functional.
  - b. The dimensions of shutters shall be one-half the width of the sash they are covering allowing them to effectively cover the window if closed.

#### 5.5.1.11 Entrances

- (1) Entrances located on new primary structures should be compatible with the Character Area and District.
  - a. Principal entrances for new primary structures shall be oriented towards the public realm.
  - b. Wood panelled doors are most appropriate for the District. Aluminum doors that mimic wood panelling may be considered on a case-by-case basis when they are not visible from the public realm, providing they effectively replicate wood doors in their detailing, finishes and colour.

- c. Wood is the most appropriate material for screen doors. Aluminum and steel screen doors may be considered on a case-by-case basis on side or rear elevations that are not visible from the public realm.
- d. Sliding doors and other doors that do not swing shall not be permitted on elevations of new primary structures that are visible from the public realm.
- e. Doors and door surrounds of new entrances shall be detailed in a style and materials appropriate to the Character Area and District.

#### **5.5.1.12 Foundations**

- (1) New primary structures should have unobtrusive foundations that do not detract from the Character Area or District.
- (2) Foundations should not use cuts and types of stone or materials that are not in keeping with the Character Area and District.

#### **5.5.1.13 Porches, Verandahs and Porticos**

- (1) New porches, verandahs and porticos may be permitted on new primary structures providing they are in a style that is appropriate for the Character Area and District.
  - a. When architectural elements such as columns and entablatures are used on new porches, verandahs or porticos, they should conform to classical proportions.
  - b. Materials for porches, verandahs and porticos should be wood with brick and stone used for piers or bases. The use of composite and engineered wood or wrought iron may be considered on a case-by-case basis.

#### **5.5.1.14 Garages and Coach Houses**

*For the purposes of this Plan, new garages on Non-Contributing Properties will be addressed in the 'New Development – Primary Structures' section. It is noted that new detached garages are always preferred to new attached garages.*

*For the purposes of this Plan, the intent of new garages (detached or attached) is primarily for residential storage (i.e. cars, household materials) with any additional living space as a secondary consideration.*

*For guidelines on new Detached Additional Residential Units on Contributing or Non-Contributing Properties, see Section 5.6.*

- (1) New detached garages are strongly preferred in the District.
- (2) New attached garages may be permitted on a case-by-case basis, providing they are subordinate to and compatible with the Character Area and District.
- (3) New garages, whether detached or attached, shall be located and massed to minimize their visibility from the public realm
  - a. Detached and attached garages shall be lower in profile than the new primary structure and shall be complementary to the Character Area and District.
  - b. Landscaping treatments should be used to screen garages from the public realm.
  - c. New garages shall not block or obstruct views of surrounding heritage buildings from the public realm.
  - d. New garages shall respond appropriately to changes in grade and topography to ensure they remain subordinate in the streetscape to surrounding heritage buildings.

- e. New attached garages shall be subordinate to the new primary structure and located on rear or side elevations of the structure. When located on side elevations, attached garages must be set back from the main elevation of the new primary structure.
- f. New detached garages shall be located to the rear or side of the new primary structure.
- g. Garage roofs, doors and windows shall be complementary to the architecture of the Character Area and District.
- h. Garage doors shall each be a single car width, with separated overhead doors for each bay.
- i. Detached and attached garages shall have no more than two door bays.
- j. When applying these guidelines to proposals for new garages on corner lots, special consideration may be given in recognition of their visibility from the public realm.

#### **5.5.1.15 Utility Service Equipment**

- (1) Utility and service equipment shall not negatively impact the Character Area or the District and shall be located so as to minimize its visibility from the public realm.
  - a. Service hardware (such as utility meters, cable TV and telephone connections), commercial mechanical elements (such as dryer vents, heat reclamation vents, furnace and water heater exhausts, gas fireplace exhausts and kitchen exhausts), and ground mounted electrical and mechanical hardware (such as heat pumps, transformers and air conditioning units) shall not be located on main elevations and should not be visible from the public realm. If visible from the public realm, screen appropriately using landscaping features. (See Section 5.7 – Landscape and Site Design Guidelines for Privately Owned Lands, for requirements on screening)
  - b. New aboveground infrastructure, including hydro lines, should be buried.
  - c. Solar panels may be permitted if oriented so that they do not compromise the Character Area or District and are preferred on side and rear elevations.

#### **5.5.1.16 Exterior Walls**

- (1) Exterior walls of new primary structures shall be compatible with the Character Area and District.
- (2) Traditional materials including wood and brick are appropriate cladding materials for new primary structures. Stone may be used as a foundation material only on a case-by-case basis and shall be a type and cut appropriate for the Character Area and District.
- (3) Exterior Insulation and Finish Systems (EIFS) and vinyl and aluminum siding shall not be permitted on new primary structures.
- (4) Composite materials, such as wood siding or shingles may be considered on a case-by-case basis.

## **5.6 New Development – Detached Additional Residential Unit**

***This section shall apply for the development of new Detached Additional Residential Units on both Contributing and Non-Contributing Properties.***

***For guidelines on Garages and Coach Houses, refer to Sections 5.3.4.14 or 5.4.2.13.***

***For guidelines on new Primary Structures, refer to Section 5.5***

***For guidelines on Outdoor Use Associated Structures, refer to Section 5.7.4.***

### **5.6.1 General Guidelines**

#### **5.6.1.1 Impact on Heritage Character of the District**

- (1) New detached additional residential units may be permitted, providing they do not negatively impact the Character Area and District, and, if applicable, the heritage building on a contributing property.
- (2) New detached additional residential units should be complementary to the Character Area and the District.
- (3) Complementary detached additional residential units physically and visually conserve or enhance the cultural heritage value and heritage attributes of the District, on both contributing and non-contributing properties.
  - e. To be physically complementary refers to the use of materials and construction methods that do not detract from or damage the District's heritage attributes.
  - f. To be visually complementary refers to the selection of materials and design, massing, proportions and details so as to conserve and enhance the District's cultural heritage value.
- (4) New detached additional residential units shall be designed to be compatible with and respect the heritage character of the Character Area and District, and if applicable, the heritage building on a contributing property, through attention to height, built form, setback, massing, material, orientation, relationship to the street, and other architectural details or physical elements.
- (5) New detached additional residential units shall contribute to the sense of place of the District and shall not negatively impact or detract from the heritage character of the Character Area and District, and if applicable, the heritage building on a contributing property.

#### **5.6.1.2 Location, Scale and Massing**

- (1) The scale, massing and location of new detached additional residential units shall be physically and visually compatible with the Character Area and District, and if applicable, the heritage building on a contributing property.
- (2) Maintain prominent views of heritage buildings (on site or adjacent to) from the public realm. Do not block or obscure prominent views of adjacent heritage buildings from the public realm with landscaping, fencing, servicing or utility equipment.
- (3) Design new detached additional residential units so that any significant landscape features of the existing property are maintained, including mature trees and site topography. Use creative design solutions to integrate new units into the existing setting.



- (4) New detached additional residential units on lots that are corner sites must address all public facing streets, if applicable.

### **5.6.1.3 Height**

- (1) Protect and maintain the historic low-rise scale of the District.
- (2) The maximum height for new detached additional residential units shall be a half storey less than the primary structure on the property. In the case of one-storey primary structures, the new detached additional residential unit shall be limited in height to match the primary structure.
- (3) New detached additional residential units shall be compatible and complementary to the primary structure on the property, surrounding properties, the Character Area and the District.
- (4) Varying grades and elevation changes in the land shall be taken into account for new detached additional residential units. See the appropriate Character Area section in this Plan for additional guidance. The new detached additional residential unit should not negatively impact the primary structure on the property, neighbouring buildings, Character Area or District.

### **5.6.1.4 Style**

- (1) The architecture of new detached additional residential units shall be compatible with the impacted Character Area, District, and if applicable, the heritage building on a contributing property.
- (2) The architecture of new detached additional residential units should be respectful of the local vernacular of the District and maintain appropriate design scale and details in respect of neighbouring properties in the Character Area, and if applicable, the heritage building on a contributing property.

### **5.6.1.5 Roofs**

- (1) The roof form of a new detached additional residential unit shall be physically and visually compatible with the Character Area, District, and if applicable, the heritage building on a contributing property.
  - a. Gable roofs and hipped roofs are the most appropriate roof forms for the District.
  - b. Gambrel roofs may be acceptable on a case-by-case basis.
  - c. Mansard roofs are not appropriate for the District.
  - d. Flat roofs and shed roofs may be permitted for small portions of the detached additional residential unit if they are no more than one storey in height on a case-by case basis where compatible with the Character Area, District, and if applicable, the heritage building on a contributing property.
  - e. Flat roof sections that are part of a hipped roof structure cannot account for more than 15% of the roof of the unit.
- (2) Roofing materials used on new detached additional residential units shall be physically and visually compatible with the roofing materials of the Character Area and District, and if applicable, the heritage building on a contributing property.

- a. Asphalt shingles, cedar shingles are appropriate roofing materials for new primary structures. The use of alternative materials that mimic the appearance of asphalt or cedar may be considered on a case-by-case basis.
  - b. Metal roofing materials shall not be permitted for full roof systems and may only be used for accents where compatible with the architecture of the Character Area and District.
  - c. Membranes may be considered on flat roofs.
  - d. Roofing materials such as PVC, terracotta and ceramic that do not traditionally exist in the District shall not be permitted.
  - e. Aluminum or PVC soffits and fascia shall not be permitted.
  - f. Flashing should be coloured to match the wall against which it is located.
- (3) Rooftop equipment and drainage elements shall be discreetly integrated and shall not negatively impact the Character Area and District, and if applicable, the heritage building on a contributing property.
- a. New skylights, green roofs, roof ventilation equipment, plumbing vents, solar cells and other stacks may be permitted, providing they are located on side or rear elevations that are not visible from the public realm.
  - b. New eavestroughs and downspouts shall be appropriately designed to manage water properly and direct drainage away from building foundations.

#### **5.6.1.6 Chimneys**

- (1) Chimneys on new primary residences may be permitted, providing they are complementary in scale, massing and materials to the Character Area and District, and if applicable, the heritage building on a contributing property.

#### **5.6.1.7 Dormers**

- (1) Dormers may be permitted on new detached additional residential units when they are compatible with the Character Area and District, and if applicable, the heritage building on a contributing property.
- a. Scale new dormers to complement the design and scale of the roof, windows, of the Character Area, District and if applicable, the heritage building on a contributing property.

#### **5.6.1.8 Windows**

- (1) Windows in new detached additional residential units shall be carefully considered for their location, alignment, proportions, materials and design to ensure their compatibility with the Character Area, District and, if applicable, the heritage building on a contributing property.
- a. Where traditionally operable window styles are used in new window openings, new windows shall also be operable.
  - b. Horizontal banding of windows shall not be permitted on public facing elevations but may be considered on a case-by-case basis on side or rear elevations that are not visible from the public realm.
  - c. Blank and windowless walls are discouraged but may be considered on a case-by-case basis where the wall is not visible from the public realm.
  - d. The appropriate solid-to-void ratios and rhythm of windows and bays (glazing) shall be between 15-30% as shown in Figure 15 on all public facing elevations.

- e. Windows in new units should be appropriate for the Character Area and District, and if applicable, the heritage building on a contributing property. While wood is preferred, aluminum-clad wood may be considered on a case-by-case basis.
- f. Vinyl and fiberglass windows are not permitted.
- g. Glue-on or snap-on muntins are not permitted. Simulated divided lights should be integral to the window sash.
- h. Window trim should be wood or aluminum clad-wood, with stone or brick sills and voussairs permitted on a case-by-case basis.

#### **5.6.1.9 Shutters**

- (1) Shutters may be permitted on new detached additional residential units when they are compatible with the Character Area, District, and if applicable, the heritage building on a contributing property.
  - a. Attach shutters to the window casing rather than the wall. Hinges and hooks should be used to ensure shutters are functional.
  - b. The dimensions of shutters shall be one-half the width of the sash they are covering allowing them to effectively cover the window if closed.

#### **5.6.1.10 Entrances**

- (1) Entrances located on new detached additional residential units should be compatible with the Character Area, District and if applicable, the heritage building on a contributing property.
  - a. Wood panelled doors are most appropriate for the District. Aluminum doors that mimic wood panelling may be considered on a case-by-case basis when they are not visible from the public realm, providing they effectively replicate wood doors in their detailing, finishes and colour.
  - b. Wood is the most appropriate material for screen doors. Aluminum and steel screen doors may be considered on a case-by-case basis on side or rear elevations that are not visible from the public realm.
  - c. Sliding doors and other doors that do not swing shall not be permitted on elevations of new primary structures that are visible from the public realm.
  - d. Doors and door surrounds of new entrances shall be detailed in a style and materials appropriate to the Character Area and District.

#### **5.6.1.11 Porches and Porticos**

- (1) New porches, verandahs and porticos may be permitted on new detached additional residential units providing they are in a style that is appropriate for the Character Area, District and, if applicable, the heritage building on a contributing property.
  - a. When architectural elements such as columns and entablatures are used on new porches, verandahs or porticos, they should conform to classical proportions.
  - b. Materials for porches, verandahs and porticos should be wood with brick and stone used for piers or bases. The use of composite and engineered wood or wrought iron may be considered on a case-by-case basis

#### **5.6.1.12 Utility Service Equipment**

- (1) Utility and service equipment shall not negatively impact the Character Area or the District and shall be located so as to minimize its visibility from the public realm.

- a. Service hardware (such as utility meters, cable TV and telephone connections), commercial mechanical elements (such as dryer vents, heat reclamation vents, furnace and water heater exhausts, gas fireplace exhausts and kitchen exhausts), and ground mounted electrical and mechanical hardware (such as heat pumps, transformers and air conditioning units) shall not be located on main elevations and should not be visible from the public realm. If visible from the public realm, screen appropriately using landscaping features. (See Section 5.7- Landscape and Site Design Guidelines for Privately Owned Lands, for requirements on screening)
- b. New aboveground infrastructure, including hydro lines, should be buried.
- c. Solar panels may be permitted if oriented so that they do not compromise the Character Area, District, or if applicable, the heritage building on a contributing property, and are preferred on side and rear elevations.

#### **5.6.1.13 Exterior Walls**

- (1) Exterior walls of new detached additional residential units shall be compatible with the Character Area, District, and if applicable, the heritage building on a contributing property.
- (2) Traditional materials including wood and brick are appropriate cladding materials for new primary structures. Stone may be used as a foundation material only on a case-by-case basis and shall be a type and cut appropriate for the Character Area, District, and if applicable, the heritage building on a contributing property.
- (3) Exterior Insulation and Finish Systems (EIFS) and vinyl and aluminum siding shall not be permitted on new detached additional residential units.
- (4) Composite materials, such as wood siding or shingles may be considered on a case-by-case basis.

## **5.7 Landscape and Site Design Guidelines for Privately Owned Lands (Contributing and Non-Contributing)**

### **5.7.1 General Guidelines**

#### **5.7.1.1 Soft Landscaped Areas and Trees**

- (1) Maximize, protect and maintain existing generous setbacks with landscaped areas, including gardens, low-profile hedges and open lawns. New landscaped areas are encouraged, particularly along sidewalks and curb frontages contributing and enhancing the continuity of the streetscape character of the impacted Character Area.
- (2) Protect and maintain the existing and future tree canopy within the District. A tree permit shall be required for the injury or removal of any tree located within the front yard of a private property, in accordance with the definitions, conditions and requirements specified in the Town of Oakville's Private Tree Protection By-Law 2008-156 (as amended).
- (3) All efforts shall be made to design additions and new development to accommodate existing mature trees, rather than remove them. The planting of new trees is strongly encouraged.
- (4) Monitor tree health on a regular basis and remove dead wood to avoid decay and prevent property damage.
- (5) The impact of alterations, additions and new development on soft landscaping should be mitigated through new landscaping plans that respect the Character Area and District.
- (6) Views as identified in Map 2 and open corners at intersections shall be conserved.

#### **5.7.1.2 Surface Treatments and Hardscaping**

- (1) Protect and maintain historic pathways within properties.
- (2) New pathways may be permitted, providing their siting and paving materials are compatible with the property frontage and the streetscape. Suitable materials for pathways include flagstone, pavers, gravel, and concrete. Proper methods of drainage must be installed with hardscaping.
- (3) New parking areas within front yards are not permitted. Where front yard parking areas already exist, they should be finished in materials that will reduce their visual impact, such as gravel, pavers, flagstone or permeable materials.
- (4) Minimize the visibility of driveways and surface parking areas from the public realm. Suitable materials for driveways include flagstone, pavers, gravel, concrete and asphalt.
- (5) Permeable surfaces are encouraged for paths, driveways and parking areas.

#### **5.7.1.3 Fencing and Walls**

- (1) Protect and maintain historic fencing and walls in situ when possible. Proposals for alterations, additions and new development that impact historic fencing, and walls should provide appropriate mitigation measures.
- (2) Refer to historic photographs or documentation for appropriate styles wherever possible when adding new fences and walls to contributing properties. On non-contributing properties or where historic documentation cannot be found, refer to the Character Area guidelines.
- (3) Appropriate fencing materials include wood, wrought iron, natural stone, brick and a combination of those listed.
- (4) Hedges, ornamental fencing, retaining walls and garden walls in front yards should be maintained with low profiles. They may be used to define the edge of the property in relation

to neighbouring properties and the public realm. These features should not block views of heritage buildings from the public realm.

- (5) New fencing shall not block the openness of intersections or negatively impact the Character Area.
- (6) New fencing shall meet all requirements of the Town of Oakville Fence By-law.

### ***Examples of Fencing and Walls***



Figure 68: 226 William Street – low lying lake stone wall across front of house with soft landscaping and open vistas to westerly side yard

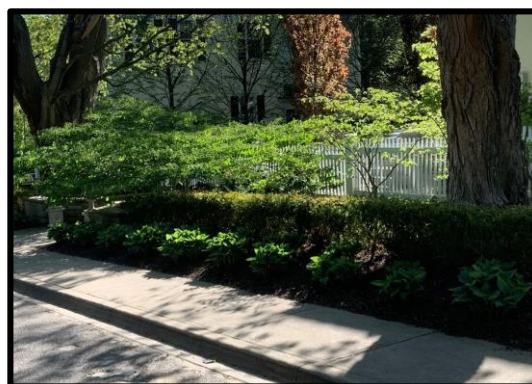


Figure 69: 32 Thomas Street (left) – combination plank with lattice fencing and soft landscaping along King Street

Figure 70: 66 Dunn Street (right) – combination of open picket fencing and low soft landscaping along north side of William Street allowing for vistas across and into property





Figure 71: 274 King Street (left) – combination plank with lattice fencing and soft landscaping enables vistas towards lake



Figure 72: 235 Front Street (right) – historic iron fencing with stone pillars and low soft landscaping



Figure 73: 308 William Street (left) – low lying picket fence running along William and Reynolds Streets supports open vistas at corner



Figure 74: 53 Navy Street (right) – low lying picket fence and soft landscaping along south side of William Street – enabling open vista at corner





Figure 75: Navy Street Erchless Estate (left) – low stone wall with stone pillars and wooden gate  
Figure 76: Navy Street (right) – Lawn Bowling Club and Market Square

#### 5.7.1.4 Outdoor Use Associated Structures (Sheds, Gazebos and Cabanas)

- (1) Small accessory buildings and structures associated with outdoor use of a property that are not residential dwellings or garages and that are under 15 square metres are encouraged to be located in side or rear yards.
- (2) These structures may be permitted in areas of a property that are visible from the public realm, providing they are physically and visually compatible with the property and do not negatively impact the Character Area and District.

#### 5.7.1.5 Screening and Buffering

- (1) For the purposes of this Plan and Guidelines, Screening occurs when ornamental fencing or evergreen material is used to block views, whereas buffering allows filtered porous views, such as partially enclosed fence (e.g. picket fencing) or a deciduous shrub border.
- (2) Plantings, ornamental fencing and low garden walls should be used to screen or buffer garbage storage areas or utility and service equipment that are otherwise visible from the public realm.



Figure 77: Continuous street edge with porous and low-lying fencing/wall treatments

**5.7.1.6 Lighting**

- (1) Lighting features may be permitted to illuminate walkways, steps, porches and entrances, providing there is no light trespass or spillover towards neighbouring properties or the public realm. Full cutoff lighting is required.
- (2) Skylighting, extensive building elevation lighting, soffit lighting and disruptive lighting shall not be permitted.
- (3) New and/or replacement lighting should meet the guidelines established in the Town of Oakville's Design Guidelines for Stable Residential Communities.

## **5.8 Landscape and Site Guidelines for Public Lands (Contributing and Non-Contributing)**

### **5.8.1 General Guidelines**

#### **5.8.1.1 Paved Areas**

- (1) Maintain streetscape features such as paved roads, sidewalks and concrete curbs. These features improve accessibility and the pedestrian environment and help to distinguish the public rights-of-way from private properties. Ensure that water drainage does not negatively impact abutting private properties.
- (2) Maintain existing sidewalks with surface treatments that complement the Character Areas and existing street furniture styles.
- (3) The Front Street right-of-way shall support safe on-road trail route and access with distinguished surface materials, patterns and/or paint.
- (4) Low impact development measures, such as permeable surfaces and water retention areas in parking lots should be considered to improve environmental sustainability along two important shorelines.
- (5) Parking areas must be designed and located so that they are as unobtrusive as possible. Tree planting and landscape areas shall be integrated into parking lot designs to soften the impact of this hardscape to the Character Area and District.
- (6) Through-traffic should continue to be discouraged using speed limits, limited street grid connectivity and other measures where necessary.
- (7) New and/or replacement sidewalks and curbs should meet the standards of the Accessibility for Ontarians with Disabilities Act (AODA).

#### **5.8.1.2 Street Trees and Boulevards**

- (1) Continue to identify, commemorate and protect heritage trees within the District.
- (2) Protect and maintain the existing tree canopies located on both sides of the street along the rights-of-way within the District, especially those that frame key views into the downtown and Lake Ontario.
- (3) Prioritize planting of new trees along sidewalks and boulevards on both sides of the street wherever technically feasible.
- (4) Reduce gaps between existing mature street tree canopies by planting companion trees where appropriate; companion trees are planted adjacent to mature trees to serve as well-established replacements in the future.
- (5) Tree health should be monitored on a regular basis and dead wood removed to avoid decay and prevent property damage. Remove and replace dead trees in accordance with town standards for tree removal and replanting to maintain the tree canopy.
- (6) Protect and maintain existing grassed boulevards. Where grassed boulevards are damaged by winter maintenance activities, they should be repaired the following spring.
- (7) All other applicable town standards for tree planting and maintenance shall be followed, including planting of historic tree species and eligible species posted on town's website, under Private Tree Protection (see Appendix F).

### **5.8.1.3 Street Furniture, Lighting and Utilities**

- (1) A variety of street lighting currently exists within the District. New and replacement lighting poles should be of a consistent design, eco-friendly, night-sky compliant and be compatible with the Character Area and the District.
- (2) Lighting fixtures' material, scale and colour should be compatible with the Character Area and District and should complement the traditional lighting fixtures in Oakville's historic downtown.
- (3) Solar and environmentally friendly lighting should be considered when there is no negative impact to the Character Area and District.
- (4) Street furniture should be unified and should complement the historic architecture of the District, as well its connection to Oakville's historic downtown.
- (5) Overhead wires should be buried, when possible, to minimize conflicts with the mature tree canopy and significant sightlines and views to the downtown, streetscapes, Lake Ontario and Sixteen Mile Creek.

### **5.8.1.4 Views**

Map 2 depicts the significant views within the District.

- (1) Protect the tree-framed views of Lake Ontario along public rights-of-way, including Navy Street, Thomas Street, George Street, Dunn Street, Trafalgar Road, Reynolds Street and Allan Street. New street trees should be located along sidewalks or boulevards to frame these views, not obscure them.
- (2) Protect the secondary vistas to the lake, creek, downtown, churches and building facades along Key Streetscape Overlay.
- (3) Protect the secondary vista openings within the streetscape at the road intersections within the District.

### **5.8.1.5 Public Parks and Open Space**

- (1) Protect and maintain parks and open spaces along the Lake Ontario and Sixteen Mile Creek shorelines. These areas should remain publicly accessible. Increased pedestrian amenities, such as connecting pathways or paving, may be permitted.
- (2) Conserve and enhance the physical connections between the parks and public landmarks within Character Area 1 - Waterfront Open Space, including Market Square, Erchless Estate, Lakeside Park, Dingle Park and George Parkette.
- (3) Protect and maintain the individually designated properties within the park system.
- (4) Park programming should not negatively impact Character Area – Waterfront Open Space.
- (5) Alterations, additions and new development within the parks and open spaces in the District shall conserve the cultural heritage value and heritage attributes identified in the cultural heritage landscape designations for the Oakville Harbour and the Erchless Estate.
- (6) Conserve and enhance the safety of the physical trail and right-of-way connections between the parks and public landmarks within Character Area 1 - Waterfront Open Space, including Market Square, Erchless Estate, Lakeside Park, Dingle Park and George Parkette.
- (7) Integrate and celebrate Indigenous history in Character Area 1 - Waterfront Open Space.
- (8) Continue to identify, commemorate and protect heritage trees in parks and open spaces.
- (9) Protect and maintain the existing trees in parks and open spaces, prioritizing successional planting of new trees and historic grove-like restoration of the shoreline.

- (10) Encourage the planting of native species, including plants, shrubs and trees.
- (11) Tree health should be monitored on a regular basis and dead wood removed to avoid decay and prevent property damage. Remove and replace dead trees in accordance with town standards for tree removal and replanting to maintain the tree canopy.
- (12) All other applicable town standards for tree planting and maintenance shall be followed, including planting of historically appropriate tree species when possible.
- (13) Fencing shall be low-lying and porous. Picket or wrought iron style fencing with frequent openings for public access to the park is most appropriate.
- (14) Protect and maintain historic fencing and walls in situ when possible. Refer to historic photographs or documentation for appropriate styles wherever possible when adding new fences in parks and open spaces.

#### **5.8.1.6 Wayfinding**

- (1) Encourage the use of Oakville Historical Society signage for key historic structures within the District.
- (2) The predominant public space signage within the District has a white background and is mounted with single or double posts. Signage should be unified and well-maintained at public space entrances.
- (3) Special street signage identifying the heritage conservation district may be considered in the future.





# 6 | EXEMPT ALTERATIONS AND CLASSES OF ALTERATIONS

## 6.1 Exempt Heritage Permit Work

The exempted alterations have been guided by the principles of either being undertaken within a small area, confined to areas that are out of sight from public view, constitute routine maintenance or are easily reversible. They are consistent with the exemptions that have been set by Town Council for the other HCDs in Oakville.

### 6.1.1 Private Property

The following alterations may be carried out without obtaining a permit under Section 42 of the OHA, providing they meet the guidelines of this Plan:

- (1) **Interior modifications:** The interiors of buildings or structures are not subject to regulation within the District.

Exceptions

Interior features that are designated under Part IV of the OHA or interior features that have an exterior presence, including but not restricted to windows and doors in building façades.

- (2) **Roof materials:** Replacement of existing roof materials in kind and of the same colour.

Exceptions

Replacement of existing roof materials with different materials may require a permit.

- (3) **Skylights:** The installation of skylights located out of view from the public realm and in the same plane as the roof (e.g. on the rear slope of a roof or on a flat or low pitched roof).

Exceptions

Skylights that are visible from the public realm may require a permit.

- (4) **Solar panels:** The installation of solar panels located out of sight from the public realm and in the same plane as the roof (e.g., at the rear slope of a roof or on a flat or low pitched roof).

Exceptions

Solar panels that are freestanding on poles, panels requiring a structural frame for support that are visible from the public realm may require a permit.

- (5) **Satellite dishes:** The installation of satellite dishes that are located out of sight from the public realm. All efforts will be made to locate these in no or low visibility areas.

- (6) **Security lighting and alarm systems:** The installation of security lighting and/or alarm systems.

- (7) **Amenity lighting:** The installation of porch lighting or other amenity or seasonal lighting.



- (8) **Eavestrough and downspouts:** The removal and/or installation of new eavestroughs and downspouts in the same material and colour as existing.
- (9) **Landscaping, soft:** The removal and/or installation of vegetative landscaping, such as planting beds, shrubbery and small ornamental trees, as well as the pruning and maintenance of trees or the removal of dead branches or limbs.

Exceptions

Removal of trees which are at least 4.5m and/or greater than 15 centimetres diameter at breast height (dbh) may require a heritage permit and consultation with town staff is required and a permit may be required. The Private Tree By-law (as amended) may also apply.

- (10) **Landscaping, hard:** The removal and/or installation of hard landscaping, such as driveways, entranceways, paths and parking areas in the same materials, in the same location and of the same area and dimension, as well as the removal and/or installation of any hard landscaping located out of view from the public realm.

Exceptions

New hardscaping with different materials visible from the public realm requires a permit.

Any work that requires an excavation onto public property (i.e. between road right of ways and private lots) may require a permit from other town departments.

- (11) **Fencing:** The removal and/or installation of fencing located out of sight from the public realm. The Fence By-law may apply even if a permit is not required under this Plan.

Exceptions

The installation of fencing that is visible from the public realm may require a permit.

- (12) **Decks:** The installation and/or removal of decks that are at grade or less than 60 centimetres off the ground and are located within the rear yard and out of view from the public realm.

- (13) **Storm windows and doors:** The seasonal installation and/or removal of storm windows and screen doors in the same materials and locations.

Exceptions

New storm windows and new screen doors require a permit.

- (14) **Signage:** The installation of non-illuminated number signage on building façades.

Exceptions

Illuminated number signage or other signage requires a permit.

- (15) **Maintenance or small repairs:** Ongoing maintenance or small repairs to buildings, structures or small areas of paving that do not significantly affect the appearance of the outside of the property and do not involve the permanent removal or loss of heritage attributes.

Exceptions

The removal and/or installation of any cladding materials requires a permit.

The removal of any paintwork from a masonry building façade surface requires a permit.

- (16) Replacement of non-heritage features of a non-contributing property that have been damaged and/or are in poor condition to exactly match the pre-existing features.

Exceptions

Changes of material, colour and design of the non-heritage features may require a permit.

- (17) **Painting:** The painting of wood cladding, doors, window frames, muntins and mullions, trim, eavestroughs, downspouts and minor architectural detailing in the same colour as existing.

Exceptions

Changing the colour of paint for any of the above requires a permit.

The painting of unpainted masonry materials is not permitted.

- (18) **Awnings:** The removal of existing awnings or canopies

Exceptions

The installation of new awnings or canopies requires a permit.

As with any modifications being contemplated, it is beneficial to contact town staff to discuss proposals before commencing work. Some of the above modifications may also require a building permit and/or other town permits. It is the responsibility of the property owner to confirm all requirements with appropriate town departments.

### **6.1.2 Public Realm Property**

Public realm property includes lands that are primarily located in road rights-of-way and the park and trails located in Character Area 1 - Waterfront Open Space at the foot of Navy Street, Thomas Street, George Street, Dunn Street, Trafalgar Street and Reynold Street. The following alterations may be carried out without obtaining a permit under Section 42 of the OHA:

- (1) **Maintenance or minor repairs:** Ongoing maintenance or minor repairs to road or sidewalks surfaces and areas of paving that do not significantly affect the appearance of the surface and that are exempt from review or approval under the Municipal Class Environmental Assessment.

Exceptions

The installation of any traffic calming device (not including signage), new road or sidewalk surfaces, new crosswalk surfaces or motifs and new boulevards may require a permit.

- (2) **Installation and/or repair of underground utilities or services:** Subsurface excavation for the installation and repair of utilities (water, sewage, gas, or communications). Surfaces are expected to be returned to their prior existing condition on the completion of work.

(3) **Repair and replacement of outfalls, shoreline, slope stability and erosion control**

**works:** Works that are required to address technical, engineering and safety issues along the shoreline and creek that are intended to match previously existing conditions and/or do not have an impact on the heritage attributes of the District, including vistas and viewlines.

Exceptions

Works that require substantially different materials and massing that will impact the heritage attributes of the District.

(4) **Repair of above-ground utilities or services:** Work undertaken for the repair of existing above-ground utilities (hydro, communications and lighting), including conduits, poles and associated boxes or covers.

Exceptions

The installation of any new luminaires and/or poles, boxes and covers. Exemptions may be made on a case-by-case basis if the new items match existing designs and materials and do not negatively impact views and vistas in the District.

(5) **Landscaping, soft:** The installation of any soft or vegetative landscaping confined to boulevard installation and associated planting beds. Tree and vegetation trimming to ensure sightlines at intersections are clear. All reasonable efforts will be made to maintain the mature tree canopy in the public realm.

Exceptions

The removal and/or planting of trees (with anticipated mature height of 4.5 metres or greater) and/or greater than 15 centimetres diameter at breast height (dbh) may require a permit.

(6) **Landscaping, hard:** The removal and re-installation of hard landscaping, such as driveways, entranceways, paths and parking areas that already exist, in the same materials and the same area and dimensions.

Exceptions

Changes to surfacing materials, enlargement of parking areas, driveways, entranceways and paths may require a permit.

(7) **Street Signs:** The removal and/or installation of street name signs and traffic signs in the existing design.

Exceptions:

New street signs with a different design than currently exists in the District may require a permit.

(8) **Street Furniture:** The removal and replacement or installation of street furniture in the existing design.

Exceptions

The installation of new street furniture in a new design requires a permit. This includes, but not restricted to: seating, planters, tree grates, banners, hanging baskets, garbage receptacles and bike racks.

- (9) **Painting:** The painting of signage, street furniture and buildings in the same colour as existing.

Exceptions

Changing the colour of paint for any of the above requires a permit.  
The painting of unpainted masonry materials is not permitted.

- (10) **Fencing:** The replacement of fencing to match existing.

Exceptions

New materials, design, or a change of height for fencing requires a permit.

- (11) **Security lighting and alarm systems:** The installation of security lighting and/or alarm systems.

- (12) **Amenity lighting:** The installation of amenity or seasonal lighting.

As with any modifications being contemplated, it is beneficial to contact town staff to discuss proposals before commencing work.

## 6.2 Emergency Work

In some instances, emergency work may have to be carried out to public or private property without the benefit of a heritage permit or ascertaining whether such work is exempt from regulation.

Required emergency work may be permitted where the timing of repairs makes it impossible to consult with town staff regarding a heritage permit. Notwithstanding this provision, all work should be undertaken in a manner that does not unnecessarily demolish or remove historic fabric. Photographs of 'before and after' should be taken to confirm the condition of the building and the nature of the finished repairs and supplied to town staff as a record of the work.

Some emergency work may be exempted from heritage permits on non-contributing properties where the damaged features are being replaced to exactly match the pre-existing features.



# 7 | HERITAGE CONSERVATION DISTRICT REVIEW PROCESS



## 7.1 Heritage Conservation District Review Process

The completion of this project marks the first major revision of the Old Oakville Heritage Conservation District Plan and Guidelines since the HCD was designated in 1979. This update was undertaken with the purpose of ensuring that this Plan meets the requirements of the OHA, which was significantly revised in 2005 and again in 2022. The update was also undertaken to ensure that this Plan corresponds with contemporary conditions in the District.

It is a good practice for municipalities to periodically undertake formal reviews of Plans. This formal review process complements the ongoing monitoring of the District's evolution that town staff perform as the Plan is implemented. It ensures that the Plan remains up to date with current legislation, with current best practices in heritage conservation, and meets community needs and expectations.

It is advised that town staff undertake a formal review and update of the Old Oakville Heritage Conservation District Plan and Guidelines every five to eight years. The extent of this review and update project will depend on a number of circumstances, including major revisions to related legislation or major changes in the built form of the District. However, it is anticipated that these periodic reviews will result in minor changes to this document, not re-writes.

The review process should include engagement with property owners, community members and other interested parties. An assessment of the heritage permits issued in the period following the last review should be undertaken. Recommendations for any potential revisions to this Plan should be made through a staff report reviewed by the Heritage Oakville Advisory Committee and approved by Town Council.

In addition, the policies and guidelines of this Plan may be amended by By-law after consultation, circulation to potentially impacted parties and public notice. Minor administrative and technical changes to the Plan may be implemented by a resolution of Town Council.





# 8 | GLOSSARY AND TERMS

## 8.1 Glossary of Terms

**Accessibility:** The degree to which an historic place is easy to access by as many people as possible, including people with disabilities (S&G 2010:253).

**Adjacent:** In terms of cultural heritage resources, potential impacts of proposed development or site alteration on the heritage attributes of protected heritage resources, adjacent can include real properties or sites that are contiguous (PPS 2024).

**Alter:** means to change in any manner and includes to restore, renovate, repair or disturb and “alteration” has a corresponding meaning (*Ontario Heritage Act*, s. 1).

**Character Areas:** a framework that acknowledges distinct features through the historic, streetscape and landscape analysis of Old Oakville’s evolving urban fabric. Five distinct streetscape and landscape Character Areas were established in the Old Oakville Heritage Conservation District Study and have area-specific site design guidance in addition to the general guidelines to manage change in Old Oakville. The five Character Areas and one streetscape overlay, sharing the unique historic and experienced heritage and quality of Old Oakville, collectively contribute to the overall values and heritage attributes of the HCD, and are categorized as:

- Area 1 - Waterfront Open Space
- Area 2 - Old Oakville Settlement Area
- Area 3 - Gully Landscape
- Area 4 - Mixed Residential Development Pattern
- Area 5 - St. Andrew’s Traditional
- Key Streetscape Overlay

**Compatible:** when used together with any building, use, alteration or any other form of change means consistent with the heritage attributes value of a property, and which has little or no adverse impact on its appearance, heritage attributes, and integrity (*Downtown Oakville Heritage Conservation District Plan and Guidelines*, 2013).

**Conserved:** means “the identification, protection, management and use of built heritage resources, cultural heritage landscapes and archaeological resources in a manner that ensures their cultural heritage value or interest is retained. This may be achieved by the implementation of recommendations set out in a conservation plan, archaeological assessment, and/or heritage impact statement that has been approved, accepted or adopted by relevant planning authority and/or decision-makers. Mitigative measures and/or alternative development approaches can be included in these plans and assessments” (PPS 2024).

**Contributing property:** exhibits physical heritage attributes which directly contribute to the cultural heritage value or interest of the District. They support the identified cultural heritage values from the Statement of CHVI They have met more than two criteria as identified in O. Reg 9/06.

**Cultural heritage landscape:** means a defined geographical area that may have been modified by human activity and is identified as having cultural heritage value or interest by a community, including an Indigenous community. The area may involve features such as buildings, structures, spaces, views, archaeological sites or natural elements that are valued together for their interrelationship, meaning or association (PPS 2024).



**Cultural Heritage Value or Interest (CHVI):** also referred to as Heritage Value, is identified if a property meets one of the criteria outlined in O. Reg. 9/06 namely historic or associate value, design or physical value and/or contextual value. Provincial significance is defined under *Ontario Heritage Act (OHA)* O. Reg. 10/06.

**Detached Additional Residential Unit:** a building separate from the Primary Structure that is used for residential purposes. This does not include garages, coach houses or structures associated with landscaping. For a full description, please refer to the Zoning By-law currently in effect.

**Distinguishable:** means a change that strikes a balance between imitation and contrast, thereby complementing the property in a manner that respects its heritage value or interest.

**Dormer:** a window that pierces through, or project from, the sloping roof, usually to that of a bedroom area (Adapted from Kyles 2022).

**Exceptional circumstances:** fire, natural hazards, structural condemnation by CAO, catastrophic failure of structure.

**Façade:** the "face" of a building, usually the front. To be a façade as opposed to simply an elevation, the building must have been designed with a particular style and incorporate design elements such as an impressive entrance or window surrounds. The arrangement of windows on a facade is called fenestration (Kyles 2022).

**Gable:** the triangular end of a roof above the eaves which closes the roof on that end. Also, the triangular end of a dormer or a triangular cut in a roof for a window or door. The slope of the gable end depends on the slope of the roof. For Gothic designs the slope tends to be acute; for Classical buildings the slope is gentler (Kyles 2022).

**Heritage Attributes:** "the principal features or elements that contribute to a protected heritage property's cultural heritage value or interest, and may include the property's built constructed, or manufactured elements, as well as natural landforms, vegetation, water features, and its visual setting (e.g. significant views or vistas to or from a protected heritage property (PPS 2024).

**Heritage building:** the primary structure located on a Contributing Property.

**Heritage Impact Assessment (HIA):** a study undertaken to assess the impacts of a proposed development or site alteration against the identified cultural heritage value or interest and heritage attributes of a protected heritage resource, or a property located within a Heritage Conservation District. The scope of a Heritage Impact Assessment is determined in consultation with the town. The HIA considers alternative development approaches or mitigation measures to address any impacts to a cultural heritage resource and its attributes. A Heritage Impact Assessment may be required where construction, alteration, demolition, or additions to a property located within a Heritage Conservation District.

**Heritage Oakville Advisory Committee:** or 'Heritage Oakville', is a municipal heritage committee that Council established, by by-law, to advise and assist council on heritage related matters. Heritage Oakville reviews heritage permits and other heritage-related matters [OHA s. 28(1)].

**Historic Fabric:** materials of all kinds that relate to the District's cultural heritage value and heritage attributes.

**Hipped roof:** a roof that slopes on four sides.

**In situ:** this term means 'in place' and as used in this document, it refers to the action of protecting, maintaining and/or stabilizing the existing materials in the location where they were found (S&G 2010:254)

**In kind:** with the same form, material, and detailing as the existing. (S&G 2010:254).

**Individual Designation:** means real property designated under section 29, Part IV of the OHA by municipal by-law. The designation by-law should include an adequate description of the property, a statement explaining the cultural heritage value or interest of the property and a description of the heritage attributes of the property (Section 29(4) of the OHA).

**Intervention:** any action, other than demolition or destruction, that results in a physical change to an element of a historic place (S&G 2010: 254).

**Maintenance:** routine, cyclical, non-destructive actions necessary to slow the deterioration of a historic place. It entails periodic inspection; routine, cyclical, non-destructive cleaning; minor repair and refinishing operations; replacement of damaged or deteriorated materials that are impractical to save (S&G 2010: 254).

**Minimal intervention:** the approach that allows functional goals to be met with the least physical intervention (S&G 2010:254).

**Monitoring:** the systematic and regular inspection or measurement of the condition of the materials and elements of an historic place to determine their behaviour, performance, and rate of deterioration over time (S&G 2010: 255).

**Muntin:** a strip of wood or metal separating and holding panes of glass in a window or a vertical framing member set between two rails in a door (S&G 2010: 255).

**Non-contributing property:** does not meet two or more of the criteria outline in O. Reg 9/06 and therefore do not exhibit design or physical value, historical or associative value, or contextual value.

**Non-Heritage building:** the primary structure on a non-contributing property.

**Non-destructive testing:** testing that does not result in the permanent deformation or damage of the element being tested (S&G 2010:255).

**Piecing-in:** to repair or add to by inserting a piece (S&G 2010: 255).

**Primary Structure:** the largest building or structure on a property.

**Prototype:** an original model on which something is patterned (S&G 2010: 255).

**Restoration:** the action or process of accurately revealing, recovering or representing the state of a historic place or of an individual component, as it appeared at a particular period in its history, while protecting its heritage value (S&G 2010:255).

**Rehabilitation:** the action or process of making possible a continuing or compatible contemporary use of a historic place or an individual component, while protecting its heritage value (S&G 2010:255).

**Reversible:** means a change that permits restoration to the prior state or condition later without damaging the heritage attributes of a property. This is particularly important if a change is related to a new use that may also later change. Reversible alterations are not destructive.

**Subordinate:** means an alteration that does not detract from the property or affect its heritage value.

**Soffit:** the underside of a roof overhang, portico, beam, or arch. It can also be the underside of a drain or sewer. These can be plain or very ornate (Kyles 2022).

**Vernacular:** made locally by inhabitants; made using local materials and traditional methods of construction and ornament; specific to a region or location (S&G 2010:256).

A photograph of a suburban street scene. In the foreground, a concrete sidewalk runs along a green lawn. A large, mature tree with a thick trunk stands prominently on the right side of the lawn. In the background, there are several houses, including a two-story yellow house and a white house with a brown roof. The scene is set in a residential neighborhood with lush greenery.

# 9 | BIBLIOGRAPHY AND SOURCES

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## Appendix A: Consultation Summary

The OHA Part V, Section 41.1, requires that the information related to this Plan is made available to the public, as well as presented at least one public meeting and the Municipal Heritage Committee (i.e., Heritage Oakville).

The public engagement plan was set out to not only fulfill the requirements noted above, but to meaningfully engage the community and key stakeholders through the development of both the HCD Study and Plan document updates. The following table outlines the key engagement activities completed during both phases of the project.

**Table 3: Community and Key Stakeholder Engagement Summary**

Study Activities	Engagement Description
Stakeholder Meetings 	<p>Virtual and in-person internal and external stakeholder meetings were held between February 2022 and November 2023. The town also met with stakeholders as needed or through requests to discuss the Old Oakville HCD. Meetings included internal department heads, Oakville Lakeside Residents Association (OLRA), Oakville Museum, Oakville Public Library, Street Jude's Anglican Church, and the Oakville Historical Society.</p> <p>Meeting topics included HCD Study introductions, a walking tour, updates on policy related to the study, draft HCD Study updates, and a review of inventory sheets.</p>
StoryMaps 	<p>A <a href="#">StoryMap</a> was developed to introduce the Old Oakville HCD project to the public. StoryMaps are an online resource linked from the website to convey information such as maps, imagery, and multimedia content in a visual way. The StoryMap went live on December 6, 2022, and was updated throughout the project.</p>
Online Surveys 	<p>The first online survey was developed to collect feedback from the public on the approach to the HCD Study. The online survey went live from December 1, 2022, to February 13, 2023, and received a total of 51 responses. The second online survey was specific to collect feedback on the Draft Study and was open from April 3, 2023, to May 19, 2023. The survey received a total of 30 responses.</p>

Study Activities	Engagement Description
Community Meetings 	<p>Three community meetings were held: first was a Study Introduction held on October 21, 2021, by town staff, second was a Study Update held on December 6, 2022 by the consultants and town staff, and third the Draft HCD Study held on April 18, 2023 by town staff.</p> <p>The goal of the community meetings was to provide the public with an introduction to the project, project timelines and opportunities to engage and provide updates on the HCD Study. The community meetings acted as additional opportunities for the public to provide feedback based on what they'd heard so far.</p>
Town, Council and Committee Meetings	<p>Presentation was made to Heritage Oakville on the Draft HCD Study in August 2023, and the Final HCD Study was presented to the Planning and Development Council Meeting in February 2024.</p>
HCD Plan Activities	Engagement Description
Stakeholder Meetings 	<p>Virtual and in-person internal and external stakeholder meetings, including the Heritage Oakville Advisory Committee and the Oakville Lakeside Residents Association, were held from March to August 2024 to discuss the HCD Plan guidance.</p>
StoryMaps 	<p>A <a href="#">StoryMap</a> was updated to include the Draft and Final HCD Plan.</p>
Community Meetings 	<p>Two community meetings were held: first was a Plan Introduction held on October 17, 2023, by town staff and consultants, and second the Draft Plan and Guidelines was held in March 2024 by town staff and consultants.</p> <p>The goal of the community meetings is to provide the public with an introduction to the HCD Plan and Guidelines and opportunities to provide feedback.</p>
Town, Council and Committee Meetings	<p>Monthly updates on the progress of the Plan have been presented to the Heritage Oakville Advisory Committee.</p> <p>The statutory public meeting required by the OHA was held on February 3, 2025.</p>

## **Appendix B: Town of Oakville Council Objectives**

### **Overall Intent**

(1) Town Council understands the crucial role of the historic built and natural environment to the quality of life and prosperity of the District, Downtown Oakville, and the town generally. It is the objective of Town Council to create policies and guidelines to protect the Old Oakville Heritage Conservation District from inappropriate development or changes. It is the intent of Council to guide and manage physical change and development within the District by:

- Adopting the updated Old Oakville Heritage Conservation District Plan and Guidelines;
- Making decisions about heritage permit applications for alterations, demolitions and new construction under Part V of the OHA according to the updated Old Oakville Heritage Conservation District Plan and Guidelines;
- Initiating appropriate public works, improvements and financial incentives to conserve and enhance the character of the Old Oakville Heritage Conservation District within the financial capabilities of the Town of Oakville; and
- Complementing these actions by making appropriate amendments to Official Plan policies, the town's Zoning By-law and other relevant by-laws.

### **Old Oakville HCD Heritage Character**

Council recognizes that:

- The Old Oakville Heritage Conservation District comprises a distinctive assemblage of heritage buildings and streetscapes that are an organically evolved cultural heritage landscape, being a historic harbourside village residential community dating from the early-19th century and early 20th century;
- The five streetscape and landscape Character Areas and one supplementary map overlay, developed as part of this HCD Study update, share unique historic and experienced heritage and quality of Old Oakville. The physical attributes of the area have evolved into a tangible streetscape character informed by the built environment's historical significance. All five Character Areas and one map overlay collectively contribute to the overall values and heritage attributes of the HCD;
- The unique heritage character of the Old Oakville Heritage Conservation District and its diverse streetscapes are to be conserved and protected in the process of future change;
- Change in the future is expected within the Old Oakville Heritage Conservation District, yet it must be carefully managed in a manner that does not adversely affect the distinctive heritage character of the District; and,
- Any proposed change within the District shall be considered within a number of Council-approved conservation, design, landscaping and planning guidelines and with consideration of the individual merits of the proposed change.

## **Town of Oakville conservation management approach**

Council recognizes that:

- District designation under Part V of the OHA, does not seek to stop or halt change or seek the restoration of the District to a former past historical state, but simply establishes a mechanism for the municipal review and determination of heritage permit applications for changes to properties, both public and private within the District.
- District designation under Part V of the OHA does not compel, nor does Council seek to compel, the restoration of heritage properties within the District.

## **Custodial responsibility**

Council recognizes that:

- Owners of heritage property are the prime custodians of the Old Oakville Heritage Conservation District.

## **Alteration of properties**

Council recognizes that:

- Property owners may wish to add on to buildings and structures, alter building and landscapes or otherwise change their property to accommodate required working or living space and new facilities and Council may permit such work provided it is in conformity with the applicable guidelines contained in this Plan.

## **Restoration of heritage properties**

Council recognizes that:

- Property owners may wish to restore heritage properties and Council may encourage such work by considering financial assistance available for eligible work and ensuring conformity with the applicable guidelines in this Plan.

## **Fair and equitable consideration**

Council will undertake to ensure that:

- All residents and property owners within the Old Oakville Heritage Conservation District shall be afforded fair and equitable consideration in the determination of heritage permit applications within the District.

## **Conservation Principles**

The federal and provincial government have well established standards and guidelines in place for the conservation of heritage properties and identified heritage attributes. These standards and guidelines should be carefully reviewed and considered prior to undertaking any conservation work to a contributing or non-contributing property within the District.

## **Accessibility**

Council recognizes that:

- It is important to encourage accessible design, elements and accommodations, including the use of Universal Design Standards, on heritage buildings when they do not negatively impact the heritage attributes of the District.

- Accessibility elements added on to heritage buildings, such as ramps, should be removable and/or repairable and/or reversible.
- Buildings within the District that provide public facilities, including places of worship, the Oakville Museum and Historical Society and the Oakville Club, may be subject to the *Ontarians with Disabilities Act*. All efforts to accommodate accessibility accommodations that do not negatively impact the heritage attributes of the District should be considered.

## **Sustainability**

Council recognizes that:

- At the June 24, 2019, Council meeting, Oakville Town Council passed a motion declaring a climate emergency in Oakville.
- The greenest building is the one that already exists. Repair and restoration are always encouraged over replacement, avoiding sending historical materials to landfill.
- Heritage buildings can generally be retrofitted with energy efficient internal systems without significant impact to historic fabric.
- Technology is making constant progress and new materials/products will be considered on a case-by-case basis for use within the District.
- Poly vinyl chloride, polystyrene, polymer and other plastic building materials contain harmful chemicals and are not easily reusable or biodegradable and shall not be permitted within the District.

## Appendix C: General Standards for Preservation, Rehabilitation and Restoration

The *Standards and Guidelines for the Conservation of Historic Places in Canada* also provide general standards for preservation, rehabilitation and restoration with further explanation regarding how these standards should be applied. In 2013, the town endorsed the Standards and Guidelines for application in the planning, stewardship and conservation of heritage resources in the town. The *Standards and Guidelines for the Conservation of Historic Places in Canada* were used as guiding principles for drafting the Old Oakville HCD Plan.

**Table 4: General Standards for Preservation, Rehabilitation and Restoration**

Treatment	No.	Description
General Standards	1	Conserve the heritage value of an historic place. Do not remove, replace or substantially alter its intact or repairable character defining elements. Do not move a part of an historic place if its current location is a character-defining element.
General Standards	2	Conserve changes to an historic place that, over time, have become character-defining elements.
General Standards	3	Conserve heritage value by adopting an approach calling for minimal intervention
General Standards	4	Recognize each historic place as a physical record of its time, place and use. Do not create a false sense of historical development by adding elements from other historic places or other properties, or by combining features of the same property that never coexisted
General Standards	5	Find a use for an historic place that requires minimal or no change to its character-defining elements
General Standards	6	Protect and, if necessary, stabilize an historic place until any subsequent intervention is undertaken; Protect and preserve archaeological resources in place. Where there is potential for disturbing archaeological resources, take mitigation measures to limit damage and loss of information
General Standards	7	Evaluate the existing condition of character-defining elements to determine the appropriate intervention needed. Use the gentlest means possible for any intervention. Respect heritage value when undertaking an intervention
General Standards	8	Maintain character-defining elements on an ongoing basis. Repair character-defining elements by reinforcing their materials using recognized conservation methods. Replace in kind any extensively deteriorated or missing parts of character-defining elements, where there are surviving prototypes
General Standards	9	Make any intervention needed to preserve character-defining elements physically and visually compatible with the historic place and identifiable on close inspection. Document any intervention for future reference
Additional: Rehabilitation	10	Repair rather than replace character-defining elements. Where character-defining elements are too severely deteriorated to repair, and where sufficient physical evidence exists, replace them with new elements that match the forms, materials and detailing of sound versions of the same elements. Where there is insufficient physical evidence, make the form, material and detailing of the new elements compatible with the character of the historic place

<b>Treatment</b>	<b>No.</b>	<b>Description</b>
Additional: Rehabilitation	11	Conserve the heritage value and character-defining elements when creating any new additions to an historic place or any related new construction. Make the new work physically and visually compatible with, subordinate to and distinguishable from the historic place
Additional: Rehabilitation	12	Create any new additions or related new construction so that the essential form and integrity of an historic place will not be impaired if the new work is removed in the future
Additional: Restoration	13	Repair rather than replace character-defining elements from the restoration period. Where character-defining elements are too severely deteriorated to repair and where sufficient physical evidence exists, replace them with new elements that match the forms, materials and detailing of sound versions of the same elements
Additional: Restoration	14	Replace missing features from the restoration period with new features with forms, materials and detailing based on sufficient physical, documentary and/or oral evidence



## **Appendix D: *Eight Guiding Principles in the Conservation of Built Heritage Properties***


The Ministry of Citizenship and Multiculturalism (MCM) has established the following *Eight Guiding Principles in the Conservation of Built Heritage Properties* that are often referenced when preparing HCD Plan guidelines. These principles were referenced while drafting the design guidelines for the Old Oakville HCD Plan.



- 1) Respect for Documentary Evidence:
  - Do not base restoration on conjecture: conservation work should be based on historic documentation such as historic photographs, drawings and physical evidence.
- 2) Respect for the Original Location:
  - Do not move buildings unless there is no other means to save them: site is an integral component of a building or structure. Change in site diminishes cultural heritage value.
- 3) Respect for Historic Material:
  - Repair/conservé – rather than replace building materials and finishes, except where necessary: minimal intervention maintains the heritage content of the built resource.
- 4) Respect for Original Fabric:
  - Repair with like materials: repair to return the resource to its prior condition, without altering its integrity.
- 5) Respect for the Building's History:
  - Do not restore to one period at the expense of another period: do not destroy later additions to a building or structure solely to restore to a single period.
- 6) Reversibility:
  - Alterations should be able to be returned to original conditions. This conserves earlier building design and technique: e.g. when a new door opening is put into a stone wall, the original stones are numbered, removed and stored, allowing for future restoration.
- 7) Legibility:
  - New work should be distinguishable from old: buildings or structures should be recognized as products of their own time, and new additions should not blur the distinction between old and new.
- 8) Maintenance:
  - With continuous care, future restoration will not be necessary: with regular upkeep, major conservation projects and their high costs can be avoided.



## Appendix E: Examples of Good Neighbours

Since the inception of the District in 1981, there have been a number of new buildings constructed that are sympathetic to the cultural heritage values and heritage attributes of Old Oakville. They can be considered “good neighbours” to their adjacent contributing properties and to the District as a whole. This type of integrative design is anchored in the topography of the land, contextually appropriate within the existing streetscape, respects traditional lines of sight and views; and adds to the established architectural integrity and heritage styles of the District.



These buildings provide vision for new development within the District. Note the intention is not for these buildings to be copied, but rather used as inspiration for how the guidelines can be applied.

Address	Photo	What Makes This Building a Good Neighbour?
221 Front Street		<ul style="list-style-type: none"> <li>- Historic ¼ acre lot</li> <li>- Deep setback to align with adjacent properties</li> <li>- Consistent with streetscape guidelines regarding emphasis on prominence of Front Street pedestrian traffic</li> <li>- Does not distract from property at 212 Front Street (Worn Doorstep)</li> <li>- New Traditional style that reflects historic Arts &amp; Crafts style found in the District</li> <li>- Use of traditional materials associated with style including shingle cladding and diamond pane windows</li> <li>- 1 ½ storey massing with front porch, hipped roof, gable end and set back garage that emulates a carriage house</li> </ul>

Address	Photo	What Makes This Building a Good Neighbour?
22 George Street		<ul style="list-style-type: none"> <li>- Historic ¼ acre lot</li> <li>- Corner property with important public views on two elevations</li> <li>- Setback on corner lot of key District streetscape enables open views along Front Street pedestrian laneway as well as views north on George Street</li> <li>- Mix of roofline heights creates 1 ½ storey impact that respects step down slope on Front Street</li> <li>- New Traditional style that reflects historical cottage vernacular style</li> <li>- Use of traditional materials associated with style including horizontal wood cladding, multi framed wood windows</li> <li>- Set back garage that emulates carriage house</li> </ul>
177 King Street		<ul style="list-style-type: none"> <li>- Historic ¼ acre lot</li> <li>- Corner property with important public views on two elevations</li> <li>- Setback aligns with adjacent contributing properties to the east on King Street</li> <li>- New Traditional style that reflects Georgian style found in the District</li> <li>- Impact of 2 storey height minimized through low hip roof</li> <li>- Set back modest garage</li> <li>- Use of traditional materials associated with style including brick cladding and multipaned wood windows</li> <li>- Complements landmark St. Jude's property to the west (brick cladding) and contributing property at 29 Thomas to the south (2 storey)</li> </ul>

Address	Photo	What Makes This Building a Good Neighbour?
234 William Street		<ul style="list-style-type: none"> <li>- Historic ¼ acre lot</li> <li>- Corner property with important public views on two elevations</li> <li>- Setback aligns with adjacent contributing property at 226 William Street (St. Jude's Parsonage)</li> <li>- New Traditional style that reflects Georgian styles found in the District</li> <li>- 2 storey height minimized through low hip roof style</li> <li>- Set back modest garage</li> <li>- Use of traditional materials associated with style, such as brick cladding that also complements adjacent contributing property</li> </ul>
258 William Street		<ul style="list-style-type: none"> <li>- Association with nearby historic estate maintained by lake stone wall in back yard</li> <li>- Setback on site to align with adjacent contributing property to the west and good neighbour to the east</li> <li>- 1 ½ storey modest home with gable roof and traditional materials, including board and batten wood siding</li> </ul>




Address	Photo	What Makes This Building a Good Neighbour?
266 William Street		<ul style="list-style-type: none"> <li>- Historic ¼ acre lot</li> <li>- Setback aligns with adjacent contributing properties</li> <li>- New Traditional style that reflects Georgian styles found in the District</li> <li>- Massing of 2 storey height minimized through low gable roof</li> <li>- Detached modest garage at the rear of the property</li> <li>- Use of traditional materials associated with style including wood clapboard siding and multipaned wood windows</li> </ul>
274 William Street		<ul style="list-style-type: none"> <li>- Historic ¼ acre lot</li> <li>- Corner property with important public views on two elevations</li> <li>- Setback aligns with adjacent properties</li> <li>- New Traditional style that reflects vernacular 20th century architecture</li> <li>- 1 ½ storey with gable roof and shed dormers minimizes impact of height</li> <li>- Use of traditional materials associated with this style including a combination of stucco and wood clapboard siding; and multipaned wood windows</li> </ul>


Address	Photo	What Makes This Building a Good Neighbour?
323 William Street		<ul style="list-style-type: none"><li>- Historic ¼ acre lot</li><li>- Corner property with important public views on two elevations</li><li>- Setback aligns with adjacent contributing properties</li><li>- New Traditional style that reflects vernacular 20th century architecture</li><li>- 1 ½ storey with cross gable roof minimizes height</li><li>- Use of traditional materials, combining stucco and wood cladding</li><li>- Low picket fence along Reynolds Street allows views from the public realm into the property</li></ul>



349 William Street		<ul style="list-style-type: none"> <li>- Historic ¼ acre lot</li> <li>- Corner property with important public views on two elevations</li> <li>- Setback aligns with adjacent contributing properties</li> <li>- New Traditional style that reflects vernacular 20th century architecture</li> <li>- 1 ½ storey with side gable roof minimizes height</li> <li>- Use of traditional materials, horizontal wood cladding</li> <li>- Low picket fence along sidewalk and curb allows views from the public realm into the property</li> </ul>
30 Dunn Street		<ul style="list-style-type: none"> <li>- Corner property with important public views on two elevations</li> <li>- Low profile and siting of allows for southwest views to Dingle Park and Lake Ontario originating at King Street</li> <li>- Mid 20th century vernacular that reflects Arts and Crafts cottage style</li> <li>- 1 storey building with low hip roof fits low profile of District</li> <li>- Use of traditional materials including horizontal wood siding and multipane windows</li> <li>- Low profile massing built into hillside takes advantage of natural grade changes</li> </ul>



44 George Street		<ul style="list-style-type: none"><li>- Historic ¼ acre lot</li><li>- Corner property with important public views on two elevations</li><li>- New Traditional style that reflects 19<sup>th</sup> century vernacular (note: middle portion replicates original structure)</li><li>- 1 ½ storey building with two one storey wings and gable roof fits the low profile of the District;</li><li>- Use of traditional materials, including stucco cladding and wood trim</li></ul>
21 Dunn Street		<ul style="list-style-type: none"><li>- Historic ¼ acre lot associated with contributing property (Lightbourn family) to north</li><li>- Use of traditional materials including wood cladding, multi framed wood windows</li><li>- Detached garage setback to the rear of the property emulates a carriage house</li><li>- Open iron fencing along Waterfront Trail and Dunn Street allows open views from the public realm</li><li>- Note: the corner 'tower' projection is a product of its time and is not intended to be inspiration for new development</li></ul>

<p>275 King Street</p>		<ul style="list-style-type: none"><li>- Historic ¼ acre lot</li><li>- Scale, height and massing complementary to contributing properties to south on King Street and to east on Trafalgar Road</li><li>- Setback on corner lot with low profile landscaping enables open views along King Street as well as views north on Trafalgar Road towards contributing properties</li><li>- Introduction of traditional materials including wood cladding and wood windows</li><li>- Note: the projecting garage is a product of its time and is not intended to be inspiration for new development</li></ul>
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## Appendix F: Town's List of Appropriate Tree Species

Common Name	Family	Genus	Species	Cultivars	Ideal Soil Needs	Native	Est. Height (m) at Maturity	Est. Canopy Width (m) at Maturity	Canopy Stature
Apple, common	Rosaceae	<i>Malus</i>	<i>pumila</i>		Medium	Yes	7	7	Small
Aspen, Large-toothed	Salicaceae	<i>Populus</i>	<i>grandidentata</i>		Large	Yes	18	12	Large
Aspen, Trembling	Salicaceae	<i>Populus</i>	<i>tremuloides</i>		Large	Yes	10	5	Small
Autumn Brilliance Serviceberry	Rosaceae	<i>Amelanchier</i>	<i>x grandiflora</i>	'Autumn Brilliance'	Medium	No	8	6	Small
Autumn Moon Full Moon Maple	Sapindaceae	<i>Acer</i>	<i>shirasawanum</i>	'Autumn Moon'	Medium	No	6	6	Small
Basswood	Malvaceae	<i>Tilia</i>	<i>americana</i>		Large	Yes	27	13	Large
Beech, American	Fagaceae	<i>Fagus</i>	<i>grandifolia</i>		Large	Yes	30	20	Large
Beech, Blue	Betulaceae	<i>Carpinus</i>	<i>caroliniana</i>		Medium	Yes	8	6	Small
Beech, Dawyck Gold	Fagaceae	<i>Fagus</i>	<i>sylvatica</i>	'Dawyck Gold'	Large	No	16	2	Small
Beech, Dawyck Purple	Fagaceae	<i>Fagus</i>	<i>sylvatica</i>	'Dawyck Purple'	Large	No	8	2	Small
Beech, European	Fagaceae	<i>Fagus</i>	<i>sylvatica</i>		Large	No	15	12	Large
Beech, Purple Fountain	Fagaceae	<i>Fagus</i>	<i>sylvatica</i>	'Purple Fountain'	Large	No	6	4	Small
Beech, Red Obelisk	Fagaceae	<i>Fagus</i>	<i>sylvatica</i>	'Red Obelisk'	Large	No	13	4	Small
Beech, Tri-colour	Fagaceae	<i>Fagus</i>	<i>sylvatica</i>	'Rosea-Marginata'	Large	No	13.5	8	Medium
Birch, Cherry	Betulaceae	<i>Betula</i>	<i>lenta</i>		Large	Yes	15	12	Large
Birch, European White	Betulaceae	<i>Betula</i>	<i>pendula</i>		Large	No	15	10	Medium
Birch, Gray	Betulaceae	<i>Betula</i>	<i>populifolia</i>		Large	Yes	10	6	Small
Birch, River	Betulaceae	<i>Betula</i>	<i>nigra</i>		Large	No	13	10	Medium
Birch, White (Paper)	Betulaceae	<i>Betula</i>	<i>papyrifera</i>		Large	Yes	18	10	Large
Birch, Yellow	Betulaceae	<i>Betula</i>	<i>alleghaniensis</i>		Medium	Yes	18	15	Large
Black Gum	Nyssaceae	<i>Nyssa</i>	<i>sylvatica</i>		Medium	Yes	13.5	8.5	Medium
Buckeye, Ohio	Sapindaceae	<i>Aesculus</i>	<i>glabra</i>		Large	Yes	13.5	13.5	Large
Butternut	Juglandaceae	<i>Juglan</i>	<i>cinera</i>		Large	Yes	12	11	Medium
Catalpa, Northern	Bignoniaceae	<i>Catalpa</i>	<i>speciosa</i>		Large	No	12	6	Small
Cherry, Black	Rosaceae	<i>Prunus</i>	<i>serotina</i>		Small	Yes	15	6	Medium
Cherry, Choke	Rosaceae	<i>Prunus</i>	<i>virginiana</i>		Medium	Yes	5	5	Small
Cherry, Kwanzan	Rosaceae	<i>Prunus</i>	<i>serrulata</i>	'Kwanzan'	Small	No	7	5	Small
Cherry, Pin	Rosaceae	<i>Prunus</i>	<i>pensylvanica</i>		Small	Yes	8	8	Medium
Chestnut, American	Fagaceae	<i>Castanea</i>	<i>dentata</i>		Large	Yes	18	18	Large
Cockspur Hawthorn	Rosaceae	<i>Crataegus</i>	<i>crus-galli</i>		Medium	No	7.5	7.5	Small
Cottonwood, Black	Salicaceae	<i>Populus</i>	<i>trichocarpa</i>		Large	No	27	21	Large
Cottonwood, Eastern	Salicaceae	<i>Populus</i>	<i>deltoidea</i>		Large	Yes	27	21	Large
Crabapple, Prairie Fire	Rosaceae	<i>Malus</i>		'Prairie Fire'	Medium	No	7	7	Small
Crabapple, Royal Raindrops	Rosaceae	<i>Malus</i>		'Royal Raindrops'	Medium	No	7	7	Small
Crabapple, Sargent	Rosaceae	<i>Malus</i>		'Sargent'	Medium	No	7	7	Small
Crabapple, White Angel	Rosaceae	<i>Malus</i>		'White Angel'	Medium	No	7	7	Small
Crimson Cloud English Hawthorn	Rosaceae	<i>Crataegus</i>	<i>laevigata</i>	'Superba'	Medium	No	6	6	Small
Cucumber Tree	Magnoliaceae	<i>Magnolia</i>	<i>acuminata</i>		Large	Yes	16	16	Large
Cutleaf Beech	Fagaceae	<i>Fagus</i>	<i>sylvatica</i>	'Asplenifolia'	Large	No	18	14	Large
Cypress, Bald	Cupressaceae	<i>Taxodium</i>	<i>distichum</i>		Large	No	20	8	Medium
Elm, Accolade	Ulmaceae	<i>Ulmus</i>	<i>japonica</i>	'wilsoniana'	Large	No	23	20	Large
Elm, American Princeton	Ulmaceae	<i>Ulmus</i>	<i>americana</i>	'Princeton'	Large	No	21	15	Large
Elm, Valley Forge	Ulmaceae	<i>Ulmus</i>	<i>americana</i>	'Valley Forge'	Large	No	21	21	Large
Fir, Balsam	Pinaceae	<i>Abies</i>	<i>balsamea</i>		Large	Yes	15	6	Medium
Fir, Douglas	Pinaceae	<i>Pseudotsuga</i>	<i>menziesii</i>		Large	No	20	5	Medium
Fir, Fraser	Pinaceae	<i>Abies</i>	<i>fraseri</i>		Medium	No	11	6	Small
Fir, White	Pinaceae	<i>Abies</i>	<i>concolor</i>		Large	No	14	6	Medium
Frans Fontaine Hornbeam	Betulaceae	<i>Carpinus</i>	<i>betulus</i>	'Frans Fontaine'	Large	No	12	6	Small
Ginkgo (Maidenhair)	Ginkgoaceae	<i>Ginkgo</i>	<i>biloba</i>		Medium	Yes	17	11	Large
Ginkgo, Autumn Gold	Ginkgoaceae	<i>Ginkgo</i>	<i>biloba</i>	'Autumn Gold'	Medium	No	10	10	Medium
Ginkgo, Golden Colonade	Ginkgoaceae	<i>Ginkgo</i>	<i>biloba</i>	'JFS-UGA2'	Medium	No	13	7.5	Medium
Ginkgo, Princeton Sentry	Ginkgoaceae	<i>Ginkgo</i>	<i>biloba</i>	'Princeton Sentry'	Medium	No	13	5	Small
Golden Full Moon Maple	Sapindaceae	<i>Acer</i>	<i>shirasawanum</i>	'Aureum'	Medium	No	6	6	Small
Hackberry	Cannabaceae	<i>Celtis</i>	<i>occidentalis</i>		Large	Yes	20	18	Large
Hazelnut, Turkish	Betulaceae	<i>Corylus</i>	<i>colurna</i>		Medium	No	15	8	Medium
Hemlock, Eastern	Pinaceae	<i>Tsuga</i>	<i>canadensis</i>		Large	Yes	20	5	Medium
Hickory, Bitternut	Juglandaceae	<i>Carya</i>	<i>cordiformis</i>		Large	Yes	25	20	Large
Hickory, Pignut	Juglandaceae	<i>Carya</i>	<i>glabra</i>		Large	No	17	8	Medium
Hickory, Shagbark	Juglandaceae	<i>Carya</i>	<i>ovata</i>		Large	Yes	20	20	Large
Hickory, Shellbark	Juglandaceae	<i>Carya</i>	<i>lociniosa</i>		Large	Yes	23	15	Large
Hop tree	Rutaceae	<i>Ptelea</i>	<i>trifoliata</i>		Medium	Yes	5	5	Small
Hornbeam, European	Betulaceae	<i>Carpinus</i>	<i>betulus</i>		Large	No	17	12	Large
Hornbeam, European Pyramidal	Betulaceae	<i>Carpinus</i>	<i>betulus</i>	'Fastigiata'	Large	No	12	5	Small
Horsechestnut	Sapindaceae	<i>Aesculus</i>	<i>hippocastanum</i>		Large	No	12	12	Medium
Horsechestnut, Double	Sapindaceae	<i>Aesculus</i>	<i>baumannii</i>		Large	No	15	12	Large
Horsechestnut, Red	Sapindaceae	<i>Aesculus</i>	<i>x carnea</i>	'Briotii'	Large	No	12	12	Medium
Ironwood (American Hophornbeam)	Betulaceae	<i>Ostrya</i>	<i>virginiana</i>		Large	Yes	12	8	Medium
Karpik Red Maple	Sapindaceae	<i>Acer</i>	<i>rubrum</i>	'Karpik'	Large	No	12	6	Small

Katsura, Japanese	Cercidiphyllaceae	<i>Cercidiphyllum</i>	<i>japonicum</i>		Large	No	15	4	Small
Kentucky Coffee Tree	Fabaceae	<i>Gymnocladus</i>	<i>dioicus</i>		Large	Yes	17	13	Large
Kentucky Coffee Tree, Espresso	Fabaceae	<i>Gymnocladus</i>	<i>dioicus</i>	'Espresso'	Large	No	15	10	Medium
Larch, European	Pinaceae	<i>Larix</i>	<i>decidua</i>		Large	No	15	7	Medium
Lilac, Japanese Tree	Oleaceae	<i>Syringa</i>	<i>reticulata</i>	'Ivory Silk'	Medium	No	8	4	Small
Linden, Little-leaf	Malvaceae	<i>Tilia</i>	<i>cordata</i>		Large	No	17	20	Large
Locust, Honey	Fabaceae	<i>Gleditsia</i>	<i>triacanthos</i>		Large	Yes	17	10	Medium
Locust, Honey Shademaster	Fabaceae	<i>Gleditsia</i>	<i>triacanthos</i>	var 'Inermis Shademaster'	Large	No	17	10	Medium
Locust, Honey Skyline	Fabaceae	<i>Gleditsia</i>	<i>triacanthos</i>	var 'Inermis Skyline'	Large	No	15	13	Large
Locust, Honey Streetkeeper	Fabaceae	<i>Gleditsia</i>	<i>triacanthos</i>	var 'Inermis Draves'	Large	No	15	7	Medium
Locust, Honey Sunburst	Fabaceae	<i>Gleditsia</i>	<i>triacanthos</i>	var 'Inermis Sunburst'	Large	No	15	13	Large
Maple, Amur	Sapindaceae	<i>Acer</i>	<i>ginnala</i>		Medium	No	6	6	Small
Maple, Armstrong	Sapindaceae	<i>Acer</i>	<i>rubrum</i>	'Armstrong'	Large	No	20	5	Medium
Maple, Autumn Spire	Sapindaceae	<i>Acer</i>	<i>rubrum</i>	'Autumn Spire'	Large	No	16	8	Medium
Maple, Black	Sapindaceae	<i>Acer</i>	<i>nigrum</i>		Large	Yes	20	15	Large
Maple, Brandywine	Sapindaceae	<i>Acer</i>	<i>rubrum</i>	'Brandywine'	Large	No	10	4	Small
Maple, Celebration	Sapindaceae	<i>Acer</i>	<i>x Freemanii</i>	'Celebration'	Large	No	14	6	Medium
Maple, 'Columnar'	Sapindaceae	<i>Acer</i>	<i>rubrum</i>	'Columnare'	Large	No	15	5	Small
Maple, Flame Amur	Sapindaceae	<i>Acer</i>	<i>ginnala</i>	'Flame'	Medium	No	6	6	Small
Maple, Freemanii	Sapindaceae	<i>Acer</i>	<i>x Freemanii</i>		Large	No	16	13	Large
Maple, Freemanii	Sapindaceae	<i>Acer</i>	<i>x Freemanii</i>	'Jeffersred'	Large	No	16	13	Large
Maple, Green Mountain	Sapindaceae	<i>Acer</i>		'Green Mountain'	Large	No	22	17	Large
Maple, Hedge	Sapindaceae	<i>Acer</i>	<i>campestre</i>		Medium	No	10	10	Medium
Maple, Manitoba 'Flamingo'	Sapindaceae	<i>Acer</i>	<i>negundo</i>	'Flamingo'	Large	No	8	6	Small
Maple, Mountain	Sapindaceae	<i>Acer</i>	<i>spicatum</i>		Small	Yes	6	3	Small
Maple, Paperbark	Sapindaceae	<i>Acer</i>	<i>griseum</i>		Medium	No	7	5	Small
Maple, Red	Sapindaceae	<i>Acer</i>	<i>rubrum</i>		Large	Yes	16	15	Large
Maple, Red 'October Glory'	Sapindaceae	<i>Acer</i>	<i>rubrum</i>	'October Glory'	Large	No	15	12	Large
Maple, Red Sunset	Sapindaceae	<i>Acer</i>	<i>rubrum</i>	'Red Sunset'	Large	No	18	12	Large
Maple, Ruby Slippers Amur	Sapindaceae	<i>Acer</i>	<i>ginnala</i>	'Ruby Slippers'	Medium	No	6	6	Small
Maple, Scarlet Sentinel	Sapindaceae	<i>Acer</i>	<i>rubrum</i>	'Scarlet Sentinel'	Large	No	15	8	Medium
Maple, Silver	Sapindaceae	<i>Acer</i>	<i>saccharinum</i>		Large	Yes	18	15	Large
Maple, Silver Queen	Sapindaceae	<i>Acer</i>	<i>Saccharinum</i>	'Silver Queen'	Large	No	16	13	Large
Maple, Striped	Sapindaceae	<i>Acer</i>	<i>pensylvanicum</i>		Large	Yes	9	7	Small
Maple, Sugar	Sapindaceae	<i>Acer</i>	<i>saccharum</i>		Large	Yes	20	15	Large
Maple, Sugar 'Columnar'	Sapindaceae	<i>Acer</i>	<i>saccharum</i>	'Columnare'	Large	No	20	4	Small
Maple, Tartarian	Sapindaceae	<i>Acer</i>	<i>tataricum</i>		Small	No	5	6	Small
Maple, Tartarian 'Hot-Wings'	Sapindaceae	<i>Acer</i>	<i>tataricum</i>	'Hotwings'	Small	No	7	6	Small
Maple, 'Bowhall'	Sapindaceae	<i>Acer</i>	<i>rubrum</i>	'Bowhall'	Large	No	15	5	Small
Mountain-Ash, American	Rosaceae	<i>Sorbus</i>	<i>americana</i>		Medium	Yes	6	6	Small
Mountain-Ash, Showy	Rosaceae	<i>Sorbus</i>	<i>decora</i>		Medium	Yes	7	6	Small
Mulberry, Red	Moraceae	<i>Morus</i>	<i>rubra</i>		Medium	Yes	12	12	Medium
Oak, Black	Fagaceae	<i>Quercus</i>	<i>velutina</i>		Large	Yes	20	20	Large
Oak, Bur	Fagaceae	<i>Quercus</i>	<i>macrocarpa</i>		Large	Yes	18	13	Large
Oak, Chinquapin	Fagaceae	<i>Quercus</i>	<i>muehlenbergii</i>		Large	No	15	15	Large
Oak, English	Fagaceae	<i>Quercus</i>	<i>robur</i>		Large	No	18	13	Large
Oak, English Pyramidal	Fagaceae	<i>Quercus</i>	<i>robur</i>	'Fastigata'	Large	No	15	5	Small
Oak, English Skinny Genes	Fagaceae	<i>Quercus</i>	<i>robur</i>	'Skinny Genes'	Large	No	15	3	Small
Oak, English Skyrocket	Fagaceae	<i>Quercus</i>	<i>robur</i>	'Skyrocket'	Large	No	20	5	Medium
Oak, Pin	Fagaceae	<i>Quercus</i>	<i>palustris</i>		Large	Yes	20	13	Large
Oak, Red	Fagaceae	<i>Quercus</i>	<i>rubra</i>		Large	Yes	16	15	Large
Oak, Red Kindred Spirit	Fagaceae	<i>Quercus</i>	<i>rubra</i>	'X Bicolor Nadler'	Large	No	10	2	Small
Oak, Shumard	Fagaceae	<i>Quercus</i>	<i>shumardii</i>		Large	Yes	12	12	Medium
Oak, Swamp White	Fagaceae	<i>Quercus</i>	<i>bicolor</i>		Large	Yes	15	15	Large
Oak, White	Fagaceae	<i>Quercus</i>	<i>alba</i>		Large	Yes	20	20	Large
Orange, Osage	Moraceae	<i>Maclura</i>	<i>pomifera</i>		Medium	No	12	12	Medium
Orange, Osage 'White Shield'	Moraceae	<i>Maclura</i>	<i>pomifera</i>	'White Shield'	Medium	No	12	12	Medium
Pagoda Tree, Japanese	Fabaceae	<i>Sophora</i>	<i>japonica</i>		Large	No	22	20	Large
Pawpaw	Annonaceae	<i>Asimina</i>	<i>triloba</i>		Medium	Yes	6	4.5	Small
Pine, Austrian	Pinaceae	<i>Pinus</i>	<i>nigra</i>		Large	No	18	15	Large
Pine, Eastern White	Pinaceae	<i>Pinus</i>	<i>strobus</i>		Large	Yes	24	11	Large
Pine, Eastern White Pyramidal	Pinaceae	<i>Pinus</i>	<i>strobus</i>	'Fastigata'	Large	No	15	2.5	Small
Pine, Red	Pinaceae	<i>Pinus</i>	<i>resinosa</i>		Large	Yes	20	10	Large
Pine, Scots	Pinaceae	<i>Pinus</i>	<i>sylvestris</i>		Large	No	15	9	Medium
Planetree, Exclamation	Platanaceae	<i>Platanus</i>	<i>x acerifolia</i>	'Morton Circle'	Large	No	16	10	Medium
Planetree, London	Platanaceae	<i>Platanus</i>	<i>x acerifolia</i>		Large	No	20	20	Large
Planetree, London Bloodgood	Platanaceae	<i>Platanus</i>	<i>x acerifolia</i>	'Bloodgood'	Large	No	16	13	Large
Plum, Canada	Rosaceae	<i>Prunus</i>	<i>nigra</i>		Small	Yes	5	5	Small
Poplar, Balsam	Salicaceae	<i>Populus</i>	<i>balsamifera</i>		Large	Yes	13	6	Medium
Redbud, Eastern	Fabaceae	<i>Cercis</i>	<i>canadensis</i>		Small	Yes	9	9	Medium
Redbud, Forest Pansy	Fabaceae	<i>Cercis</i>	<i>canadensis</i>	'Forest Pansy'	Small	No	9	9	Medium
Redbud, Hearts of Gold	Fabaceae	<i>Cercis</i>	<i>canadensis</i>	'Hearts of Gold'	Medium	No	6	6	Small
Redbud, Silver Cloud	Fabaceae	<i>Cercis</i>	<i>canadensis</i>	'Silver Cloud'	Small	No	8	9	Medium
Redbud, Texas White	Fabaceae	<i>Cercis</i>	<i>canadensis</i>	'Texas White'	Small	No	8	9	Medium



## THE CORPORATION OF THE TOWN OF OAKVILLE

### BY-LAW NUMBER 2025-055

A by-law to declare that certain land is not subject to part lot control (Block 124, Plan 20M-1272, Lots 26, 27, 28 and Blocks 32, 33, 34, Plan 20M-1281 – Tinor Development (BT) Corp.)

**WHEREAS** By-law 2006-125 delegates to the Director of Planning and Development the authority to approve certain applications to designated lands not subject to part lot control; and,

**WHEREAS** the Director of Planning and Development has approved such an application for the lands described in Schedule “A”;

### COUNCIL ENACTS AS FOLLOWS:

1. Part lot control pursuant to subsection 5 of Section 50 of the *Planning Act*, R.S.O. 1990, c.P-13, as amended does not apply to lands as set out in Schedule “A” attached hereto.
2. This by-law expires one (1) year from the date it has been passed by Council.
3. Schedule “A” forms part of this by-law.
4. The solicitor is hereby authorized to amend the parcel designation, if necessary, upon registration of this by-law.

PASSED this 17<sup>th</sup> day of March, 2025

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MAYOR

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CLERK

**Schedule “A”**

1. Block 124, Plan 20M-1272, designated as Parts 16 to 19, inclusive, on Plan 20R-22841, Oakville
2. Lot 26, Plan 20M-1281, designated as Parts 1 and 2 on Plan 20R-22841, Oakville
3. Lot 27, Plan 20M-1281, designated as Parts 3 and 4 on Plan 20R-22841, Oakville
4. Lot 28, Plan 20M-1281, designated as Parts 5 and 6 on Plan 20R-22841, Oakville
5. Block 32, Plan 20M-1281, designated as Parts 7 to 13, inclusive, on Plan 20R-22841, Oakville
6. Block 33, Plan 20M-1281, designated as Parts 1 to 7, inclusive, on Plan 20R-22890, Oakville
7. Block 34, Plan 20M-1281, designated as Parts 14 and 15 on Plan 20R-22841, Oakville





**THE CORPORATION OF THE TOWN OF OAKVILLE**

**BY-LAW NUMBER 2025-060**

A by-law to repeal By-law 1985-101, a by-law to declare that certain land is not subject to part lot control (Block 3, 4 and 5, Plan 20M=246)

**COUNCIL ENACTS AS FOLLOWS:**

1. By-law 1985-101 is hereby repealed.

PASSED this 17<sup>th</sup> day of March, 2025

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MAYOR

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CLERK



THE CORPORATION OF THE TOWN OF OAKVILLE

BY-LAW NUMBER 2025-061

A by-law to amend By-law 2021-136, being a by-law to designate St. John's United Church at 262 Randall Street as a property of cultural heritage value or interest.

**WHEREAS** section 30.1 of the *Ontario Heritage Act*, R.S.O. 1990, c. O.18, as amended (OHA), authorizes the council of a municipality to amend a by-law designating property made under section 29 of the OHA to: clarify or correct the statement explaining the property's cultural heritage value or interest or the description of the property's heritage attributes; correct the legal description of the property; or otherwise revise the language of the by-law to make it consistent with the requirements of the OHA or the regulations;

**WHEREAS** the Council for the Corporation of the Town of Oakville on December 7, 2021 passed By-law 2021-136, being "A by-law to designate St. John's United Church at 262 Randall Street as a property of cultural heritage value or interest";

**WHEREAS** the Council for the Corporation of the Town of Oakville on January 20, 2025, after consultation with the Heritage Oakville Advisory Committee, approved a proposal to amend By-law 2021-136 to clarify the statement explaining the property's cultural heritage value or interest and the description of the property's heritage attributes;

**WHEREAS** the Council for the Corporation of the Town of Oakville, by resolution passed on January 20, 2025, has caused to be served on the owners of the lands and premises at 262 Randall Street, Oakville, ON and upon the Ontario Heritage Trust, notice of intention to amend By-law 2021-136, pursuant to the requirements of subsection 30.1(4) of the OHA, and further, has caused the notice of intention to amend the by-law to be published on the town's website in accordance with the town's *Ontario Heritage Act* Alternative Notice Policy;

**WHEREAS** no notice of objection to the proposed amendment was served on the municipality February 24, 2025, being the last date for filing an objection;

**AND WHEREAS** pursuant to subsection 30.1(7) of the OHA, the Council of the Town of Oakville may now pass the proposed amending by-law;

**COUNCIL ENACTS AS FOLLOWS:**

1. That Schedule “B” of By-law 2021-136 be deleted and replaced with Schedule “B” attached hereto; and,
2. That the Town Solicitor be authorized cause a copy of this by-law to be served on the owner of the designated property and on the Ontario Heritage Trust, and to be registered against the property described in Schedule “A” at the Land Registry Office.

PASSED this 17<sup>th</sup> day of March, 2025

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MAYOR

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CLERK

SCHEDULE "A" TO  
BY-LAW 2025-061

In the Town of Oakville in the Regional Municipality of Halton, property description  
as follows:

St. John's United Church

262 Randall Street

LTS A & B & PT LTS D & E BLK 4, PL 1, PT 1, 20R17371; OAKVILLE. S/T EASE IN  
FAVOUR OF PT LTS D & E, BLK 4, PL 1, PT 2, 20R17371 OVER PTS 1 & 2,  
20R17445 AS IN HR611286.

Town of Oakville, Regional Municipality of Halton

PIN: 24813-0366

SCHEDULE "B" TO  
BY-LAW 2025-061

STATEMENT OF CULTURAL HERITAGE VALUE OR INTEREST

Description of Property – St. John's United Church, 262 Randall Street

The St. John's United Church property is located at the southeast corner of Randall Street and Dunn Street in downtown Oakville. The property contains a 19<sup>th</sup> and early 20<sup>th</sup> century brick church building known as St. John's United Church.

Statement of Cultural Heritage Value or Interest

*Design Value or Physical Value*

The church property has cultural heritage value for its Gothic Revival church building and Neo-Gothic Lusk Hall addition, as well as its Memorial Garden. These structures are well-executed examples of their styles and display a high degree of craftsmanship.

The Gothic Revival style was prevalent in Ontario from 1830-1900 and very common in churches of all Christian denominations. In the latter half of the century, during which time the St. John's United Church building was constructed, the style was influenced not only by Gothic architecture from England but from central and southern Europe as well. The subject church building contains the hallmark of this style, the pointed lancet window, which is embellished throughout with polychromatic brickwork that echoes the window's pointed arch. The church's Gothic Revival style is also evident in its steep roof with cross gables, buttresses, quatrefoil windows and date marker, as well as the Gothic style details in the dichromatic brickwork. The church is a well-executed example of its style and displays a high degree of craftsmanship.

The Neo-Gothic style used for Lusk Hall was a more understated version of the Gothic style, prevalent in the first half of the 20<sup>th</sup> century and most common in scholastic buildings. Whereas Gothic style churches were heavily decorated in picturesque details like lancet windows pointing to the heavens, Neo-Gothic structures adopted some of the Gothic elements but applied them on a simpler and often larger scale for institutional buildings. Lusk Hall contains many of the features typical of this style, including decorative buttresses, a parapet with a battlement motif, monochromatic brickwork, a multiplicity of muntins in the windows, and arched doorways and windows.

The church property also has cultural heritage value for its Memorial Garden located on the south side of the church building and the west side of Lusk Hall. Designed by architect George Farrow, the Memorial Garden was constructed in 1986 and is delineated by a low brick wall containing a paved area, scattering grounds and commemorative plaques. The pavers were designed to mimic the arched windows of the church and the plaques on the wall of the church building resemble shields. At the entrance to the garden is the highly visible carillon tower, designed by Ron Baird, one of Canada's most renowned and successful sculptors. The 14-metre-tall steel tower houses electronic carillon bells and is topped with a kinetic eagle weathervane with a cross.

#### *Historical Value or Associative Value*

The subject property has cultural heritage value for its direct associations with St. John's United Church and its roots as a Methodist Church. The Wesleyan Methodist Church in Oakville was established in the early 1830s after two decades of church services being organized by saddle-bag preachers, or circuit riders. The church built its first structure in the 1830s and after being moved to a handful of buildings, finally settled in the current structure in 1877 which has been its home ever since. The Memorial Garden, including its scattering grounds, garden wall, plaques, carillon tower and oak tree, were built in memory of the church's early members, including the Shroeder, Leonard and New families.

The property is also directly associated with the development of Oakville throughout the 19<sup>th</sup>, 20<sup>th</sup> and 21<sup>st</sup> centuries. Many individuals who played a significant role in the development of Oakville attended the church and made important contributions to the church and the town over the years. The original 1877 church building and the 1923 Lusk Hall together have been significant buildings within the church community itself, but also within the larger community of Oakville. In addition to regular church services and group meetings, the space has hosted innumerable events such as weddings, funerals, baptisms, fundraisers – ordinary but significant occasions that are fundamental to the everyday life of a small-town community.

#### *Contextual Value*

The property also has contextual value as a landmark within downtown Oakville. The church building is important in defining, maintaining and supporting the character of downtown as an integral part of its fabric. As is the case in all small Ontario towns, as Oakville was when the structure was built, churches and their easily recognizable steeples are significant and familiar landmarks that define a downtown area. St. John's United Church is physically, functionally, visually and historically linked to its surroundings and not only provides a physical link to Oakville's past but continues to define the downtown area in the present.

### Description of Heritage Attributes

Key attributes of the property which embody the cultural heritage value of St. John's United Church include the following, as they relate to the north, east, south and west exterior elevations of the 1877 church building:

- The form of the T-shaped building formed by the sanctuary and rear vestry/Sunday School wing, including its steeply pitched gable roofs, parapets, truncated chimneys below the roofline, and brick and stone buttresses;
- The dichromatic red and buff brick walls in Common Brick Bond, including headers and other decorative brickwork;
- Fenestration of the original door and window openings;
- Set of historic wood doors on northwest corner of the vestry/Sunday School wing, including metal brackets;
- All historic wood, stained glass, and leaded glass windows, including wood trim and stone sills;
- Wood quatrefoil date marker reading "1877 A.D.";
- Wood louvered vents with associated stone lintels and sills;
- Exposed wood roof eaves on vestry/Sunday School wing;
- Stone coping on parapets and buttresses;
- Lakestone foundation where it is exposed above grade; and
- Slate roof.

Key attributes of the property which embody the cultural heritage value of St. John's United Church include the following, as they relate to the interior of the 1877 church building:

- The remaining elements of the original gallery, including its structure, decorative woodwork and metalwork, and cast iron columns supporting it.

Key attributes of the property which embody the cultural heritage value of St. John's United Church include the following, as they relate to the north, east, south and west exterior elevations of the 1923 Lusk Hall:

- The two storey rectangular form of the building, including its polygonal bay window projection on the west elevation, and brick buttresses;
- The red brick cladding in Common Brick Bond, including red brick headers and decorative angled brickwork on the parapet;
- Fenestration of the original door and window openings;
- The Gothic design of the wood portions of the windows;
- Stone window sills;
- Stone date marker reading "Lusk Hall 1923 A.D.".

Key attributes of the property which embody the cultural heritage value of St. John's United Church include the following, as they relate to the Memorial Garden and courtyard on the southwest portion of the property:

- The oak tree in the middle of the courtyard, along with the accompanying bronze plaque noting it was planted in 1969 in memory of Gordon Leonard;
- The Memorial Garden, including low masonry walls and the presence of pavers mimicking the arched windows of the church;
- The metal carillon tower;
- The bronze 'shield' plaques on the wall of the church; and
- The bronze plaque commemorating the carillon bells and tower on the wall of the church.

For the purposes of clarity, the 1952 one-storey vestibule and the 1967 one-storey rear wing are not considered to be heritage attributes.





**THE CORPORATION OF THE TOWN OF OAKVILLE**

**BY-LAW NUMBER 2025-067**

A by-law to declare that certain land is not subject to part lot control (Block 123, Plan 20M-1272, and Blocks 22, 23, 24, 25, 30 and 31, Plan 20M-1281 – Haven-Oak Homes Phase 5 Inc.)

**WHEREAS** By-law 2006-125 delegates to the Director of Planning and Development the authority to approve certain applications to designated lands not subject to part lot control; and,

**WHEREAS** the Director of Planning and Development has approved such an application for the lands described in Schedule “A”;

**COUNCIL ENACTS AS FOLLOWS:**

1. Part lot control pursuant to subsection 5 of Section 50 of the *Planning Act*, R.S.O. 1990, c.P-13, as amended does not apply to lands as set out in Schedule “A” attached hereto.
2. This by-law expires one (1) year from the date it has been passed by Council.
3. Schedule “A” forms part of this by-law.
4. The solicitor is hereby authorized to amend the parcel designation, if necessary, upon registration of this by-law.

PASSED this 17<sup>th</sup> day of March, 2025

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MAYOR

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CLERK

**Schedule “A”**

1. Block 123, Plan 20M-1272, designated as Parts 1 and 2 on Plan 20R-22876, Oakville
2. Block 22, Plan 20M-1281, designated as Parts 4 and 5 on Plan 20R-22876, Oakville
3. Block 23, Plan 20M-1281, designated as Parts 6 and 7 on Plan 20R-22876, Oakville
4. Block 24, Plan 20M-1281, designated as Parts 8 and 9 on Plan 20R-22876, Oakville
5. Block 25, Plan 20M-1281, designated as Parts 10 and 11 on Plan 20R-22876, Oakville
6. Block 30, Plan 20M-1281, designated as Parts 1 to 13 inclusive, on Plan 20R-22899, Oakville
7. Block 31, Plan 20M-1281, designated as Parts 14 to 25, inclusive, on Plan 20R-22899, Oakville



## THE CORPORATION OF THE TOWN OF OAKVILLE

### BY-LAW NUMBER 2025-068

A by-law to declare that certain land is not subject to part lot control Blocks 271, 276 and 281, plan 20M-1288 – Mattamy (Joshua Creek) Limited)

**WHEREAS** By-law 2006-125 delegates to the Director of Planning and Development the authority to approve certain applications to designate lands not subject to part lot control; and

**WHEREAS** the Director of Planning and Development has approved such an application for the lands described in Schedule “A”;

#### **COUNCIL ENACTS AS FOLLOWS:**

1. Part lot control pursuant to subsection 5 of Section 50 of the *Planning Act*, R.S.O. 1990, c.P-13, as amended does not apply to lands as set out in Schedule “A” attached hereto.
2. This by-law expires one (1) year from the date it has been passed by Council.
3. Schedule “A” forms part of this by-law.
4. The solicitor is hereby authorized to amend the parcel designation, if necessary, upon registration of this by-law.

PASSED this 17<sup>th</sup> day of March, 2025

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MAYOR

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CLERK

## **SCHEDULE “A”**

1. Block 271, Plan 20M-1288, designated as Parts 1 to 8, inclusive, on Plan 20R-22895, Oakville
2. Block 276, Plan 20M-1288, designated as Parts 1 to 6, inclusive, on Plan 20R-22894, Oakville
3. Block 281, Plan 20M-1288, designated as Parts 1 to 12, inclusive, on Plan 20R-22900, Oakville



**THE CORPORATION OF THE TOWN OF OAKVILLE**

**BY-LAW NUMBER 2025-069**

A by-law to confirm the proceedings of a meeting of Council.

**COUNCIL ENACTS AS FOLLOWS:**

1. Subject to Section 3 of this by-law, every decision of Council taken at the meeting at which this by-law is passed and every resolution passed at that meeting shall have the same force and effect as if each and every one of them had been the subject matter of a separate by-law duly enacted.
2. The execution and delivery of all such documents as are required to give effect to the decisions taken at the meeting at which this by-law is passed and the resolutions passed at that meeting are hereby authorized.
3. Nothing in this by-law has the effect of giving to any decision or resolution the status of a by-law where any legal prerequisite to the enactment of a specific by-law has not been satisfied.

PASSED this 17<sup>th</sup> day of March, 2025

\_\_\_\_\_  
Rob Burton

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Andrea Holland Acting Town Clerk